

PLAN COMMISSION

INTER-DEPARTMENTAL REVIEW COMMITTEE REPORT

CONDITIONAL USE FOR A PLANNED DEVELOPMENT – YORKTOWN COMMONS

NOVEMBER 23, 2015

Title

PC 15-27C

Property Owner

KRE YTC Venture LLC
203 Yorktown
Lombard, IL 60148

Petitioner – Consultant

Rosanova & Whitaker, Ltd.
c/o Russell Whitaker
30 W. Jefferson St., Ste. 200
Naperville, IL 60540

Property Location

Northeast corner of Butterfield
Rd. and Highland Ave.
Trustee District #3

Zoning

B3PD Community Shopping
District Planned Development

Existing Land Use

Regional Shopping Center

Comprehensive Plan

Regional Commercial

Approval Sought

Approve a conditional use for a
planned development with
companion deviations, use
exceptions, and variations.

Prepared By

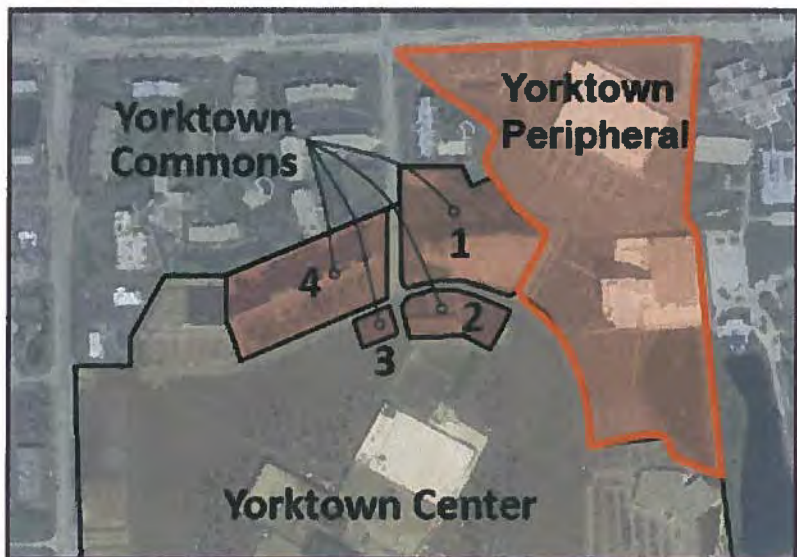
Matt Panfil, AICP
Senior Planner



LOCATION MAP

DESCRIPTION

Now that Parcels 1 through 4 have been removed from their respective planned developments, see PC 15-27A and PC 15-27B, the petitioner requests approval of a conditional use for an approximately fifteen (15) acre, mixed-use planned development entitled Yorktown Commons.



There are no specific physical developments associated with this request. Similar to previously approved planned development such as Highlands of Lombard and Fountain Square, the petitioner is only seeking to establish the terms for which development may occur.

PROJECT STATS

Planned Development Size

Parcel 1:	5.9 acres
Parcel 2:	1.8 acres
Parcel 3:	0.5 acres
Parcel 4:	6.0 acres

Total: 14.2 acres

Submittals

1. Petitions for public hearings, dated October 19, 2015;
2. Three (3) separate project narratives with responses to standards corresponding to each item, PC 15-27 A through C, dated October 19, 2015;
3. Yorktown Commons PD Design Guidelines, prepared by Torti Gallas + Partners, and dated October 19, 2015;
4. Yorktown Commons Traffic Study and Appendix, prepared by Eriksson Engineering Associates, Ltd., and dated October 19, 2015;
5. Parking Memorandum from Stephen B. Corcoran, P.E., PTOE, Dir. of Traffic Engineering for Eriksson Engineering, Associated, Ltd., dated October 15, 2015;
6. Natural Resource Review prepared by the Illinois Dept. of Natural Resources at the request of V3 Companies, Ltd., and dated July 21, 2015;
7. Civil Design Guidelines, prepared by V3 Companies, Ltd., and dated October 19, 2015;
8. Prelim. Stormwater Analysis, prepared by V3 Companies, Ltd., and dated October 19, 2015;

This approach will allow for the property to be developed competitively with other properties within the Yorktown Shopping Center and the Butterfield Road Corridor. While no definitive plans, structures, or tenants are being presented at this time, lot, bulk, and design standards and guidelines are enumerated within the attached "Yorktown Commons Planned Development Design Guidelines" (Exhibit B), which is essentially a regulatory tool for future development.

Design Standards:

Critical to the land use planning of this proposal is the incorporation of said design guidelines which are presented in a format which is commonly referred to as a "form-based code."

By utilizing a form-based approach, the petitioner maintains a certain degree of market flexibility in regards to the various combinations of development that may occur within the subject property, and the Village has a level of predictability of how the subject area will look and function through the establishment of certain lot, bulk, and design standards. The concept plans depicting building type and location included within the Yorktown Commons Planned Development Design Guidelines document are only illustrative and could be adjusted in any number of ways to meet market conditions or aesthetic / design preferences.

Flexibility to the market is important to the petitioner as the nature of regional shopping centers continues to evolve from their automobile-dominated, post-war origins. The petitioner's team has conducted extensive research that suggests an increased desire on behalf of both businesses and residents for a more traditional mixed-use type of environment where housing, work, and shopping and entertainment are all in close proximity. The new supply of residents will also benefit the surrounding commercial uses by expanding their customer base.

Review and Approval Process:

As development of the property occurs, the Plan Commission will maintain oversight of the development through the "Administration" provisions established in Section IV of the Yorktown Commons Planned Development Design Guidelines. The process will allow for each separate parcel to apply for Site Plan Approval as a distinct phase of the overall development. The Director of Community Development will determine if the plan conforms substantially to the preliminary plan established within PC 15-27C or if it constitutes either a minor change or major change to the preliminary plan, as defined by Section IV (E) and (F) of the Yorktown Commons Planned Development Design Guidelines.

Submittals (cont.)

9. Wetland Delineation and Assessment Report, prepared by V3 Companies, Ltd., and dated August 12, 2015;
10. Prelim. Plat of Subdivision, prepared by V3 Companies, Ltd., and dated October 14, 2015; and
11. Five (5) ALTA/ACSM Land Title and Topographic Surveys, one each for Parcels One through Three and both an east and west survey for Parcel Four, prepared by V3 Companies, Ltd., and dated October 5, 2015.

If a proposal conforms substantially to the preliminary plan, no public hearing is required and the proposal will appear before the Plan Commission for final Site Plan Approval. If the proposal represents a major change to the preliminary plan, then the item will require a public hearing before the Plan Commission, who will make a recommendation to the Village Board for final consideration.

Development Schedule / Phasing:

Although the anticipated phases are not an obligation and can change depending on market conditions, etc., pursuant to Section 155.507 (B)(3)(h) the petitioner has indicated that the anticipated development schedule is the same as the numbers assigned to each relevant parcel, with Parcel 1 being anticipated to be developed first and Parcel 4 anticipated to develop last.

APPROVAL(S) REQUIRED

Pursuant to Section 155.502 (F)(3) of the Lombard Ordinance, the petitioner requests approval of a conditional use for a planned development with the following companion deviations, use exceptions, and variations:

1. Deviations and variations from the B3 Community Shopping District's lot and bulk requirements, Sections 155.415 (D), (E), (F), (G), (H), (I), (J), and (K) of the Lombard Zoning Ordinance, to allow for the establishment of a form-based code predicated upon lot, bulk, and design standards and guidelines as set forth within the Yorktown Commons Planned Development Design Guidelines included within the petitioner's submission;
2. Approve the Yorktown Commons Planned Development Design Guidelines as the preliminary plan for the subject property and to extend the timeframe for the final plan approval as set forth in the Yorktown Commons Planned Development Design Guidelines;
3. A variation from Section 155.508 (B)(3) of the Lombard Zoning Ordinance to allow for use exceptions, ground-floor residential dwelling units, for more than forty percent (40%) of the site are or more than forty percent (40%) of the total floor area, whichever is less;
4. A variation from Section 155.707 of the Lombard Zoning Ordinance to waive transitional landscape yard requirements where residential uses are proposed in the B3 Community Shopping District; and

5. A variation from the Planned Development Standards, Article IX of the Lombard Zoning Ordinance, to establish the process for administration of the Yorktown Commons Planned Development as set forth in Section IV of the Yorktown Commons Planned Development Design Guidelines and granting site plan approval authority to the Plan Commission, pursuant to Section 155.511 of the Lombard Zoning Ordinance.

EXISTING CONDITIONS

As summarized in the PC 15-27A and PC 15-27B IDRC Reports, the geographic area of the proposed Yorktown Commons Planned Development is as follows:

- Parcel 1: Vacant parcel with a constructed ditch and remnant parking lot from the Yorktown Condos temporary sales office / model showroom (circa 1973).
- Parcel 2: Vacant structure (former Bamboo Room).
- Parcel 3: Surface parking for JC Penny's Department Store.
- Parcel 4: Eastern part of the Yorktown Convenience Center.

INTER-DEPARTMENTAL REVIEW

Building Division:

The Building Division has been involved with numerous conversations with the petitioner regarding building type and previously approved Building Code amendments. A full Building Code review will be conducted for any and all future development submissions.

Fire Department:

As the proposal involves only the preliminary stages of an overall redevelopment plan for the area, the Fire Department has no issues or concerns regarding the project at this time.

Private Engineering Services (PES):

Private Engineer Services has the following comments regarding the proposed project:

1. Within the Preliminary Stormwater Analysis (Exhibit C), PES requests additional language to be included within the document acknowledging that the assessment is based upon the current Village of Lombard and DuPage County Stormwater Ordinances with effective dates in 2013. Future development permit applications will be required to follow the stormwater ordinances in effect at the time of their submission; and
2. The Wetland Delineation and Assessment Report (Exhibit D) verifies that no wetland areas, Waters of DuPage, or Waters of the U.S. were delineated on the subject property.

The report also found that eastern cottonwood and common buckthorn were the most dominant plant species within the subject area, which are not plant species inherently worthy of preservation at this site.

Public Works:

The Department of Public Works has the following comments regarding the proposed project:

1. Public Works supports the concept of changing the street lighting standard from the commonly used cobra-arm commercial lighting to the Village's specified residential lighting standards. The specific locations and lighting to be used will be reviewed as part of any future development;
2. The right-of-way cross section referenced within the Yorktown Commons Planned Development Design Guidelines is consistent with the concepts being promoted within the Department of Public Works' bicycle and pedestrian planning efforts currently underway. However, the final design of right-of-way cross sections is still subject to Public Works' review at the time of the submission of any development.

Also, the Village will require these right-of-way improvements as part of the development of either Parcel 1 or Parcel 4, whichever occurs first; and

3. Although not under the purview of the Plan Commission, the Department of Public Works also notes that they have engaged a consultant to determine whether the existing sanitary lift station and force main serving the subject area has adequate capacity for the proposed additional effluent. A report with the results of this study is expected by January 2016.

At this time, it remains possible that the proposed planned development will need to either increase the capacity of the existing lift station or construct a new sanitary lift station, and possibly also install a new force main. Please note that the submitted Civil Design Guidelines (Exhibit E), which include parcel specific recommendations for demolition, layout and paving, sanitary sewer, water main, stormwater management, lighting, and private utilities, do not mention this issue.

Planning Services Division:

The Planning Services Division notes the following:

1. Surrounding Zoning & Land Use Compatibility

	Zoning	Land Use
North	B3PD & R5PD	Yorktown Peripheral PD, Yorktown Apartments, and Yorktown Condos
South	B3PD	Yorktown Shopping Center Planned Development
East	B3PD	Yorktown Shopping Center and Yorktown Peripheral Planned Developments
West	B3PD	Yorktown Shopping Center Planned Development

In consideration that the nearest adjacent properties were either once part of the same development (Yorktown Shopping Center and Yorktown Peripheral planned developments) or are themselves multi-family residential, staff finds the proposed Yorktown Commons Planned Development and associated preliminary plan for redevelopment as either mixed or residential-only uses is consistent with the zoning and land uses of surrounding properties.

2. Comprehensive Plan Compatibility

As the petitioner identifies in the project narrative for PC 15-27C, the Lombard Comprehensive Plan emphasizes a focus on, "the improvement and maintenance of existing development and encouraging

compatible development and redevelopment.” Furthermore, as part of the vision of the Comprehensive Plan, Vision 3 specifically identifies, “Lombard will create and maintain viable commercial districts throughout the Village.” Among the recommended actions to achieve Vision 3, the Comprehensive Plan includes the following:

1. Design and facilitate a Village-wide system of commercial development providing for neighborhood, community and regional commercial areas;
3. Ensure that commercial and retail development is compatible with neighboring land use areas; and
5. Ensure that new development and redevelopment of private properties is in scale with and designed to be complementary to existing development.

Taking into consideration Yorktown Shopping Center’s status as a prominent existing development, the proposal is consistent with efforts to maintain existing development and encourage compatible redevelopment. Also, the proposed Yorktown Center Planned Development Design Guidelines ensure that future development will be compatible with and consistent in scale and design with neighboring land uses.

While the proposal is for predominantly residential uses, the Comprehensive Plan defines regional commercial as, “a larger, more intensively developed mixed-use commercial area that serves as a key center for the region.” Staff finds that the mixed-use proposal is consistent with the regional commercial future land use designation for the property. Furthermore, as identified within the project narrative, the subject property is a poor location for retail-only land uses due to its poor visibility.

3. *Zoning Ordinance Compatibility*

For the Plan Commission’s convenience, a color-coded table comparing the proposed zoning standards with the underlying B3 Community Shopping District and existing Yorktown Shopping Center / Yorktown Peripheral planned developments has been included as Exhibit F. Within the table, items are coded into one of three (3) categories:

- | | |
|---------|--|
| Blue: | The proposed planned development is more permissive than the underlying zoning, but roughly equivalent to the existing planned developments. |
| Orange: | The proposed planned development is more permissive than the underlying zoning, but stricter than the existing planned developments. |
| Green: | The proposed planned development is more permissive than both of the existing planned developments. |

A. Land Uses:

Within Section VI of the Yorktown Commons Planned Development Design Guidelines, the petitioner has identified all permitted uses in the B3 Community Shopping District as permitted uses within the Yorktown Commons Planned Development. Other specifically listed permitted uses include: various types of residential uses, hotels, daycare as an accessory use, and mixed-use buildings that incorporate any of the permitted uses.

This approach to land use is similar to that within the existing Yorktown Shopping Center Planned Development, except the Yorktown Shopping Center PD allows for any permitted or conditional use within a "B" classified zoning district as a permitted use. While the petitioner has not enumerated conditional uses within the Yorktown Commons Planned Development Design Guidelines, staff requests specific language be added that all conditional uses in the B3 Community Shopping District be conditional uses within the Yorktown Commons Planned Development.

In regards to the requested variation from Section 155.508 (B)(3) of the Lombard Zoning Ordinance to allow for ground-floor residential dwelling units as use exceptions, staff notes that the Lombard Zoning Ordinance does specifically permit, "dwelling units, located above the first floor, provided: at least fifty percent (50%) of the square footage of the building footprint on the ground floor is devoted to uses permitted (permitted uses and/or approved conditional uses)" within the B3 Community Shopping District. Therefore, if the underlying zoning were in effect, the petitioner already has the right to construct mixed-use buildings within the subject property. However, such mixed-use buildings would be limited by the maximum building height limitation of two (2) stories or thirty feet (30'), whichever is less.

Staff can support the requested use exception variation based on the fact that ground floor residential dwelling units would be permitted by right in up to forty-nine percent (49%) of the ground floor area. While the petitioner intends on including ground floor commercial space, the demand for commercial space within the subject area has shown to be less than what would be required by a fifty percent (50%) ground floor commercial space requirement.

In summary, the uses within the Yorktown Commons Planned Development are more limiting than the neighboring Yorktown Shopping Center and Yorktown Peripheral planned developments; however, the proposed planned development is more flexible in housing type and the amount of ground-floor residential permitted. With the condition that the B3 Community Shopping District conditional uses identified in Section 155.415 (C) be specifically included within the Yorktown Commons Planned Development Design Guidelines, staff can support the proposed land uses associated with the project.

In regards to the petitioner's request for a variation from Section 155.415 (I) of the Lombard Zoning Ordinance, staff can support the variation provided the petitioner provide language within the Yorktown Commons Planned Development Design Guidelines that while outdoor service areas (including outdoor dining) are permitted, outside storage is not permitted within the planned development.

B. Lot Bulk:

Although the petitioner has requested variations for both lot area and lot width, Sections 155.415 (D) and (E) of the Lombard Zoning Ordinance respectively, none of the four (4) proposed parcels are less than the minimum required 20,000 square feet area of area or one-hundred foot (100') width requirements. However, it is anticipated that future resubdivisions of the subject parcels may result in substandard lot area and/or lot width. Staff can support the requested variations in order to allow maximum flexibility in building type and layout for future development. The other design standards within the Yorktown Commons Planned Development Design Guidelines will further assure quality development, regardless of lot area or lot width.

C. Setbacks:

In lieu of the traditional building setbacks within the B3 Community Shopping District, thirty feet (30') for front, rear, and corner side yards and ten feet (10') for interior side yards, the petitioner is proposing to utilize "build-to-lines" and "frontage occupancy" standards which instead of mandating a specific minimum distance a building is to be from a property line, require a certain percentage of a structure to be built within a certain distance of the property line. Staff finds that the proposed build-to-lines and frontage occupancy requirements will help establish a more pedestrian-oriented street-wall where buildings are closer to each other and their entrances are closer to sidewalks.

In consideration that the proposed build-to-lines and frontage occupancy requirement further the vision of the Yorktown Commons Planned Development, and that said regulations are generally more restrictive than the existing Yorktown Shopping Center and Yorktown Peripheral planned developments, staff supports the requested setback variations associated with Section 155.415 (F) of the Lombard Zoning Ordinance.

D. Building Height:

The petitioner has requested a variation from Section 155.415 (G) to allow for structures on Parcel 1, Parcel 2, and Parcel 4 to be six (6) stories or one-hundred feet (100'), whichever is less. The Lombard Zoning Ordinance allows a maximum height of two (2) stories or thirty feet (30'), whichever is less. The requested height variation is similar in scope to the maximum structure height permitted within the Yorktown Shopping Center and Yorktown Peripheral planned developments, which essentially allowed for four (4) buildings to be up to approximately one-hundred and five feet (105') to one-hundred and twenty-five feet (125') in height. The range in height varies because the height is to be measured from sea level and the topography varies by up to twenty-feet throughout the site.

As the proposed six (6) stories or one-hundred feet (100') height maximum is consistent with the surrounding multi-family residential structures and the building heights allowed within the adjacent planned development, staff can support the requested building height variation.

E. Open Space:

The petitioner is proposing a minimum of twelve and one-half percent (12.5%) open space, which is consistent with the minimum open space requirement for planned developments with deviations, as established in Section 155.508 (C)(7) of the Lombard Zoning Ordinance. However, the petitioner's definition of open space includes hardscape uses such as courtyards, squares, pedestrian paths, etc., which means that the amount of open space, as defined by the Lombard Zoning Ordinance as, "that portion of a lot or property maintained as lawn, garden, field, woods, wetland, or other natural landscape area and is free of buildings, structures and impervious surfaces" can be zero percent (0%).

While the proposed open space requirement is less restrictive than the underlying zoning, it is much more restrictive than the Yorktown Shopping Center and Yorktown Peripheral planned development requirements which stipulate only up to thirty percent (30%) of the site to be occupied for building purposes provided the remaining area shall remain as open spaces. However, the definition of open spaces applied in the original Yorktown planned development includes space which may be utilized for parking, loading berths, signs, traffic lanes, and pedestrian walkways and

landscaping. While the amount of open space, as defined by the Lombard Zoning Ordinance, can also be zero percent (0%) in this scenario, the Yorktown Commons requires a certain percentage of open space to be utilized for pedestrian amenities, not just parking or drive aisles.

Therefore, staff can support the requested variation from Section 155.415 (H) in consideration that it furthers the vision of the Yorktown Commons Planned Development as a more “urban,” pedestrian-oriented environment, and that the proposed open space requirements are more beneficial to the public than those within the adjacent planned developments. It is important to note that any impervious area would be subject to Village stormwater regulations.

F. Transitional Yards:

Section 155.415 (J) of the Lombard Zoning Ordinance requires that wherever a rear or interior side yard of a lot in the B3 Community Shopping District abuts a lot in the CR Conservation Recreation District or a residence district, all principal and accessory buildings and structures shall be set back from the abutting lot line a distance of forty feet (40'). Staff concurs with the petitioner's request to waive this requirement because the proposed Yorktown Common Planned Development is predominantly residential in nature. Also, the multi-family residential zoning districts within the Lombard Zoning Ordinance (R4, R5, and R6) do not require transitional building setbacks when a multi-family residential use is adjacent to another multi-family residential use.

Similar to the transitional building setback discussed above, Section 155.415 (K) of the Lombard Zoning Ordinance also requires a thirty foot (30') wide transitional landscape yard wherever a rear or interior side yard of a lot in the B3 Community Shopping District abuts a lot in the CR Conservation Recreation District or a residence district. Staff can support the requested transitional landscape yard variation based on the same reasoning as in the case above; the proposed planned development is predominantly residential in nature and the Lombard Zoning Ordinance does not require transitional landscape yards when a multi-family residential use is adjacent to another multi-family residential use.

Section 155.707 of the Lombard Zoning Ordinance establishes the size and required improvement for transitional landscape yards. While the granting of a variation to Section 155.415 (K) would eliminate the necessity for a landscape yard, both staff and the petitioner felt it was in the best interest to request a variation from Section 155.707 for the sake of clarity.

G. Signage:

In combination with the underlying signage regulations established within the Lombard Sign Ordinance, the petitioner has proposed additional, context-sensitive, signage regulations found within Section IX of the Yorktown Commons Planned Development Design Guidelines. The proposed signage regulations differ from the Lombard Sign Ordinance requirements in the following:

1. Sign design is subject to review by the proposed Yorktown Commons Architectural Review Board (i.e., owner) prior to submission of a building permit application to the Village;

2. The Community Development Director is permitted to approve a sign that is not consistent with the design guidelines when the sign is designed as a site-specific piece of civic art, if it is determined that such sign is consistent with the intent of the planned development (assuming Village Code compliance);
3. Wall signs are limited in typology to a maximum three foot (3') tall band sign or board sign which consists of painted or vinyl graphics on a signboard;
4. Instead of regulation window sign size by limiting the total area to a maximum of twenty percent (20%) of the total window surface area, the petitioner proposes that the height of window signs be limited to one-third ($1/3^{\text{rd}}$) the height and the width of window signs be limited to ninety percent (90%) of the width of the glass in the sash where the sign is installed;
5. In regards to project signs, the petitioner proposes that the maximum distance from which a projecting sign may project from the wall to be five feet (5') instead of the Lombard Sign Ordinance requirement that projecting signs project no more than two-thirds ($2/3^{\text{rd}}$) the width of the public sidewalk or a maximum of four feet (4') from the supporting wall, whichever is less. In order to be consistent with the Lombard Sign Ordinance, staff asks that the five foot (5') maximum projection be reduced to four feet (4'). If a further extension is warranted, an application can be made for a signage deviation in association with a Site Plan Approval application. Please note that the Plan Commission has final authority to grant signage deviations associated with Site Plan Approval and such a signage deviation would not require Village Board approval.

The petitioner proposes that such a sign may only be up to thirty-two inches (32") in height, whereas the Lombard Sign Ordinance has no such requirement. Also, the petitioner requires project signs to be a minimum of nine feet (9') above the sidewalk, whereas the Lombard Sign Ordinance requires a minimum of eight feet (8') above the sidewalk;

6. The petitioner requests that sculptural and A-frame sign boards, referred to as sandwich board signs within the Lombard Sign Ordinance, to be permitted to be placed on the sidewalk if they are temporary and removed during non-operating hours. Staff can support this request, provided additional language is provided within the Yorktown Commons Planned Development Design Guidelines that specifies that sandwich board signs are to comply with Section 153.234 of the Lombard Sign Ordinance;
7. In regards to awnings, the petitioner has specified that advertising on the valance may be up to nine inches (9") in height, whereas the Lombard Sign Ordinance allows for advertising up to ten inches (10") in height.
8. In regards to real estate signs, staff requests that the petitioner provide additional language that real estate signs are to comply with Section 153.234 of the Lombard Sign Ordinance;

9. The petitioner has specified that temporary signs be permitted for a maximum time period of seventy-two (72) hours, which is shorter in duration than any of the permitted temporary signs within the Lombard Sign Ordinance.

H. Parking:

In combination with the underlying parking regulations established within Section 155.600 of the Lombard Zoning Ordinance, the petitioner has proposed additional, context-sensitive, parking regulations found within Section VIII (C) of the Yorktown Commons Planned Development Design Guidelines. The proposed parking regulations differ from the underlying zoning in the following:

1. The petitioner proposes a system of three (3) layers in which parking is regulated. The first layer is located between the lot line along the primary frontage and the build-to-line. The second layer is located between the build-to-line and a twenty foot (20') setback from the build-to-line. The third layer is located in the remaining portion of the lot behind the second layer. Using this system, the petitioner is proposing to restrict off-street parking within the third layer along primary streets and within the second or third layer along secondary streets.

The petitioner is proposing to require townhouse parking to be access via an alley or rear drive and garages for townhome units shall not have direct access from Grace Street or the Yorktown Center ring road. Also, single-family and two-family residential dwelling units shall be accessed via an alley.

Furthermore, the petitioner requests that off-site parking may fulfill the requirements for off-street parking if the off-street parking spaces are within five-hundred feet (500') of the building or use served by the off-street parking area. Such parking is permitted by right in the Yorktown Shopping Center Planned Development.

Staff can support these proposals as they are more restrictive than the Lombard Zoning Ordinance and should help shape a pedestrian-oriented environment that is consistent with the vision of the planned development.

2. To further provide a more pedestrian-oriented environment, the petitioner proposes to require all multi-level parking structures to be designed with a "liner building" facing the street(s). A liner building is defined within Section XII of the Yorktown Commons Planned Development Design Guidelines as, "a building that conceals a separately constructed garage and that is designed for occupancy by residential, commercial, or mixed-uses."
3. While Section VIII (C)(3)(ii)(a) of the Yorktown Commons Planned Development Design Guidelines stipulates that there shall be no fewer than one (1) parking space per unit, the petitioner has agreed that the minimum parking spaces requirement be consistent with Section 155.602 Table 6.3 of the Lombard Zoning Ordinance, which requires a minimum of one and one-half (1½) parking spaces for one and two bedroom multi-family dwelling units.

4. In Figure 37 of the Yorktown Commons Planned Development Design Guidelines (right), the petitioner proposes a parallel parking space width of seven feet (7') and a compact parking space width of eight feet (8'). The Village recently completed an update, which was researched and evaluated against modern industry standards, to the Lombard Zoning Ordinance parking dimension standards at the behest of the current property owner (see PC 14-02). The recently adopted dimensions specify a parallel parking space to be a minimum of nine feet (9') in width and the narrowest, compact parking space, to be a minimum of eight foot three inches (8'3") in width. The proposed dimensions which are more permissive in the Yorktown Commons Planned Development Design Guidelines than the existing dimensions in the Lombard Zoning Ordinance are highlighted in red.

While staff understands the pedestrian-oriented reasoning behind the requested dimension reductions, if such reduced parking space dimensions are required for future development, it is in the best interest of the Village that such dimensions be specifically identified and reviewed on the merits of the specific development. Therefore, staff requests that the petitioner amend the parking lot dimensions highlighted in red to reflect the current Lombard Zoning Ordinance standards.

Parking Lot Dimensions

Space Angle a	Space Width b	Space Length c	Aisle 1-Way d	Aisle 2-Way e	Curb Width f
90°	9'	18'	24'	24'	9'
60°	9'	19'	18'	20'	10'-5"
45°	9'	18'	15'	20'	12'-9"
Parallel	7'	22'	12'	24'	N/A
Compact Space Dimensions					
90°	8'	16'	24'	24'	8'
60°	8'	17'-10"	18'	20'	9'-3"
45°	8'	17'	15'	20'	11'-4"

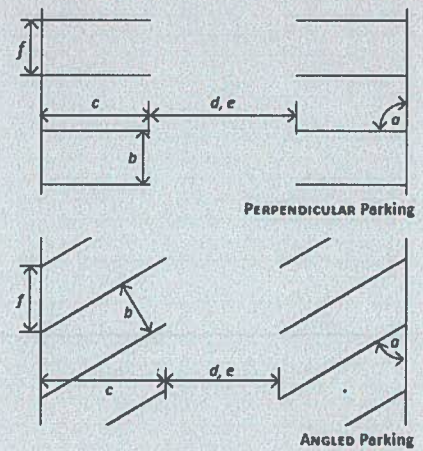


FIGURE 37: Parking Dimensions

4. Site Design

A. Yorktown Commons Planned Development Design Guidelines – Site Plan Approval

While the petitioner has included a preliminary plan with illustrative scenarios of how development may occur within the planned development, the ultimate development plans will be submitted at a later date. To facilitate an efficient review and approval process, and pursuant to Section 155.511 of the Lombard Zoning Ordinance, the petitioner requests, with staff's support, Site Plan Approval authority be granted to the Plan Commission.

B. Density:

Section VI (D) of the Yorktown Commons Planned Development Design Guidelines identifies 970 residential dwelling units, 28,000 square feet of retail space, and 100,000 square feet of other commercial space as the maximum buildout scenario for Yorktown Commons. In this scenario, the ratio of dwelling units per acre (DUs/acre) is approximately 65 DUs/acre. This ratio is very

similar to the 63 DUs/acre that was the maximum permitted density for the Yorktown Apartments Planned Development, as established by Ordinance No. 1323 in 1972.

Lombard Multi-Family Residential Developments							
Case No. or Year Built	Name	Address	Zoning	Height	# of Units	Size (Acres)	DU/Acre
Yorktown Apartments (1967 Approval)							
1998	Liberty Square Condos	2240-60 S. Grace St.	R5PD	4 stories	112	4.42	25.34
1972, SPA 14-03	Yorktown Apartments	2233, -39, and -55 S. Highland Ave.	R5PD	5-15 stories	374	7.13	52.45
		2233 S. Highland Ave.	R5PD	15 stories	236		
		2239 S. Highland Ave.	R5PD	7 stories	78		
		2255 S. Highland Ave.	R5PD	5 stories	60		
1972	Yorktown Apartments	2200-10 S. Grace St.	R5PD	6 stories	106	3.54	29.94
1969	Yorktown Green Condos	2201-22 S. Highland Ave.	R5PD	6 stories	264	12.21	21.62
Totals					856	27.30	31.36
Max. Allowed by Ord. No. 1323					1722	27.30	63.08
Unit Surplus					866		
Max. Yorktown Commons Units					970	15.00	64.67

Also, should Yorktown Commons be approved and built out to the maximum 970 dwelling units, the total number of dwelling units within the Yorktown Apartment and Yorktown Commons planned developments would be approximately 1,826, or just 104 more dwelling units than are permitted for just the geographic area of Yorktown Apartments.

While the Yorktown Apartments were never constructed to their maximum allowable density, staff finds that the proposed maximum density for Yorktown Commons is consistent with previous approvals and surrounding development. Exhibit G identifies the densities for various other multi-family developments throughout the Village.

C. Traffic & Circulation:

Eriksson Engineering Associates, Ltd. prepared the Traffic Study (Exhibit H) based upon the Year 2021 traffic conditions which projects traffic beyond the anticipated construction of the development and also incorporates a regional growth rate as well as the recently approved Yorktown Apartments expansion (SPA 14-03), Highlands of Lombard apartment project (PC 15-04), and Sam's Club project (PC 15-21). The Traffic Study also was based upon the most intense buildout scenario as identified within the Yorktown Commons Planned Development Design Guidelines; 970 residential units, and 28,000 square feet of retail space, and 100,000 square feet of other commercial space.

The study found that the proposed development and its associated traffic volumes can be accommodated by the existing area road network. Also, with the modifications approved for the Butterfield Road entrance to Yorktown Center that were recently completed and the proposed improvements for the Sam's Club (PC 15-21) project, the total project traffic volumes will work well at the intersection of Fairfield Avenue, Yorktown Center ring road, and Butterfield Road.

Key recommendations within the study include, but are not limited to:

1. The intersection of the Yorktown Center ring road and the north-south access drive immediately east of Highland Avenue (near McDonald's and Carson's Furniture Gallery) should be modified to correct existing deficiencies by providing better striping and signage to guide drivers;

2. Each parcel should provide sidewalks along their frontage with Grace Street, the Yorktown Center ring road, and the access routes to JC Penny's, Target, and the Westin; and
3. As the existing and proposed traffic volumes do not warrant Grace Street to be four (4) lanes wide, it should be modified by converting the existing curb travel lanes into a bike lane, approximately five feet (5') wide, and on street-parking, approximately seven feet (7') wide.

The Village's traffic consultant, Kenig, Lindgren, O'Hara, Aboona, Inc. (KLOA), reviewed the Traffic Study and provided comments (Exhibit I). Based on KLOA's review, the proposed Yorktown Commons Planned Development can be accommodated by the Yorktown Shopping Center's internal roadway and external intersections. There is sufficient reserve capacity at the studied intersections to accommodate the projected traffic volumes and continue operating at acceptable levels of service.

KLOA concurs with the recommendation to provide sidewalks along the frontage of each parcel on Grace Street and Yorktown Center ring road. Lastly, KLOA also concurs with the recommended striping and signage modifications at the intersection of the Yorktown Center ring road and the north-south access drive immediately east of Highland Ave (near McDonald's and Carson's Furniture Gallery). However, it is suggested that a preliminary striping plan be provided for further review.

In addition to KLOA's comments, staff notes:

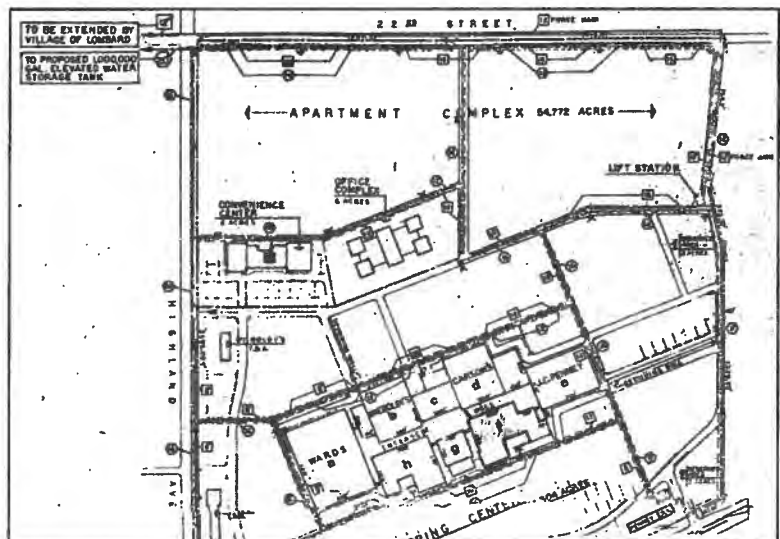
1. The traffic volume estimates established in Section III, Table 2 of the Traffic Study are to serve as a "trip bank" to be drawn upon as each site plan is approved; and
2. Bike and pedestrian connections from the surrounding residential uses to Parcel 4 are to be identified prior to, or at the time of development of said parcel.

SITE HISTORY (NON SIGN-RELATED)

1966:

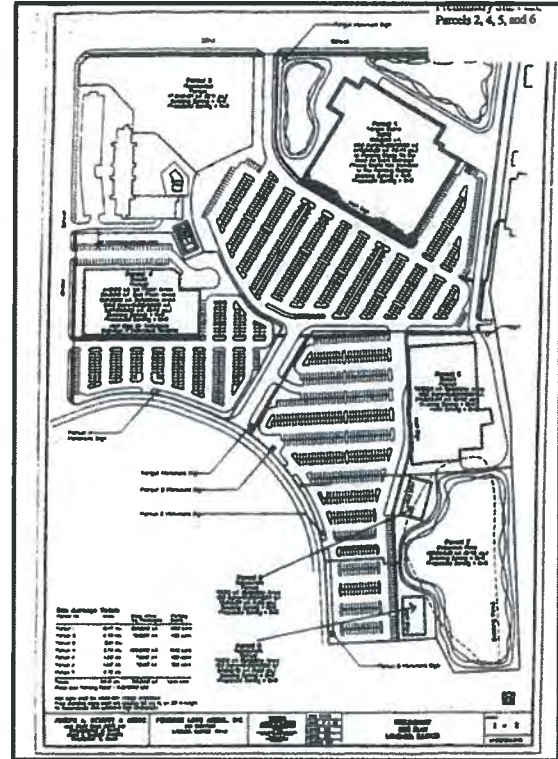
Yorktown Shopping Center Planned Development

Established of the Yorktown Shopping Center Planned Development via Ordinance No. 1172. Please note within the originally approved site plan (below) that Parcel 1 was identified for multi-family residential development and Parcel 4 was designated for use as an office complex.



PC 94-14: Yorktown Peripheral Planned Development

Amendment to the geographic boundaries of the Yorktown Shopping Center Planned Development to remove approximately 15.6 acres to establish the Yorktown Peripheral Planned Development. Please note within the originally approved site plan (lower right) that Parcel 1 was identified for use as a 100,000 square foot retail structure.



FINDINGS & RECOMMENDATIONS

Based on the above findings, the Inter-Departmental Review Committee has reviewed the petition and finds that it meets the standards for a conditional use and standards for a planned development with deviations, as established by the Lombard Zoning Ordinance. As such, the Inter-Departmental Review Committee recommends that the Plan Commission make the following motion recommending **approval** of this petition:

Based on the submitted petition and testimony presented, the proposed conditional use for a planned development with companion deviations, use exceptions, and variations **complies** with the standards required by the Village of Lombard Zoning Ordinance; and, therefore, I move that the Plan Commission accept the findings of the Inter-Departmental Review Committee Report as the findings of the Plan Commission and I recommend to the Corporate Authorities **approval** of PC 15-27C, subject to the following conditions:

1. That this relief is valid only with the approval of both PC 15-27A and PC 15-27B;
2. That this relief is limited to the establishment of a planned development with a preliminary plan and design guidelines. Any physical site improvements or alterations require approval through the Village through the established Site Plan Approval process or through a separate zoning public hearing petition, whichever is applicable; and
3. That the petitioner shall satisfactorily address all comments noted within the Inter-Departmental Review Committee Report that pertain to the proposed form-based code regulations prior to final consideration of the petition by the Village Board.

Inter-Departmental Review Committee Report approved by:

A handwritten signature in black ink, appearing to read 'William J. Heniff', is written over a horizontal line.

William J. Heniff, AICP
Director of Community Development

c. Petitioner

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EXHIBIT A – PROJECT NARRATIVE WITH RESPONSE TO STANDARDS

STATE OF ILLINOIS)
)
COUNTY OF DUPAGE)
)
VILLAGE OF LOMBARD)

**PETITION TO THE VILLAGE OF LOMBARD FOR AMENDMENT TO
THE YORKTOWN SHOPPING CENTER PLANNED DEVELOPMENT**

YTC Mall Owner, LLC and YTC Land Owner, LLC (the “Petitioner”), as the owners of certain property as more specifically set forth below, respectfully petition the Village of Lombard (the “Village”) to:

1. Revoke the Yorktown Shopping Center Planned Development with respect to the property hereafter described as the Yorktown Commons Property;
2. Amend the Yorktown Shopping Center Planned Development to reduce the required parking; and
3. Subdivide certain property as generally depicted on **Exhibit A**, hereinafter the “**Plat of Subdivision.**”

In support of the requested relief, Petitioner hereby represents to the Village as follows:

1. The Petitioner, YTC Mall Owner, LLC and YTC Land Owner, LLC, both Delaware limited liability companies, are the owners of the respective properties legally described on **Exhibit B** (the “**YTC Property**”).

2. J.C. Penney Company, a Delaware Corporation (“JCP”), is the owner of the property legally described on **Exhibit C** (the “**JCP Property**”).

3. The YTC Property and the JCP Property are collectively part of the larger Yorktown Shopping Center Planned Development (the “**Yorktown PD**”), which planned

development was approved by the Village in 1966 pursuant to Ordinance No. 1172 and facilitated the original development of the Yorktown Shopping Center.

4. The Yorktown Shopping Center Planned Development was subsequently amended in 1995 in order to remove certain properties from the original Yorktown Shopping Center Planned Development and to facilitate development of the Target property under a new planned development known as the Yorktown Peripheral Planned Development (the “**Peripheral PD**”).

5. The collective properties currently within the Yorktown PD are legally described in **Exhibit D** (collectively the “**Yorktown PD Properties**”) and the boundaries of the Yorktown PD and the Peripheral PD are generally depicted on the zoning map attached hereto as **Exhibit E**.

6. Petitioner hereby requests that the YTC Property and the JCP Property be subdivided as generally depicted on the Plat of Subdivision, as may be hereafter amended.

7. Petitioner hereby requests the Yorktown PD be revoked with respect to the properties depicted on the Plat of Subdivision as Parcel 1, Parcel 2, Parcel 3 and Parcel 4 (collectively the “**Yorktown Commons Property**”).

8. Petitioner hereby requests that the Yorktown PD be amended to reduce the required parking supply from 7,500 spaces to 6,800 spaces.

9. The proposed amendment to the Yorktown PD is necessary and appropriate to facilitate the continued success of the Yorktown Shopping Center and meets the applicable standards for a planned development with deviations as follows:

A. Any reduction in the requirements of this Chapter is in the public interest.

The actual parking requirement for the Yorktown PD and the relationship of that

parking requirement to the original planned development requirement of 7,500 spaces is relatively difficult to track. As outlined above, the area within the Yorktown PD has morphed over time, with the total area following removal of the Yorktown Commons Property being at least 50 acres less than originally approved as part of the Yorktown PD. The individual users within the Yorktown PD and the area utilized by those users are regularly changing as individual leases are executed, modified and/or terminated. Moreover, traffic and other improvements around the perimeter of the shopping center have seemingly modified the parking supply on a yearly basis. These modifications or improvements in and around the Yorktown PD have kept the shopping center relevant as part of ongoing efforts to reshape the asset consistent with industry standards and the needs/wants/desires of the general public that utilizes the shopping center. The parking study submitted concurrent with this application details the resulting parking supply for the Yorktown PD and substantiates the supply as sufficient to meet user demands based on actual parking counts conducted during peak shopping center operations. Accordingly, the proposed deviation is in the best interest of the public as it facilitates the continued improvement of the shopping center property consistent with modern trends while maintaining sufficient parking supply to meet user demand at peak operation.

B. The proposed deviation would not adversely impact the value or use of any other property.

The proposed deviation would not adversely impact the value or use of any other property as the parking study substantiates that the proposed parking supply is sufficient to meet and exceed, by a comfortable margin, the peak parking demand at the Yorktown PD.

C. That such deviations are solely for the purpose of promoting better development which will be beneficial to the residents or occupants of the planned development as well as those of the surrounding properties.

The proposed deviations will facilitate the elimination of existing parking fields that serve no practical purpose and are underutilized the vast majority, if not all of the year. As noted above, the area within the Yorktown PD has, or will shrink, by approximately 50 acres. The proposed removal of the Yorktown Commons Property from the Yorktown PD will result in a gross reduction of overall building square footage of approximately 80,000 square feet. In light of the Dunkin Donuts project and anticipated development of a parcel along Highland Avenue, we still anticipate a net reduction in building square footage of approximately 65,000 square feet. Between historic parking demand as set forth in the parking study and the reduction of usable building area, the proposed parking deviation will promote better and more efficient development between the Yorktown PD and surrounding properties.

D. That the overall floor area of the planned development shall not exceed by more than 40 percent the maximum floor area permitted for the individual uses in each applicable district.

Petitioner does not require relief to increase the permissible floor area.

E. That in residential planned developments the maximum number of dwelling units allowed shall not exceed by more than 40 percent the number of dwelling units permitted in the underlying district.

Petitioner does not propose any residential development within the Yorktown PD.

F. That all buildings are located within the planned development in such a way as to dissipate any adverse impact on adjoining buildings and shall not invade the privacy of the occupants of such buildings and shall conform to the following:

Petitioner does not propose any new buildings for the Yorktown PD.

- a. The front, side or rear yard setbacks on the perimeter of the development shall not be less than that required in the abutting zoning district(s) or the zoning district underlying the subject site, whichever is greater.*

All Yard requirements will be met under the proposed changes to the Yorktown PD.

- b. All transitional yards and transitional landscape yards of the underlying zoning district are met.*

All Yard requirements will be met under the proposed changes to the Yorktown PD.

- c. If required transitional yards and transitional landscape yards are not adequate to protect the privacy of property adjacent to the development, the Plan Commission shall recommend either or both of the following requirements:*

The proposed yards are sufficient to protect privacy of property adjacent to the development.

- i. All structures located on the perimeter of the planned development must set back by a distance sufficient to protect the privacy and amenity of adjacent existing uses;*
- ii. All structures located along the entire perimeter of the planned*

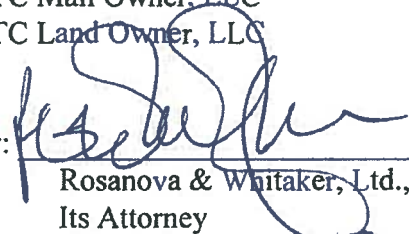
development must be permanently screened with sight-proof screening in a manner which is sufficient to protect the privacy and amenity of adjacent existing uses.

WHEREFORE, by reason of the foregoing, the undersigned Petitioner requests the Village Board and Plan Commission take the necessary steps to grant the aforementioned zoning and subdivision relief and such other relief as may be necessary and appropriate based on the request outlined above to: 1) revoke the Yorktown PD with respect to the Yorktown Commons Property; 2) to subdivide the property as depicted on the Plat of Subdivision; and 3) to grant a deviation to reduce the required parking for the Yorktown PD from 7,500 spaces to 6,800 spaces, all pursuant to the provisions of the Lombard Municipal Code.

RESPECTFULLY SUBMITTED this 19th day of October, 2015.

PETITIONER:

YTC Mall Owner, LLC
YTC Land Owner, LLC

By: 
Rosanova & Whitaker, Ltd.,
Its Attorney

SUBSCRIBED and SWORN to before me
this 19th day of October, 2015.

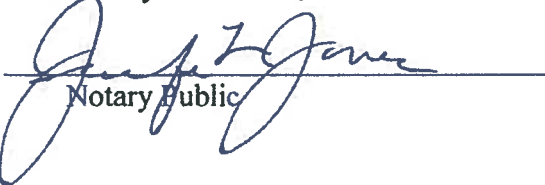

Notary Public



Exhibit List:

Exhibit A: Plat of Subdivision

Exhibit B: Legal Description of YTC Property

Exhibit C: Legal Description of JCP Property

Exhibit D: Legal Description of Yorktown PD Property

Exhibit E: Zoning Map

Exhibit A

Plat of Subdivision

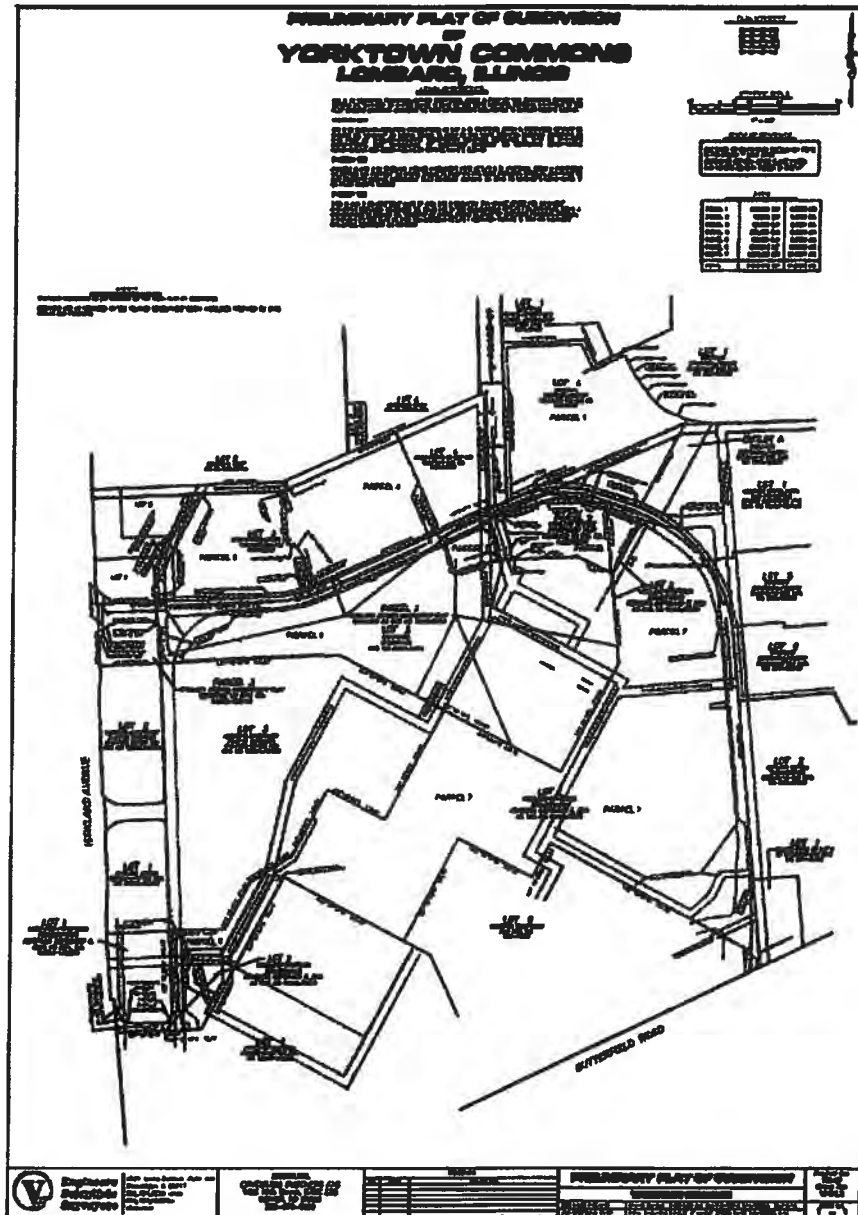


Exhibit B

YTC Property Legal Description

Bamboo Room Parcel:

Tract 1: Parcel 3 in Highland Avenue Assessment Plat of Lot 2 in Yorktown, being a subdivision of Section 29, Township 39 North, Range 11, East of the Third Principal Meridian, according to said Highland Avenue Assessment Plat recorded January 30, 2012 as document R2012-12175, in Du Page County, Illinois

Mall Parcel:

Parcel 1: Lot 2 in Highland-Yorktown Resubdivision of Lot 4 in Yorktown, being a subdivision of part of Section 29, Township 39 North, Range 11, East of the Third Principal Meridian, according to said Plat of Highland-Yorktown Resubdivision recorded September 5, 2006 as document R2006-170876, and certificates of correction recorded as documents R2006-191219, R2006-219358 and R2008-138794,

(except that part of said Lot 2 described as Commencing at a Southwest corner of said Lot 2, being a point on the Northwestern right-of-way line of Old Butterfield Road; thence North 2 degrees 58 minutes 03 seconds West, along a Westerly line of said Lot 2, a distance of 78.22 feet to an angle corner in said Lot 2; thence North 60 degrees 00 minutes 00 seconds West, along the Southwesterly line of Lot 2 aforesaid, 757.443 feet to an angle corner in said Lot 2; thence South 30 degrees 00 minutes 00 seconds West, along a Southeasterly line of Lot 2 aforesaid, 130.252 feet to an angle corner in said Lot 2, said point being also on the Southwesterly line of said Lot 2; thence North 60 degrees 00 minutes 00 seconds West, along said Southwesterly line, 282.665 feet to another angle corner in said Lot 2, said point being also the point of beginning of the parcel of air space herein described; thence North 30 degrees 00 minutes 00 seconds East, perpendicular to the last described line, 17.69 feet; thence South 60 degrees 00 minutes 00 seconds East, 160.98 feet; thence South 30 degrees 00 minutes 00 seconds West, perpendicular to the last described line, 17.69 feet to the point of intersection with the Southwesterly line of Lot 2 aforesaid; thence North 60 degrees 00 minutes 00 seconds West, along said Southwesterly line, 160.98 feet to the hereinabove designated point of beginning, said parcel having as a lower limit a horizontal plane of elevation +758.58 feet (National Geodetic Survey Datum) and having as an upper limit a horizontal plane of elevation +775.58 feet (National Geodetic Survey Datum))

also except that part of the following described tract lying within said Lot 2 described as follows: That part of Lots 2 and 4 in Yorktown, according to the plat thereof recorded September 27, 1968 as document R68-44972, described as follows: Commencing at the Southwest corner of Lot 2 in Pehrson Second Resubdivision of Lots 2 and 3 in the Resubdivision of Lot 6 of Yorktown (aforesaid) according to the plat thereof recorded September 3, 1976 as document R76-62393; Thence North 2 degrees 58 minutes 03 seconds West along the West line of the aforesaid Lot 2 in Pehrson Second Resubdivision 217.86 feet to the point of beginning of the herein described tract; Thence 611.59 feet Northwesterly along the arc of a circle tangent to the last described line convex Northeasterly having a radius of 518.00 feet and whose chord bears North 35 degrees 46 minutes 29 seconds West 575.58 feet; Thence North 70 degrees 36 minutes 56 seconds West along a line tangent to the last described curve 119.25 feet; Thence 265.51 feet Westerly along the arc of a circle tangent to the last described line, convex Northerly having a radius of 368.00 feet and whose chord bears South 88 degrees 42 minutes 54 seconds West 259.79 feet to a point on the Northwesterly line of the aforesaid Lot 2 in Yorktown, said point being 105.99 feet Northeasterly of the Southeast corner of Lot 4 in the Resubdivision of Lot 1 in Yorktown (aforesaid) according to the plat thereof recorded August 3, 1971 as document R71-37751; Thence North 68 degrees 02 minutes 44 seconds East along a line tangent to the last described curve and being the Northwesterly line of the aforesaid Lots 2 and 4 in Yorktown 604.01 feet to a corner in the Northerly line of Lot 4 in Yorktown; Thence South 89 degrees 57 minutes 16 seconds East along the North line of said Lot 4 a distance of 120.00 feet to the Northeast corner of said Lot 1; Thence South 2 degrees 58 minutes 03 seconds East along the East line of said Lot 4 a distance of 722.27 feet to the hereinabove described point of beginning)

also except that part of said Lot 2 in Highland-Yorktown Resubdivision aforesaid described as beginning at a point on the East right of way line of Highland Avenue as dedicated by document R68-10768 and the South line of said Lot 2; Thence Easterly along said South line having an Illinois East Zong Grid bearing

of North 87 degrees 35 minutes 17 seconds East 20.02 feet; Thence North 04 degrees 57 minutes 24 seconds West 49.21 feet; Thence North 29 degrees 57 minutes 24 seconds West 47.32 feet to a point on said East right of way line of Highland Avenue; Thence South 04 degrees 57 minutes 24 seconds East 91.21 feet along said East right of way line to the point of beginning)

all in Du Page County, Illinois

Convenience Center Parcel:

Parcel 1: Lots 3 and 4 (except that part of said Lot 4 dedicated for Grace Street per document R72-57164 recorded September 20, 1972) in the Resubdivision of Lot 1 in Yorktown, being a resubdivision in Section 29, Township 39 North, Range 11, East of the Third Principal Meridian, according to the Plat of said Resubdivision of Lot 1 recorded August 4, 1971 as document R71-37751, in Du Page County, Illinois

Exhibit C

JCP Property Legal Description

Tract 1: Parcel 2 in Highland Avenue Assessment Plat of Lot 2 in Yorktown, being a subdivision of Section 29, Township 39 North, Range 11, East of the Third Principal Meridian, according to said Highland Avenue Assessment Plat recorded January 30, 2012 as document R2012-12175, in Du Page County, Illinois

Exhibit E
Zoning Map



**ERIKSSON
ENGINEERING
ASSOCIATES, LTD.**

Figure 1

EXHIBIT B – “YORKTOWN COMMONS PLANNED DEVELOPMENT DESIGN GUIDELINES”

Draft: 10/19/2015

Yorktown Commons

Planned Development Design Guidelines



Credits



KRE YTC Venture LLC

203 Yorktown Center
Lombard, IL 60148

Continuum Partners, LLC

1400 16th Street, Suite 320
Denver, CO 80202
303-573-0050

Torti Gallas + Partners, Inc.

1300 Spring Street, 4th Floor
Silver Spring, Maryland 20910
301-588-4800
www.tortigallas.com

Rosanova & Whitaker, Ltd.

30 West Jefferson Street, Suite 200
Naperville, IL 60540
630-355-4600

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Intent and Applicability

Intent

Applicability

Relationship to Zoning Ordinance

Design Guidelines

General Framework

Supersession of the Village Code

Applicability to Other Parts of the Village Code

Applicable Zoning

Building Code

Section I. – Intent and Applicability

A. Intent

The Yorktown Commons PD establishes the vision for the parcels located along the northern edge of the Yorktown Shopping Center. The vision is to establish a pedestrian friendly mixed-use neighborhood that acts as a gateway to the shopping center from the north.

The Yorktown Commons Design Guidelines have been developed and approved in order to ensure the implementation of the vision while also allowing land use flexibility.

B. Applicability

The Yorktown Commons Design Guidelines are the mandatory development standards approved as a part of the Yorktown Commons PD. All development within the Yorktown Commons PD boundaries is subject to the following:

- ▶ PD Ordinance Regulations
- ▶ Yorktown Commons Design Guidelines
- ▶ Yorktown Commons Architectural Review Board
- ▶ Standard Village Site Plan Approval process

The boundaries of the Property are generally illustrated in **FIGURE 01** and legally described in the ordinance approving the Yorktown Common Planned Development. The Property is made up of four parcels, labeled as Parcel 1 through Parcel 4.

The Yorktown Commons Design Guidelines regulate:

1. Permitted land uses
2. Site layout standards (such as build-to-lines, frontage occupancy, parking, block standards, and lot standards)
3. Site 3-dimensional standards (such as building heights,, special urban design features, and frontage types)
4. Street Sections
5. Open Space
6. Architectural Standards



FIGURE 01: Yorktown Commons PD Location

Section II. – Relationship to Zoning Ordinance and Subdivision Regulations

A. Design Guidelines General Framework

The Design Guidelines are comprised of a Regulating Plan and development standards contained in this document. A reduced version of the Regulating Plan is contained in these Guidelines and a full size version is contained in the PD application.

B. Supersession of the Village Code

The provisions of these guidelines supersede the development standards of the Village of Lombard Zoning Ordinance and Subdivision Ordinance. Where these guidelines are silent about any matter relating to land use, building, or development, applicable provisions of the Village Code that do not conflict with these guidelines shall apply.

Section III. – Applicability of Other Parts of the Village Code

A. Applicable Zoning

The Property is zoned B3PD. The Property has historically been governed by either the original Yorktown Mall Planned Development (Ordinance 1172) or the Yorktown Peripheral Planned Development (Ordinance 1964). The terms of this Yorktown Commons Planned Development shall hereafter govern. The terms of the Yorktown Commons Planned Development and these Design Guidelines shall supplant and prevail over, in their entirety, the underlying B3 (Community Shopping) District Regulations. The terms of the Yorktown Commons Planned Development shall prevail over other conflicting provision of the Village's Municipal Code, but other generally applicable provisions of the Municipal Code shall apply where not in direct conflict with the terms of the Planned Development.

B. Building Code

The provisions of the Yorktown Commons Planned Development are not intended, in any way, to modify or amend the provisions of the Building Code or other applicable life safety ordinances and laws. In any case where building or life safety regulations are in conflict with the Yorktown Commons Planned Development, the building or life safety regulations shall apply.

Administration

Preliminary Plan

Approval

Final Plan Approval

Interpretations

Severability

Major Changes

Minor Changes

Section IV. – Administration

A. Preliminary Plan Approval

The provisions of these Design Guidelines and associated documents approved pursuant to the Planned Development for Yorktown Commons shall comprise the “Preliminary Plan” for the Property pursuant to Section 155.503 of the Village Code. The Preliminary Plan shall provide the owner of the Property the right to develop the Property consistent with the requirements of the Preliminary Plan.

B. Final Plan Approval

The “Final Plan” for the Property shall be submitted and approved consistent with the requirements of Section 155.503(c)(2), except as modified herein. No separate site plan approval shall be required under Section 155.511 of the Village Code. Each Parcel may be submitted and shall thereafter be approved as a separate and distinct phase of the overall Yorktown Commons Planned Development. There shall be no time limitation for the submittal and approval of the Final Plan for each Parcel. The Director of Community Development shall determine if the Final Plan conforms substantially to the Preliminary Plan or if the Final Plan constitutes either a Major Change or a Minor Change to the Preliminary Plan. No public hearing shall be required if the Final Plan conforms substantially

to the Preliminary Plan. The Plan Commission shall provide its recommendation and the Village Board shall approve a Final Plan that substantially complies with the Preliminary Plan, subject to technical compliance with the requirements of Section 155.507(c). The Village Board may impose conditions upon its approval of the Final Plan, but provided that no condition shall be inconsistent with the rights established under the Preliminary Plan.

C. Interpretations

If uncertainty exists regarding the interpretation of terms, development standards, architectural standards, or any other regulation contained within these guidelines, the Community Development Director shall determine the intent of the provision at question.

D. Severability

If any section, subsection, sentence, clause, or phrase of this code or its application to any person or circumstance is held invalid by the decision of any court of competent jurisdiction, the remainder of this code, or the application of the provision to other persons or circumstances is in effect and shall remain in full force and effect.

E. Major Changes

Major changes shall be those changes which:

- 1) are inconsistent with the Vision set forth in Section 5 of these Design Guidelines;
- 2) seek approval of a use not specifically permitted or substantially similar to a use permitted in Section 6 of these Design Guidelines;
- 3) require a greater than ten percent (10%) modification of the standards set forth in Sections 7, 8 and 9 of these Design Guidelines, except that the maximum Building Height shall not increase;
- or 4) require a deviation or variance from an applicable provision of the Municipal Code.

Major Changes shall require a public hearing before the Plan Commission and ordinance approval by the Village Board pursuant to the following standards:

1. That the establishment, maintenance, or operation of the Major Change will not be detrimental to, or endanger the public health, safety, morals, comfort, or general welfare;
2. That the Major Change will not be injurious to the uses and enjoyment of other property in the immediate vicinity for the purposes already permitted, nor substantially diminish and impair property values within the neighborhood in which it is to be located;
3. That the establishment of the Major Change will not impede the normal and orderly development and improvement of the surrounding property for uses permitted in the Yorktown Commons Planned Development;

4. That adequate public utilities, access roads, drainage and/or necessary facilities have been or will be provided;
5. That adequate measures have been or will be taken to provide ingress and egress so designed as to minimize traffic congestion in the public streets;
6. That the proposed Major Change is not contrary to the objectives of the Yorktown Commons Planned Development; and
7. That the Major Change shall, in all other respects, conform to the applicable regulations of the district in which it is located, except as such regulations may, in each instance, be modified pursuant to the recommendations of the Plan Commission.

F. Minor Changes

Minor Changes are changes which are not Major Changes. A Minor Change shall not require a new public hearing and may be approved by the Director of Community Development after receiving comments and recommendations from the Inter-Departmental Review Committee. The Director of Community Development may elect to bring any Minor Change to the Village Board for formalization of the approval by ordinance.



Description of the Project

Section V. – Vision

A. Description of the Project

Today's communities and markets are seeking walkable, sustainable, and healthy communities that provide a diverse mix of uses and a strong identity. Yorktown Center has a vision to reposition itself from an automobile dominated landscape to one that includes a mix of uses and a strong residential component – A neighborhood where one can live, walk along tree-lined streets, spend time in a neighborhood square, and even walk to get a cup of coffee. This new neighborhood extends the existing residential to the north, builds a unique identity with a strong appeal to the non-traditional suburban buyer or renter, while connecting to, and supporting, the economic well-being of the mall. The key elements of the plan are:

1. A new entry from the north along Grace Street with residential buildings lining the street, sidewalks, and street trees.
2. Yorktown Ring Road is transformed into a tree-lined residential street with new plaza at its heart.
3. A new public urban space at the intersection of Yorktown Ring Road and Grace Street with placemaking elements, such as retail storefronts, a small plaza, and seating.
4. A street-activated crossroads at the intersection of Yorktown Ring Road and Grace Streets linking the new mixed-use buildings with the Shopping Center.

5. New residential uses with high quality architecture that contribute to a desirable neighborhood.
6. New Neighborhood Open Space that provides opportunities for walking, passive recreation, and seating.

The illustrative site plans included here are intended to show a few of the many possible development scenarios for Yorktown Commons. Consistent with the vision of creating a high-quality walkable realm, they illustrate site plans and designs which might be built according to the Design Guidelines. They should not, however, be interpreted as representing a specific required site plan or design.

The Regulating Plan and standards create a framework that will ensure implementation of the vision, but will also allow for flexibility in the development layout and program. On the following three pages possible alternative development scenarios are illustrated. Other configurations are possible and permitted so long as they adhere to the standards of the Guidelines.



Aerial view of Yorktown Commons looking South

B. Sustainability

Yorktown Commons provides the following elements to foster environmental sustainability within a neighborhood design context. The sustainable features of the project generally fall into two categories:

1. Smart Location

- i. Building on previously-developed sites and infill locations uses land efficiently and preserves open space, ecological areas, and agricultural land around cities. It also utilizes existing infrastructure and clusters housing, jobs, stores, and public spaces together. By putting these conveniences within easy reach, public transit, biking, and walking are more feasible and the length of car trips are reduced.
- ii. The project is located in a way that is sensitive to its natural setting and protects local environmental quality.

2. Neighborhood Pattern and Design

- i. Streets designed for walkability include buildings that face the sidewalk and have architectural diversity along their path. Continuous sidewalks, low-speed traffic, and on-street parking provides a buffer between the sidewalk and the street to increase pedestrian comfort and safety. Walking has cross-cutting benefits for public health, environmental sustainability, and neighborhood vitality, and

further unlocks the advantages of neighborhoods with smart locations, a mix of uses, and compact development.

- ii. Street trees make pedestrians more comfortable by providing shade and contact with nature. Native and drought-resistant species should be favored. Monocultures should be avoided in order to provide ecological diversity and resistance to diseases.
- iii. The proposed mix of residential and commercial land uses has many benefits. The sustainable and diverse uses of a blended neighborhood tend to support each other and reinforce a sense of neighborhood character, while decreasing the need to travel long distances for goods, services, or work. This reduces car trips and facilitates walking, which contributes to health and fitness.
- iv. Connections for pedestrians, cyclists, and vehicles – both within the neighborhood and to surrounding areas – are essential. Bike parking near transit stops is encouraged. Connections to existing public transportation are safe and pedestrian-friendly.
- v. Local housing near public transit increases the likelihood that people will take transit or walk rather than drive. It offers an alternative to automobile use and captures many of the other benefits of infill development.



View looking West down Yorktown Ring Road

Vision

Illustrative Scenario 1

This scenario illustrates multi-family in the north-east quadrant and town houses in the north-west quadrant. Additional streets would be added in this scenario that have sidewalks and streets trees where homes front the street and parking is relegated to the back to create a pedestrian friendly environment. A common green is dedicated as a recreational amenity for townhouse residents.



Illustrative Scenario 2

This scenario illustrates multi-family in both the north-east quadrant and north-west quadrant. In this scenario the linear green space to the east of Grace Street continues west of Grace Street to a green space leading to Yorktown Ring Road creating a pedestrian loop that alternates between urban streetscape, courtyards, and linear greens.



Vision

Illustrative Scenario 3

This scenario illustrates multifamily in the north-west quadrant and townhouses in the north-east quadrant. From the north-west pedestrian loop and Grace Street, a formal linear green space leads you into the north-east quadrant, terminated by a garden structure. The open space changes character into a more informal, natural landscape and connects back out to the street on the east. Additional interior streets and alleys allow the buildings to form shape the open space into a beautiful, pedestrian-friendly realm.





View of Plaza at corner of Yorktown Ring Road and Grace Street

Land Use

Permitted Uses

Required Uses

Accessory Uses

Density

Home Occupations

Section VI. – Land Use

A. Permitted Uses

The underlying zoning for Yorktown Commons is B3 Community Shopping District. The permitted uses for Yorktown Commons shall be:

1. All Permitted uses in the B3 Community Shopping District
2. Residential, including the following types:
 - i. Single-Family
 - ii. Two-Family
 - iii. Townhouse / Attached Single-Family
 - iv. Stacked Townhouse
 - v. Multi-Family (fee simple, condominium, and/or rental)
3. Hotel
4. Daycare as an accessory use.
5. Mixed-Use buildings that incorporate any of the uses noted in this Land Use section shall be permitted.

B. Required Uses

1. Retail shall be required along building faces where shown on the Regulating Plan and as illustrated in **FIGURE 02**.
2. Street-activating uses – such as retail, leasing offices, club rooms, fitness rooms, lobbies, and the like – are encouraged on the ground floor of multifamily buildings, if present, where illustrated on **FIGURE 02**.

C. Accessory Uses

1. Accessory Uses shall comply with all development standards of the Yorktown Commons Guidelines

2. Accessory Uses, Buildings, or Structures shall not be permitted on a lot prior to the erection of the primary building
3. The total square foot area of the Accessory Use shall be less than the main floor area of the primary use.

D. Density

1. The maximum number of Residential dwelling units shall be 970.
2. The maximum Retail use shall be 28,000 SF.
3. The maximum of other Commercial uses shall be 100,000 SF.
4. The Hotel maximum density is 150 rooms.

E. Home Occupations

Home Occupations shall be permitted according to § 155.211 of the Village Code of Ordinances.

Legend

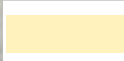


	All Permitted Uses
	Retail Storefront Required
	Street Activating Uses Encouraged



FIGURE 02: Land Use 17

Regulating Plan

Establishment

Public and Private

Street Alignments

Section VII. – Regulating Plan

A. Establishment

1. The Regulating Plan establishes the framework for the neighborhood by establishing:
 - i. Public and private street alignments
 - ii. Neighborhood Open Space
 - iii. Build-to-Lines
 - iv. Frontage Occupancy
 - v. Special Urban Design Features
 - vi. Public and Private Front Yard Types
 - vii. Building Height Requirements
 - viii. Required Retail Storefront Locations
2. The Regulating Plan is shown in **FIGURE 03**. For ease of use, each Regulating Plan element is described and illustrated in the following pages.
3. The lines and boundaries depicted in the Regulating Plan are based on, and dimensioned from, the street Right-of-Way, Property Line, or Reference Line, as described on the following pages.

B. Public and Private Street Alignments

1. Public and Private Street alignments and locations shall be consistent with the Regulating Plan and additional streets or drives shall be consistent with the standards contained in these guidelines.

Legend

---	Property Line
XX'	Build-to-Line
←X%-X%→	Frontage Occupancy
X Max	Maximum Building Height
Type	Frontyard Type
	Required Retail Frontage
	Prominent Corner Element
	Massing and Articulation Rules Apply
	Facade Standards Apply

	Open Space Required at Location Shown
	Open Space Recommended at Location Shown
	Open Space Required Somewhere in Block

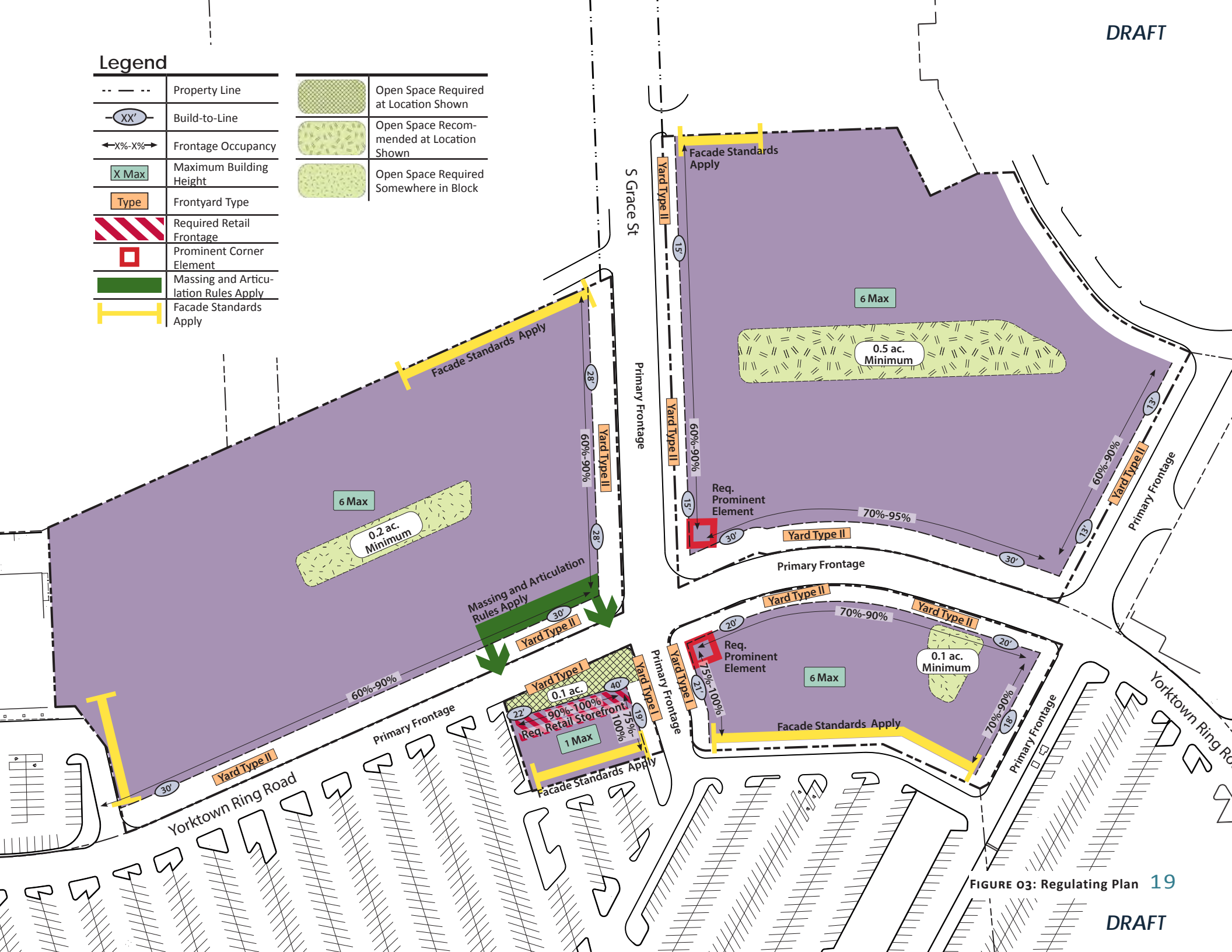


FIGURE 03: Regulating Plan 19

Regulating Plan

Open Space

Neighborhood Open Space

C. Open Space

1. The required minimum Open Space shall be 12.5% of the total site area.
2. Open Space shall be defined as land or water that is open to the air and used for active or passive recreation, including:
 - i. Lawn;
 - ii. Garden;
 - iii. Courtyard;
 - iv. Square;
 - v. Plaza;
 - vi. Park;
 - vii. Linear Green;
 - viii. Trails and Pedestrian Paths;
 - ix. Water Features and Landscaped Stormwater Management Features;
 - x. Garden Structures, or similar.
3. Open Space shall not include private yards.
4. Usable Open Space provided on a roof or a podium may be counted.
5. Open Space may be retained as part of a Parcel where ownership is not divided, but shall be conveyed to an owners' association where ownership of a Parcel will be divided. In lieu of dedication to an owners association, the developer may convey Open Space to a public entity with the consent of the Village.
6. There shall be no separate requirements for Common Open Space as defined in the Village Code and the provisions of Sections 155.509 and 155.510 shall not be applicable to the Yorktown Commons Planned Development.

D. Neighborhood Open Space

1. In accordance with the intent and vision of the Yorktown Commons Planned Development, the creation of specific Neighborhood Open Spaces shall be required.
2. Neighborhood Open Space is shown on the Regulating Plan and in **FIGURE 04** in three categories: Neighborhood open spaces are in three categories:
 - i. In the required size and location as shown in the Regulating Plan;
 - ii. In the required minimum size and recommended location shown in the Regulating Plan; and,
 - iii. In the required minimum size but not specifically located in the Regulating Plan.
3. Additional Open Space located in the interior of a block and behind buildings with no frontage on a thoroughfare shall be permitted.
4. Additional Open Space shall not occur at block corners.

Legend




	Space Required at Location Shown
	Minimum Size Required. Recommended at Location Shown
	Minimum Size Required. Location within Block Not Specified



FIGURE 04: Open Space 21

Regulating Plan

Build-to-Line

E. Build-to-Line

1. Buildings and their Primary Facades shall be located along the Build-to-Line for all stories of a building (see **FIGURE 05**).
2. In order to provide for façade articulation, building facades may step back a maximum of 2' from the Build-to-Line for a maximum of 70% of the facade and may step forward a maximum of 1' beyond the Build-to-Line for a maximum of 25% of the facade along a block face.
3. Where site geometry creates corners with acute angles, buildings may chamfer or pull back from the corner to create reasonable footprints.
4. Accessory Uses, Accessory Buildings, and Accessory Structures on corner lots.
 - i. Detached garages shall be built at the Built-to-Line on the secondary street
 - ii. Privacy fences shall built at the Build-to-Line along the secondary street.
 - iii. Except for approved projections, all other accessory uses, buildings, and structures shall be built a minimum of two (2) feet back from the Build-to-Line.
5. If a Build-to-Line encroaches into a Clear Sight Area, then Build-to-Line along each block face shall start at the edge of any Clear Sight Area.
6. Projections beyond the Build-to-Line
 - i. Porches, stoops, balconies, second-story bay windows, towers, arcades, awnings, marquees, and signs may encroach beyond the Build-to-Line, within the private front yard, to a maximum dimension of ten (10) feet (see **FIGURE 06**).
 - ii. In Commercial front yard types, at-grade projections with raised platform shall not be permitted to encroach beyond the Build-to-Line (see **FIGURE 07**).
 - iii. The minimum vertical clearance of projections shall not be less than 8'-6" above grade over a walkway, or less than 15' above grade over a driveway, interior access drive, or alley.
7. For Yorktown Commons Parcel 2, the Build-to-Line along the Yorktown Ring Road shall be measured from the Reference Line as shown and described on Subdivision Plat #.

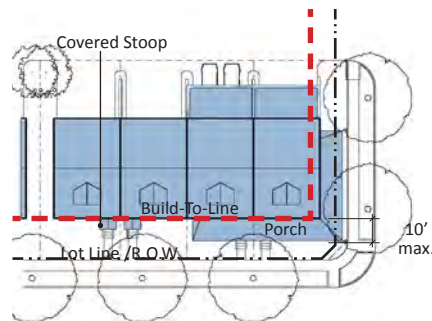


FIGURE 06: Build-To-Line, Residential Building

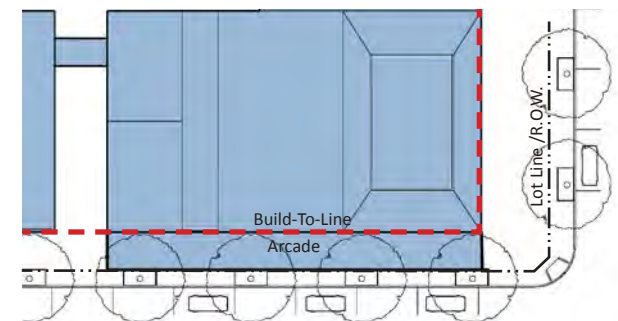


FIGURE 07: Build-To-Line, Mixed-use Building

Legend


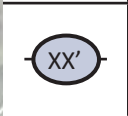
	Build-to-Line
	Dimension from ROW / Property Line to the Build- to-Line



FIGURE 05: Build-To Line 23

Regulating Plan

Special Conditions for Single-family, Two-family, and Townhouse Buildings

8. Special Conditions for Single-family and Two-family Dwellings
 - i. On the concave side of a curving street, buildings—or groups of attached buildings—shall meet the Build-to Line at their corners (**FIGURE 08**).
 - ii. On the convex side of the a curving street, buildings—or groups of attached buildings—shall meet the Build-to Line at the center of the facade (**FIGURE 09**).
 - iii. On corners not equal to 90 degrees, buildings shall have their primary facade on the Build-to Line and intersect the secondary Build-to Line at the corner (**FIGURES 10 AND 11**).
 - iv. In all cases listed above, the length of facade projected out to the Build-to-Line shall be measured for purposes of Frontage Occupancy requirements.

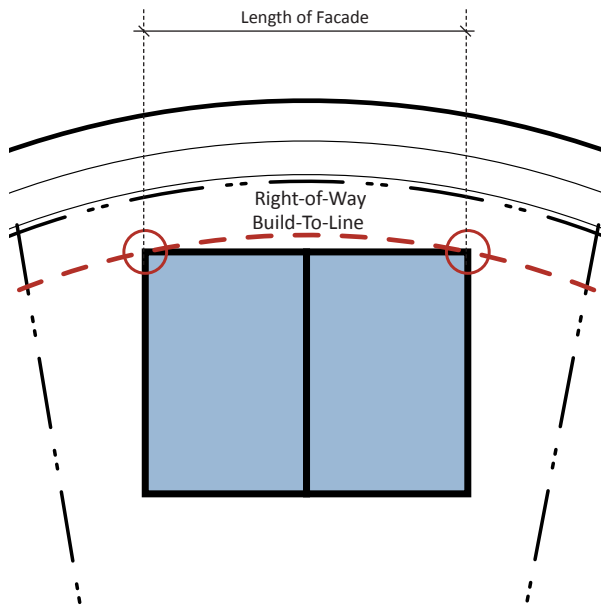


FIGURE 08: Build-to-Line on Curved Street, Concave

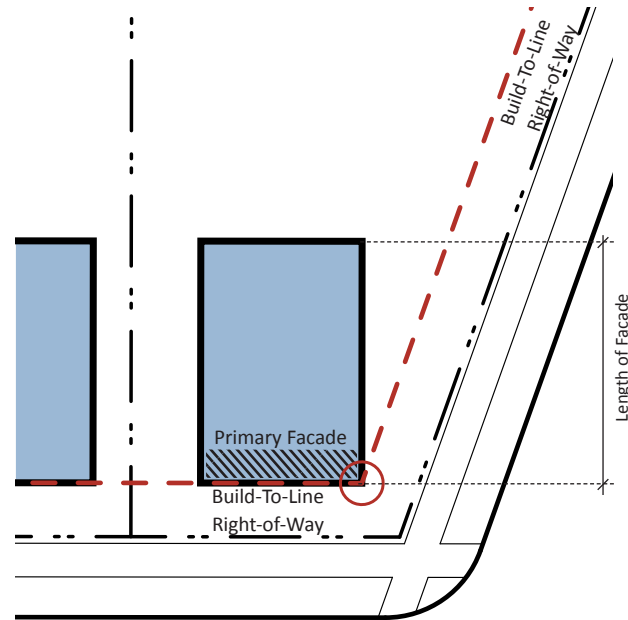


FIGURE 10: Build-to-Line on Corners Greater than 90°

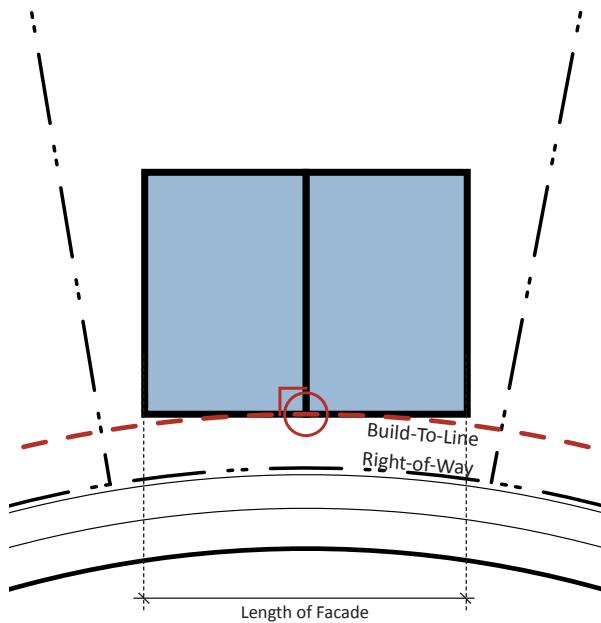


FIGURE 09: Build-to-Line on Curved Street, Convex

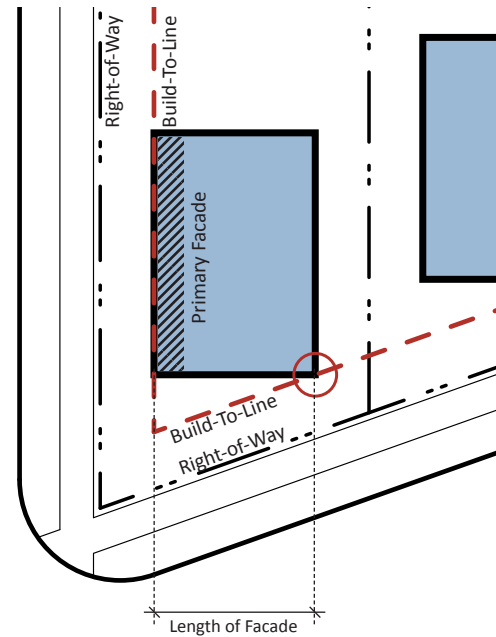


FIGURE 11: Build-to-Line on Corners Less than 90°

Regulating Plan

Frontage Occupancy

F. Frontage Occupancy

1. Frontage occupancy is the minimum length of Primary Façade that shall be built along a Build-to-Line (see **FIGURE 12**).
2. Frontage occupancy requirements shall apply to all stories of a building (excluding occupied or unoccupied space in roofs).
3. Buildings and Primary Facades shall be located at block corners. The permitted void (what is left after subtracting the minimum frontage occupancy from the length of the block face) shall not be located within 20' of a block corner.
4. Buildings located at block corners shall have two Primary Facades, one facing each public or private street (see **FIGURE 13**).
5. Detached garages shall count toward secondary frontage occupancy requirements when located on corner lots.
6. Buildings that face a public or private street with a courtyard above the first floor that is no more than 70' wide shall be permitted to count the courtyard width along the Build-to-Line as occupied frontage.
7. Multifamily buildings that face a public or private street with a courtyard no more than 70' wide shall be permitted to count the courtyard width along the Build-to-Line as occupied frontage.
8. Buildings that form a courtyard facing a public or private street on the first floor are prohibited along retail Front Yards.
9. Single-family detached and two-family building frontage occupancy shall be regulated by the lot standards instead of the Regulating Plan.
10. Lots which do not occupy an entire block face shall meet the minimum frontage occupancy requirements within the individual lot.
11. Where retail is required in the Regulating Plan, buildings shall be designed to accommodate retail uses on the ground floor. However, other uses may occupy the retail space, provided that the changes made to accommodate such other uses shall in no way prevent a transition to retail use.
12. Mixed-use, Multifamily, and Townhouse Buildings facing curving streets may be faceted to follow the curve along the Build-to-Line. Facades located within 9' of the Build-to-Line shall be counted as occupied Frontage.

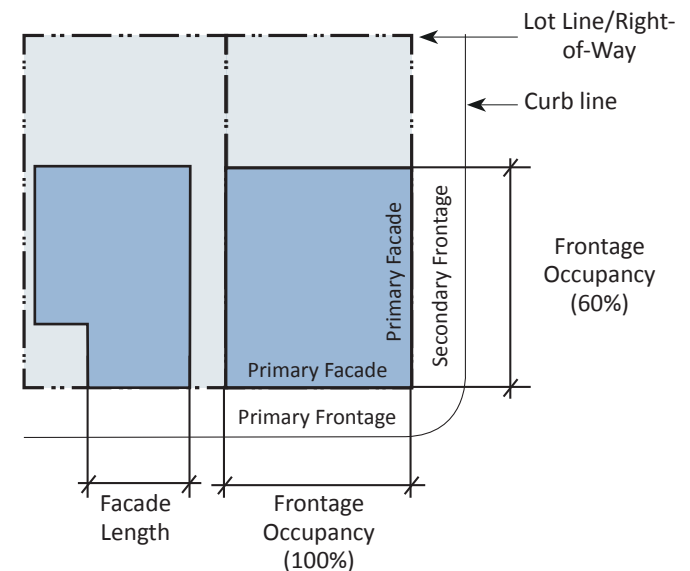


FIGURE 13: Example of Frontage Occupancy at Block Corners

Legend

---	Build-to-Line
← X%-X% →	Occupied Percentage of Built-to-Line (Range)

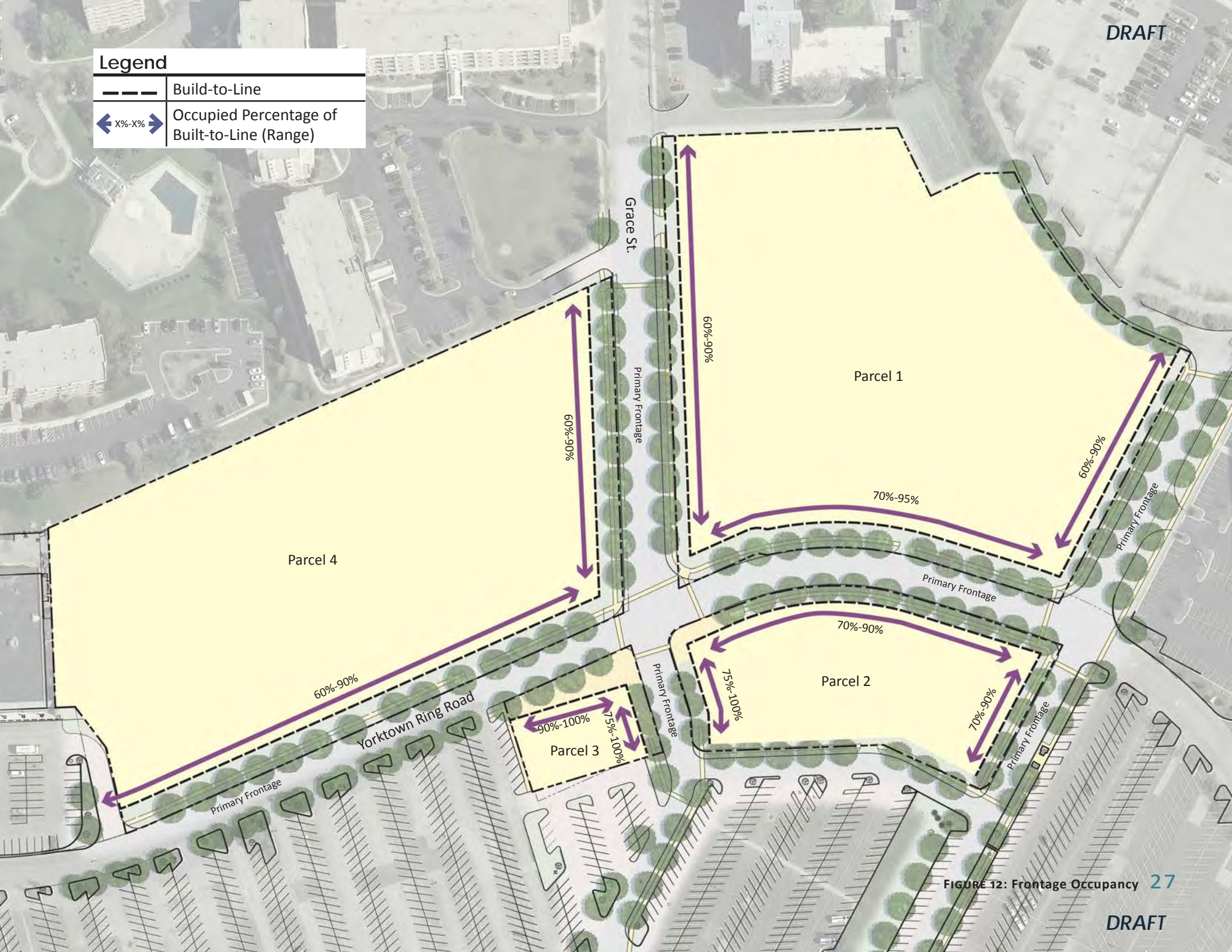


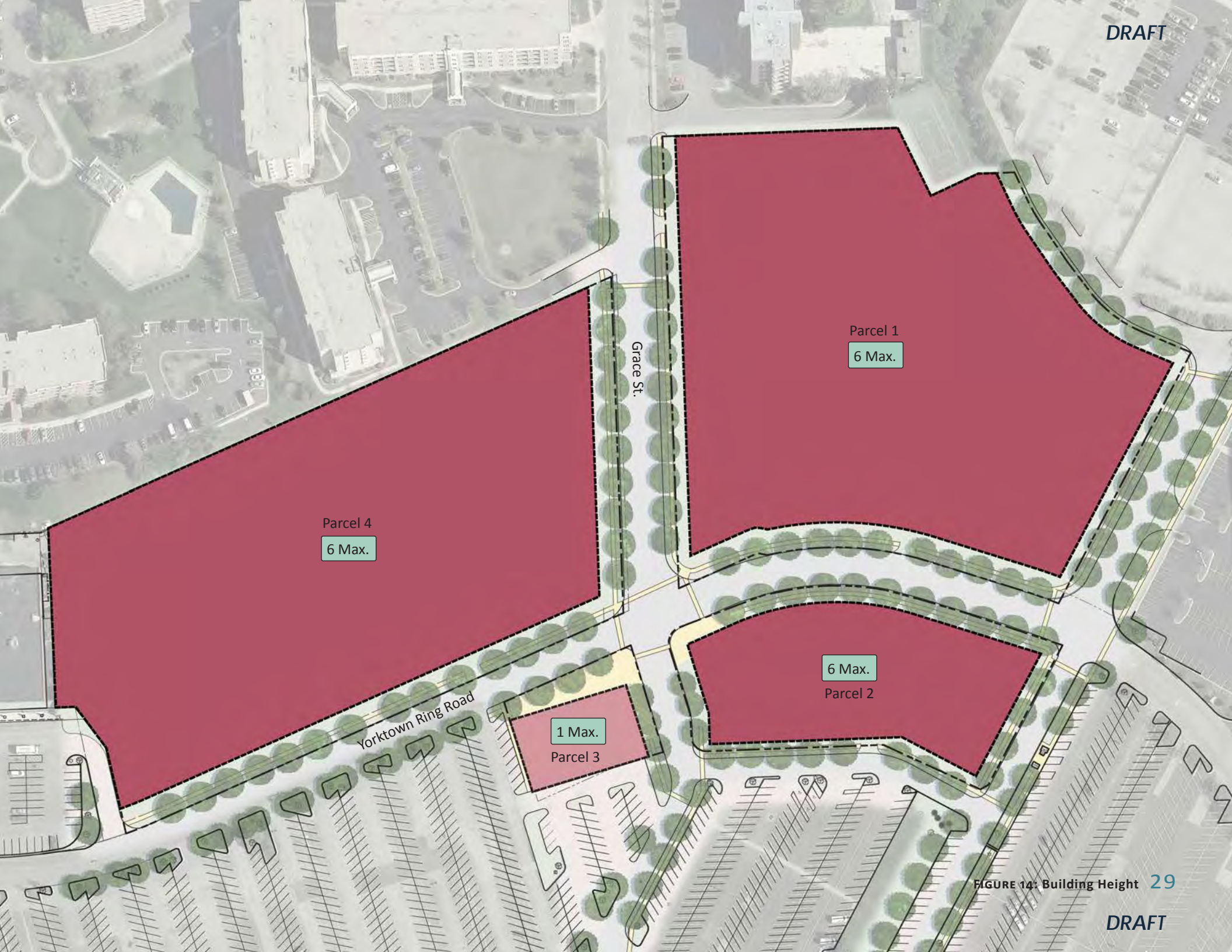
FIGURE 12: Frontage Occupancy 27

Regulating Plan

Building Height

G. Building Height

1. Maximum permitted building heights, in number of stories, shown in **FIGURE 14**.
2. Stories permitted are in addition to a basement, if present.
3. The maximum height of a story on the first floor, as measured at the horizontal midpoint of the facade, shall not exceed thirty (30) feet for non-residential or mixed-use buildings, or twenty-five (25) feet for residential buildings. All other floors shall not exceed fourteen (14) feet in height.
4. Maximum building height shall not exceed one hundred (100) feet.



Parcel 1

6 Max.

Parcel 4

6 Max.

6 Max.

Parcel 2

1 Max.

Parcel 3

FIGURE 14: Building Height 29

Regulating Plan

Special Urban Design and Architectural Features

H. Special Urban Design and Architectural Features

1. **FIGURE 15** represents the Urban Design and Architectural features that shall be required.
2. Vista Terminations:
When lots or buildings are situated such that the centerline of a street or open space intersects with said lot or building, the building facade shall be located and designed as a vista termination, i.e., designed to visually attract a viewer's attention at the end of a visual axis and designed to terminate a view with a significant architectural feature such as a tower, corner tower, symmetrical facade centered on a visual axis, an architecturally embellished entry, or similar distinctive architectural devices.
3. Building design shall incorporate special urban design features as noted in the Regulating Plan. Special Urban Design Features include:
 - i. Prominent Corner Element (**FIGURE 16**).
 - ii. Symmetrical Façade on Axis of Street (**FIGURE 17**).
 - iii. Prominent Building Element on Axis (see **FIGURE 18**).
 - iv. Building Massing and Articulation Correlates to Public Space (**FIGURE 19**).
 - v. Secondary Building Facades design to Primary Facade Standards (doors not required).

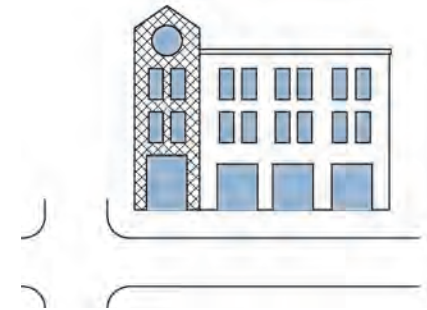


FIGURE 16: Example of Prominent Corner Element

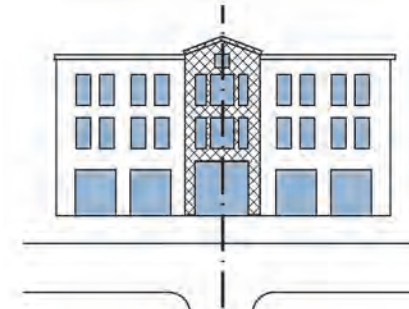


FIGURE 17: Symmetrical Façade Element on Axis



FIGURE 18: Prominent Building Element on Axis

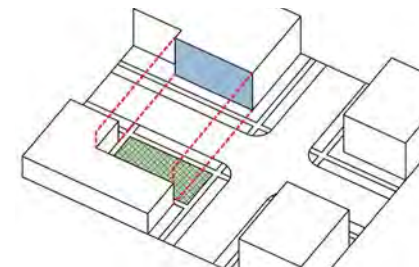


FIGURE 19: Building Massing and Articulation Correlates to Public Space

Legend





	Prominent Corner Element
	Axis of Public Space
	Massing and Articulation Must Correlate to Public Space
	Secondary Facades designed to Primary Facade Standards



FIGURE 15: Architectural Features 31

Regulating Plan

Public and Private Front Yards

I. Public Front Yards and Private Front Yards

1. A Public Front Yard is the space that occurs between the street curb and the right-of-way line (i.e., the front lot line).
2. A Private Front Yard is the space that occurs between the public right-of-way (i.e. the front lot line) and the primary building façade.
3. Public Front Yard and Private Front Yard types shall be applied as designated in the Regulating Plan for required streets.
4. Public front yard and Private front yard types for additional streets shall be selected from the types listed below.
5. Front Yard types are established as:
 - i. Tree Pit and Hardscape
 - ii. Tree Lawn and Hedge
 - iii. Tree Lawn and Stoop
6. The description of Front Yard Types are as shown in **FIGURES 20 – 22**.
7. The location of require Front Yard Types are shown on **FIGURE 23**.
8. Hardscape front yards shall be implemented with a consistent paving material from curb to building face appearing as a single sidewalk.
9. Front yard element minimum dimensions:
 - i. Sidewalk – 5' min.
 - ii. Tree Lawn – 5' min.
 - iii. Sidewalk contiguous with curb – 6'
 - iv. Tree Pit – 4' min. width and length
 - v. Hedge – 24" spacing at time of planting with a maintained height of 36" and maintained as a solid screen and not pruned as individual plants.

FRONT Yard Type I. – Tree Pit and Hardscape

The Tree Pit and Hardscape Front Yard Type shall provide:

- a. A hardscape surface from curb to building face designed to appear as a single sidewalk.
- b. A street tree pit with a minimum size of 4' long and 4' wide.
- c. Street trees shall be planted at a spacing of every 35' on each side of the street

FRONT Yard Type II. – Planting Strip and Hedge

The Tree Lawn and Hedge Front Yard Type shall provide:

- a. A 5' minimum tree lawn, immediately adjacent to the curb
- b. A 5' minimum sidewalk
- c. A hedge, immediately adjacent to the front lot line, with 24" spacing at time of planting with a maintained height of 36" and maintained as a solid screen and not pruned as individual plants.
- d. Street trees shall be planted at a spacing of 40' on each side of the street.

FRONT Yard Type III. – Tree Lawn and Stoop

The Tree Lawn and Lawn Front Yard Type shall provide:

- a. A 5' minimum tree lawn, immediately adjacent to the curb.
- b. a 5' minimum sidewalk
- c. Street trees shall be planted at a spacing of 40' on each side of the street

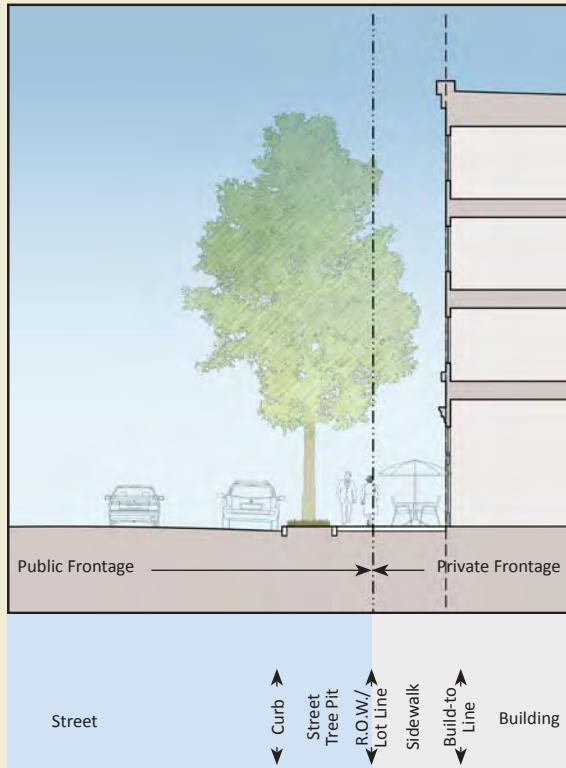


FIGURE 20: Front Yard Type I. – Tree Pit and Hardscape

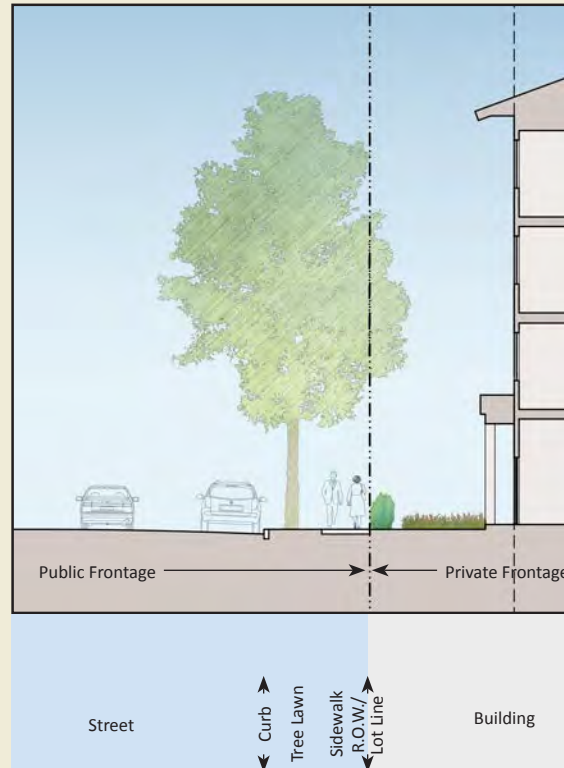


FIGURE 21: Front Yard Type II. – Planting Strip and Hedge

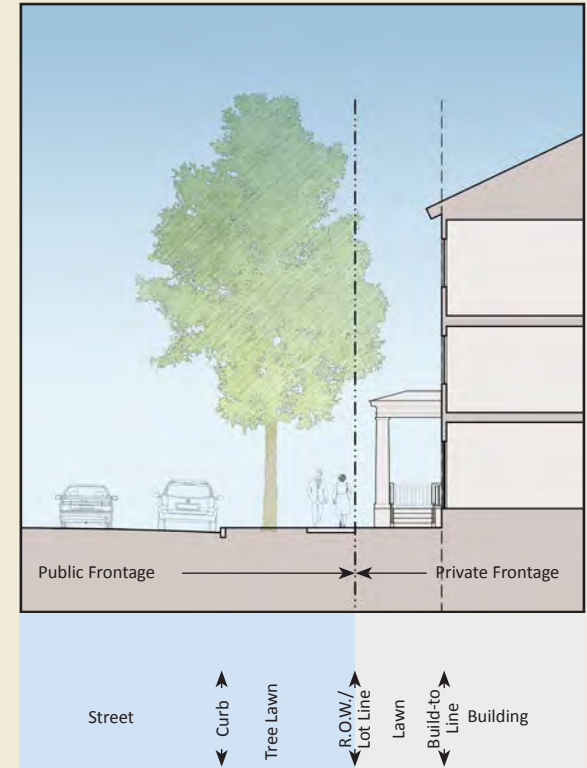


FIGURE 22: Front Yard Type III. – Tree Lawn and Stoop





Regulating Plan

Public and Private

Front Yards

10. Outdoor dining areas, including temporary tables, chairs, and umbrellas, shall be permitted. Outdoor Dining areas may encroach into the public right-of-way. The following regulations shall also apply:
 - i. Outdoor dining areas shall be permitted only along the business' tenant bay or storefront facade; and,
 - ii. Shall not block an entrance or exit to or from the business or building.
11. Outdoor display or sales of merchandise shall be permitted provided a minimum usable sidewalk width of four (4) feet is maintained. Outdoor display or sales of merchandise may encroach into the public right-of-way. The following regulations shall also apply:
 - i. Outdoor display or sales of merchandise shall be permitted only along the business' tenant bay or storefront facade;
 - ii. Shall not block an entrance or exit to or from the business or building;
 - iii. Shall not exceed 10 percent (10%) of the gross floor area of each non-related and separately operated use; and,
 - iv. Shall be permitted only during the hours of operation of the business and shall be removed at the close of each business day.
12. Temporary commercial signs (i.e. sandwich board, etc.) shall be permitted during operating hours provided they comply with the Lombard Sign Ordinance.

Legend

	Front Yard Type I: Tree Pit and Hardscape
	Front Yard Type II: Planting Strip and Hedge
	Front Yard Type I or II
	Front Yard Type III: Tree Lawn and Stoop*

**To be determined at time of site plan development.*



FIGURE 23: Front Yard Types 35

Regulating Plan

Street Sections

J. Street Sections

To facilitate bicycle and pedestrian connections to Yorktown Commons and Yorktown Center, Grace Street—from E 22nd Street in the North to the Yorktown Shopping Center Road in the south—shall be reconfigured to include on-street parking and bike lanes. The typical condition is illustrated in **FIGURE 24**; intersection designs at E 22nd Street and Yorktown Shopping Center Road will vary.

In the case of parcel being developed with a Single-family, Two-family, or Townhouse program, all units shall be arranged facing either a street (public or private) or an open space, and shall be accessed from an alley or rear drive. New streets shall follow the standards listed in this section as appropriate to the particular layout.

The sections for new interior streets are:

1. Two-way Traffic, Park One Side (**FIGURE 25**)
2. Two-way Traffic, Park Two Sides (**FIGURE 26**)
3. Alley (**FIGURE 27**)
4. Center Green, Park One Side (**FIGURE 28**)

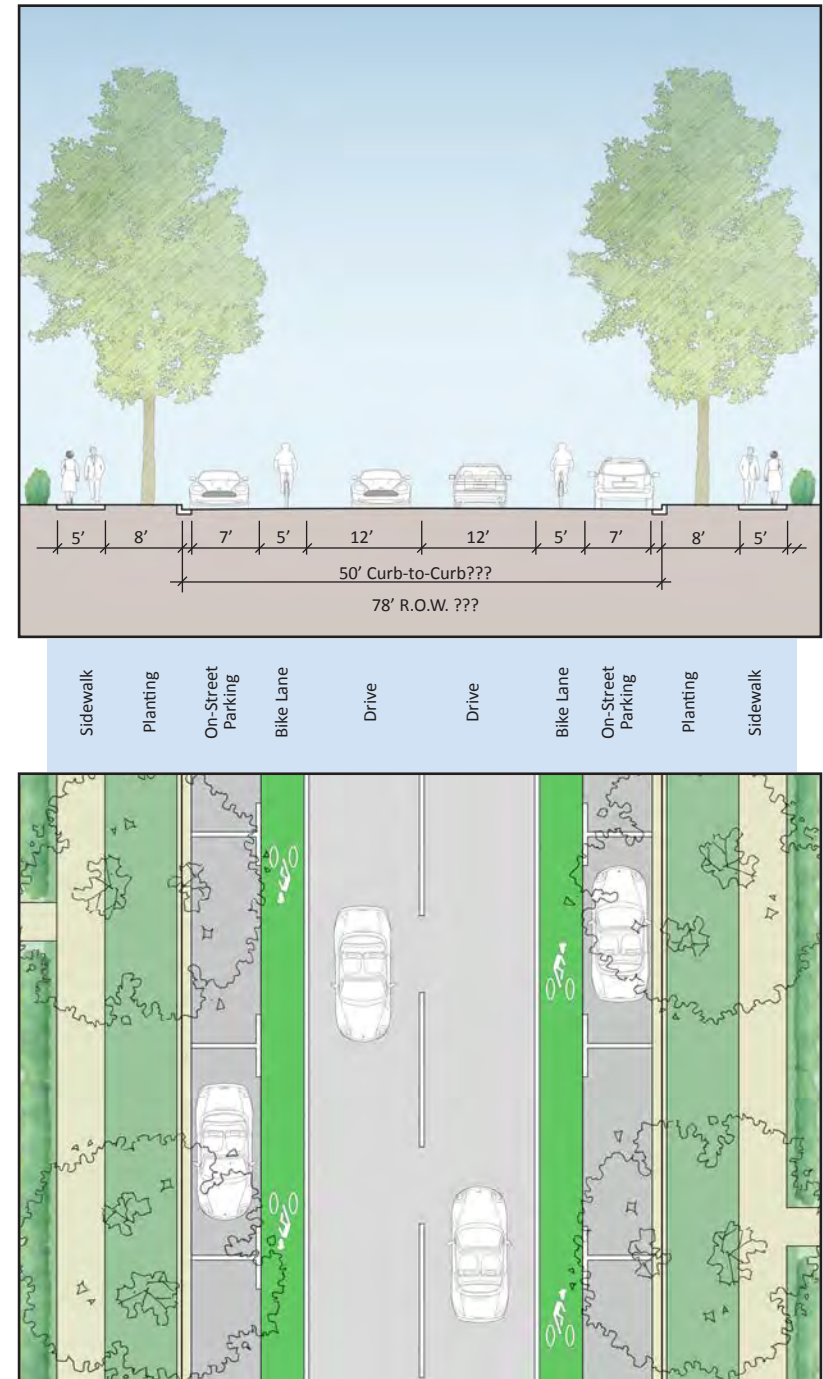


FIGURE 24: Street Sections – Illustrative Typical Section of Grace Street

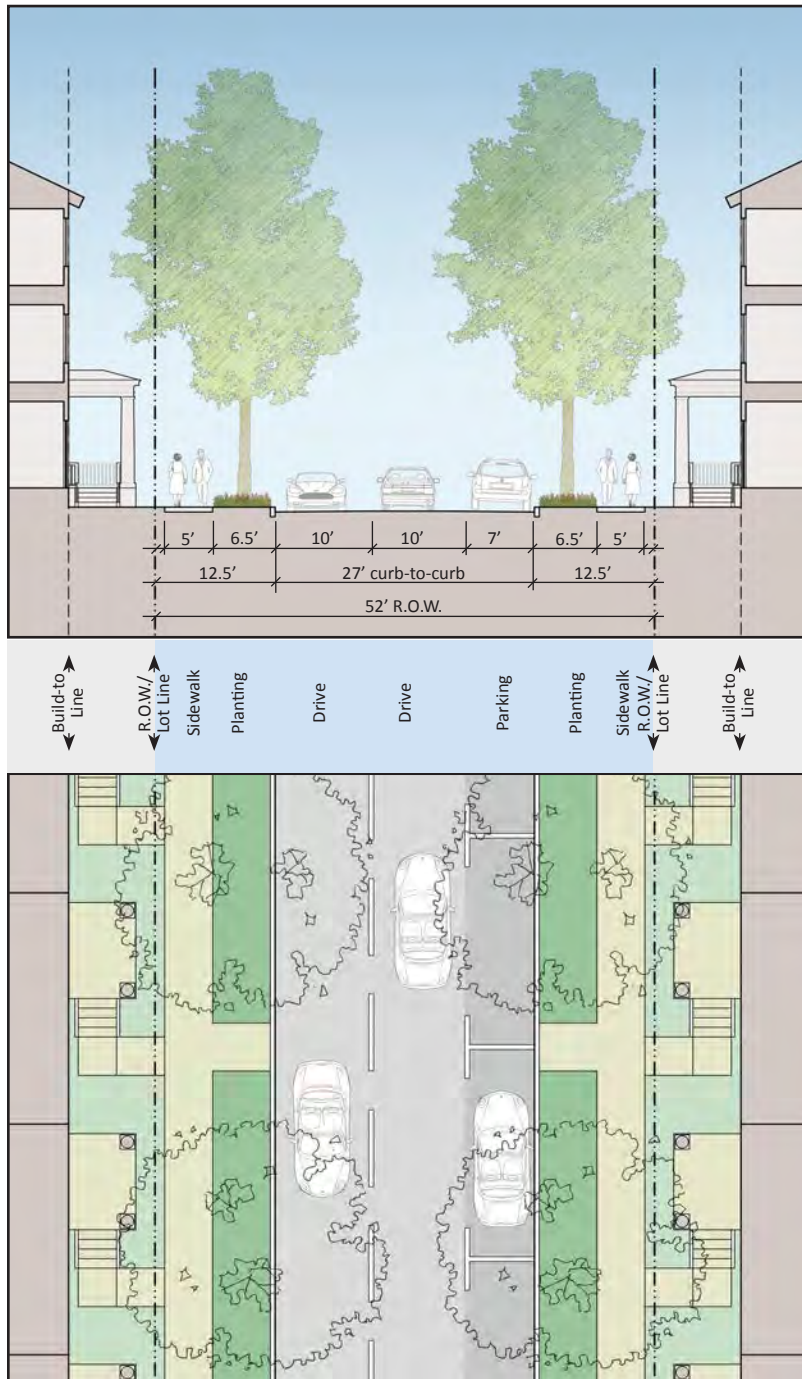


FIGURE 25: Street Sections – Park One Side

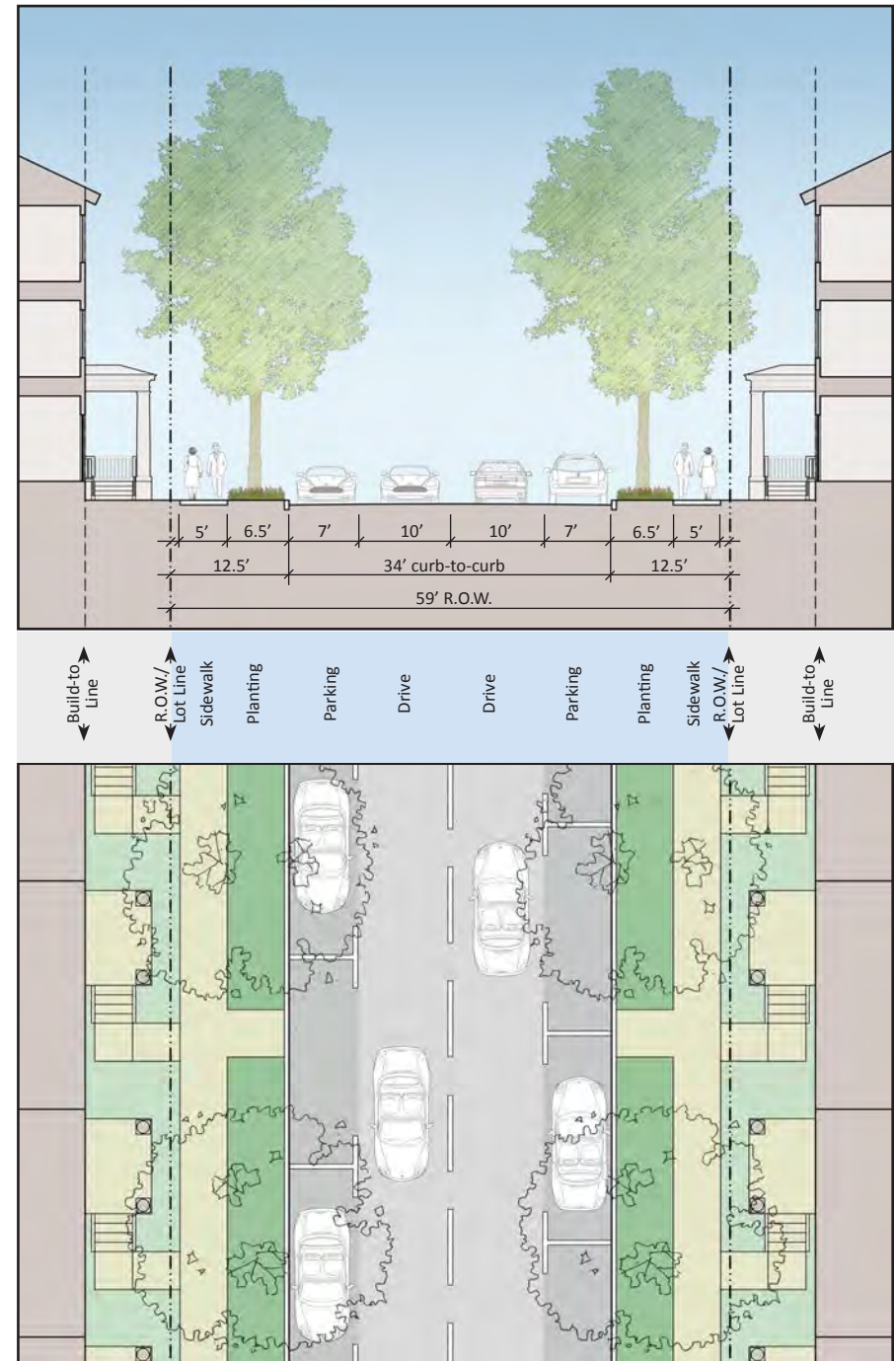


FIGURE 26: Street Sections – Park Two Sides

Regulating Plan

Street Sections

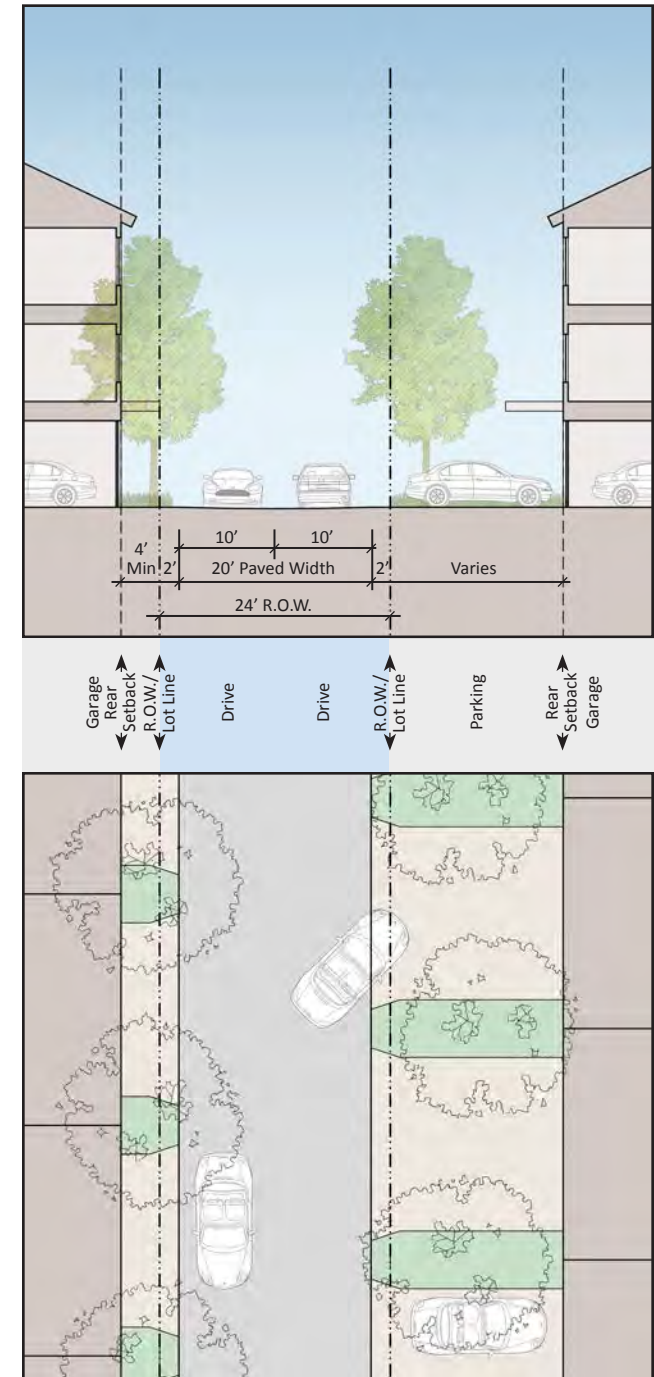


FIGURE 27: Street Sections - Alley

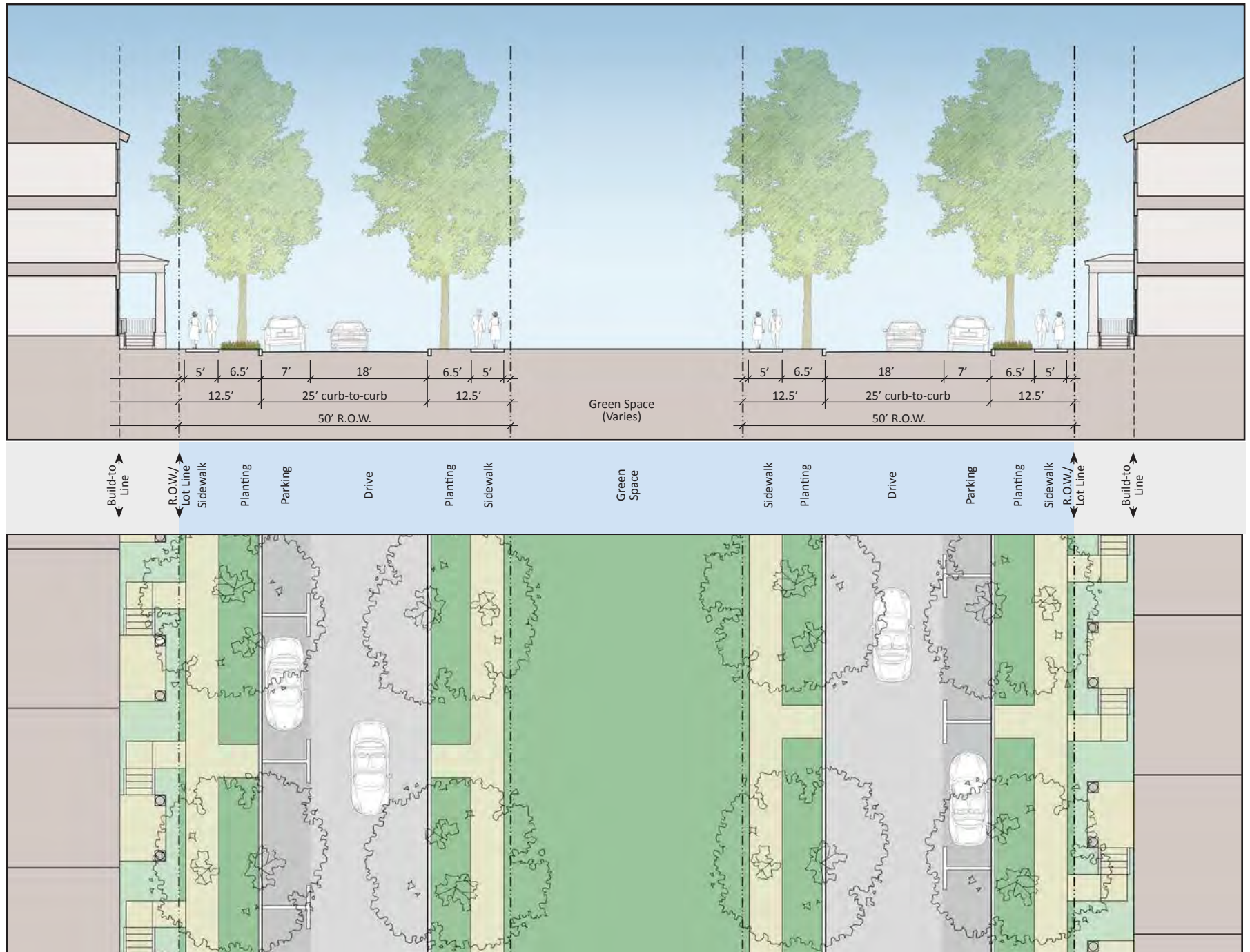


FIGURE 28: Street Sections – One-Way Pair

Development Standards

Block Configuration

Lot Standards

Section VIII. – Development Standards

A. Block Configuration

1. Blocks shall be configured as shown in the Regulating Plan.
2. Additional streets may be added to the interiors of the blocks shown on the Regulating Plan.
3. Blocks shall meet the following standards:
 - i. The maximum length of a block face shall be 650' (see **FIGURE 29**).
 - ii. The maximum perimeter of any block shall be 2,500', provided, however, the maximum perimeter length of a single-family only, two-family only, or townhouse only block (or combination) shall be 1,520' (see **FIGURE 30**).
 - iii. Blocks may be defined by either public streets or private streets.
 - iv. Block perimeter lengths shall be measured between: (i) intersecting public street right-of-way lines; or, (ii) the boundary width of where a private right-of-way line, easement, or edge of pavement would be for private streets/drives.
 - v. When a block backs up to an adjacent existing parcel where no street interrupts its length, there shall be no maximum block length parallel to the adjacent existing parcel and there shall be no maximum perimeter length (see **FIGURE 31**).
4. Cul-de-sac streets shall be prohibited. Turnarounds for service drives and alleys are permitted.

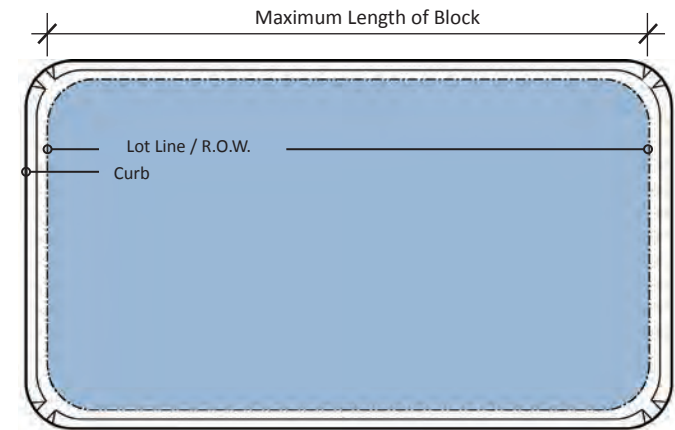


FIGURE 29: Maximum Length of a Block.



FIGURE 30: Maximum Perimeter of a Block.

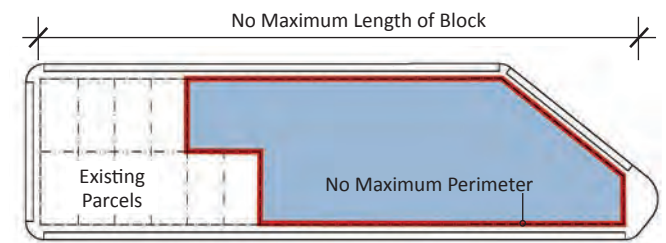


FIGURE 31: No Requirement for Abutting Existing Blocks

B. Lot Standards

1. Applicability

- i. Minimum rear yard, side yard, and frontage occupancy standards for single-family, two-family, townhouse, stacked townhouse, and similar shall comply with

FIGURE 32.

2. Access

- i. All single-family, two-family, townhouse, stacked townhouse, and similar lots shall access parking from an alley or rear drive. “Front Loaded” conditions shall not be permitted.

Lot Type	Front Build-to-Line	Secondary Build-to-Line	Minimum Rear Yard Setback	Minimum Side Yard Setback	Primary Frontage Occupancy	Secondary Frontage Occupancy
Reference dimension locations in FIGURES 33 – 35	A	B	C	D	E	F
Single-family & Two Family	9' or 12' or 15'*	Consistent with front Build-to-Line	4'†	6'; 0' for Attached Side	50% min.	40% min.
Townhouse, Stacked-townhouse, and similar	6' or 9' or 12'*	Consistent with front Build-to-Line	4'†	5'; 0' for Attached Side of End Unit and Middle Units	80% min.	83% min.
Multifamily, Mixed-Use, and Commercial	See Regulating Plan	See Regulating Plan	15'	5'; 0' for a Party Wall	See Regulating Plan	See Regulating Plan

* A front porch or covered stoop shall be required when the build-to-Line is 12' or 15'.

† Measured from Alley Right-of-Way

Note: There is no minimum requirement for Lot width.

Legend

.....	Property Line		Buildable Area
---	Build-to-Line		

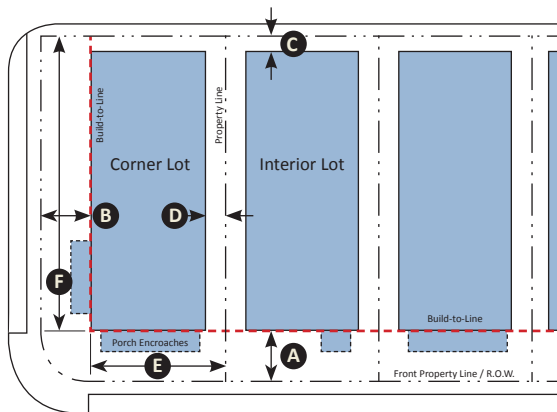


FIGURE 33: Single-family; Two-family on one Lot

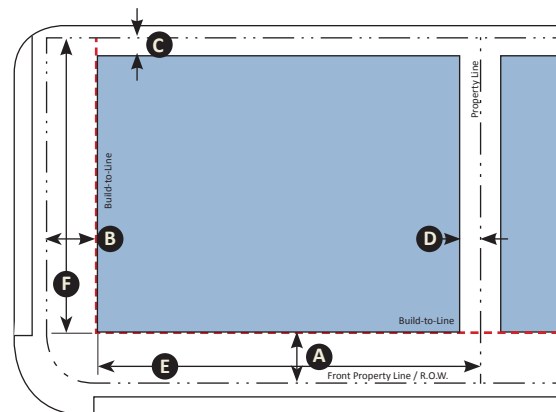


FIGURE 34: Multifamily; Mixed-Use; Commercial; Industrial

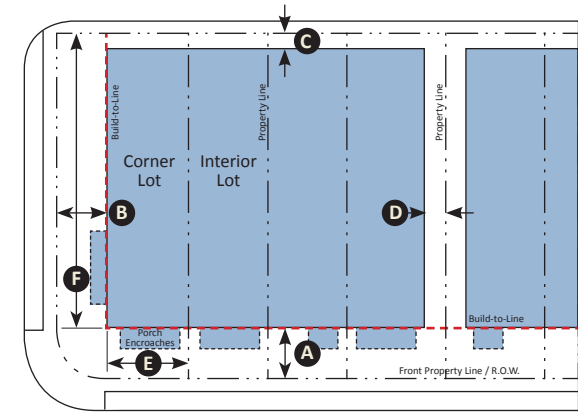


FIGURE 35: Two-family, semidetached; Townhouse Lot

Development Standards

Parking and Loading

C. Parking and Loading

Section 155.600 of the Village Code shall apply except as modified and noted herein.

1. Permitted Parking Locations

Permitted parking locations shall be governed by Layers (see **FIGURE 36**). The first layer is located between the lot line along the primary frontage and the Build-to-Line. The second layer is located between the Build-to-Line and a 20 foot setback from the Build-to-Line. The third layer is the remaining portion of the lot located behind the second layer.

- i. Off-street parking areas shall be located within the third layer along primary streets.
- ii. Off-street parking areas shall be located within the second or third layer along secondary streets.
- iii. Structured parking without a liner building shall be prohibited from occupying the Build-to-Line.
- iv. Townhouse parking shall be accessed via an alley or rear drive. Garages for individual townhouse dwellings shall not have direct garage access from a public street.
- v. Single-family and two-family parking shall be accessed via an alley.

- vi. Off-site parking may fulfill the requirements for off-street parking if it meets the following conditions:

- a. The off-site parking lot is located within five hundred (500) feet of the building, structure, or use served by the off-street parking area.

2. Parking Configuration

- i. All multi-level parking garages/decks shall be designed with a Liner Building facing the streets. This does not apply to driveways or drive aisles in parking lots.
- ii. Individual surface parking lots on adjacent parcels shall provide a vehicular connection between each other. The connection must be built at the time of construction of each parking lot.

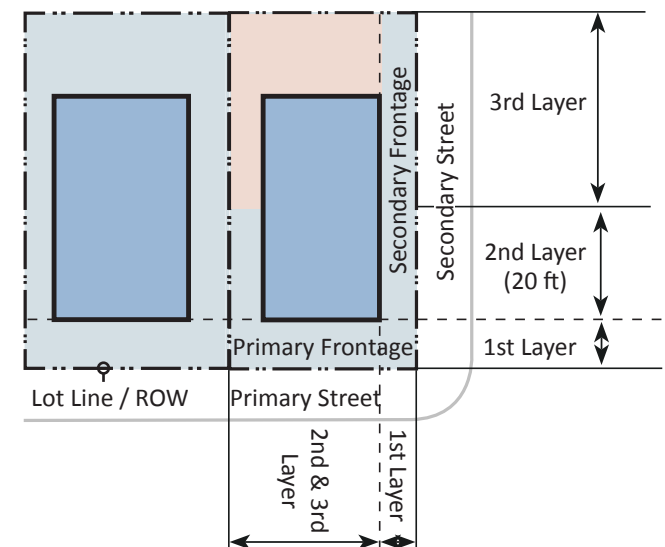


FIGURE 36: Parking Layers

3. Required Parking

- i. Accessible parking shall be provided as established in 155.602 Table 6.1
- ii. Required off-street parking spaces shall be as established in §155.602 Table 6.3 of the Lombard, IL Code of Ordinances except as noted below:
 - a. There shall be no fewer than one (1) parking space per unit.
 - b. On-street parking spaces immediately adjacent to the parcel shall count toward off-street parking requirements if there is no current plan for street widening.
 - c. 30 Percent of parking spaces provided may be built to "compact" standards.
 - d. Bicycle parking shall be provided for multi-family and commercial uses.
 - e. Short-term bicycle parking shall be located in a convenient and visible area, at least as close as the closest accessible automobile parking space.

4. Parking Lot Dimensional Standards

- i. Parking space and aisle standards shall be as found in **FIGURE 37**.

5. Loading

There shall be one (1) required off-street loading space.

Parking Lot Dimensions

Space Angle a	Space Width b	Space Length c	Aisle 1-way d	Aisle 2-Way e	Curb Width f
90°	9'	18'	24'	24'	9'
60°	9'	19'	18'	20'	10'-5"
45°	9'	18'	15'	20'	12'-9"
Parallel	7'	22'	12'	24'	N/A
Compact Space Dimensions					
90°	8'	16'	24'	24'	8'
60°	8'	17'-10"	18'	20'	9'-3"
45°	8'	17'	15'	20'	11'-4"

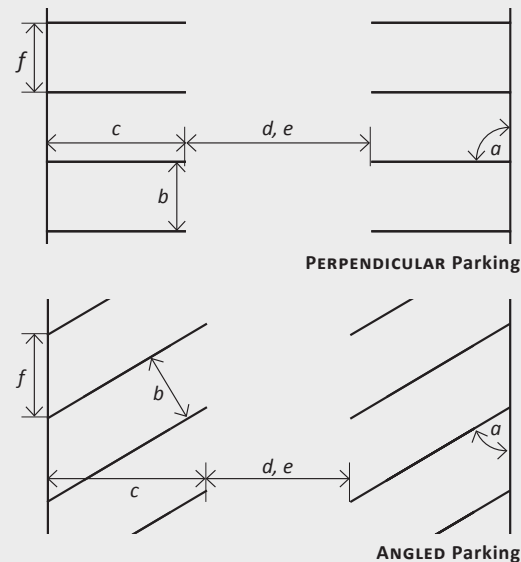


FIGURE 37: Parking Dimensions

Development Standards

Service

Walls, Fences, and Gates

Street Trees

D. Service

1. Service areas, such as trash and recycling, which are located outside of a building shall be located within the third Layer of a lot and shall not face the street.
2. Service areas shall be screened on all sides with an opaque fence or wall of the same materials as the primary building, except:
 - i. Single-family, Two-family, and Townhouse Residential uses are not required to provide trash screening.
3. Service areas, such as trash and recycling, and Loading areas located inside of a building shall have closable doors which are recessed from the face of the building. Such doors shall not be located on Yorktown Ring Road within 200' of the intersection of Grace Street and Yorktown Ring Road.

E. Walls, Fences, and Gates.

1. Above grade opaque walls shall be prohibited within a Private Front Yard.
2. Except as required to comply with Restaurant liquor laws, walls, fences, and gates shall be prohibited in Private Front Yards where operating retail or office uses are present.
3. Where permitted, fences and gates within a Private Front Yard shall not exceed a height of 36".
4. Privacy fences and garden walls shall not occur forward of the rear of a primary building.

5. Screening of alleys in single-family, two-family, and townhouse developments (see **FIGURE 38**).

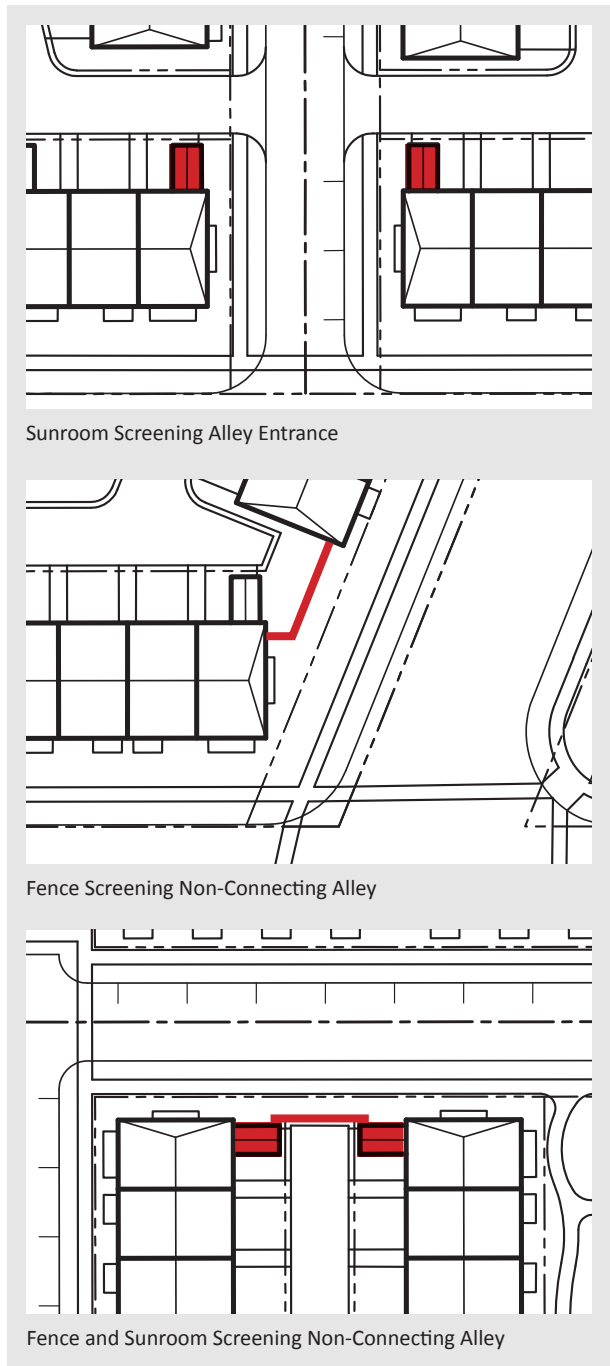
- i. When an alley goes through to the street, the rear shall be screened with either:
 - a. a garage, if applicable,
 - b. a full-height sunroom addition; or,
 - c. a wing wall of six (6) feet height minimum of the same materials as the primary facades of the building.
- ii. When an alley does not go through to the street, it shall be screened with either:
 - a. a minimum 6 feet tall screen wall is required on the street side. It shall be masonry and be designed as an extension of the house, or;
 - b. a full-height sunroom addition, or;
 - c. a combination of the two.

6. Chain link, barbed wire, razor wire, chicken wire, stockade fencing, and split rail fencing shall be prohibited.
7. The fencing type shall be standardized within a single development.

F. Street Trees

Right-of-way landscaping shall be provided per § 155.705 of the Village Code, except for:

1. Street trees shall be regularly spaced, not clustered, at a maximum spacing of forty (40) feet and shall be paired on opposite sides of the street, not staggered.

**FIGURE 38: Alley Screening**

Applicability

Materials

Review and Approval

Configurations

Section IX. – Signs

A. Applicability

1. Signs shall adhere to regulations found in Chapter 153 of the Village Code of Ordinances, Articles I, II, III, IV, and V with the following exceptions and additions.
2. The Following regulations shall apply to signs and to all permitted uses located in the Yorktown Commons Planned Development.

B. Materials

1. Signs shall be of materials consistent with the architectural style.
2. Fluorescent or glowing paint, billboards, moving or intermittently illuminated signs or advertising devices, flashing signs, and signs that make or create noise shall be prohibited within the Yorktown Commons PD.
3. Signs shall be designed so that all wiring, transformers, ballasts, starters, raceways, and other necessary equipment shall be concealed.

C. Review and Approval

1. Sign design and configuration shall be subject to review by the Yorktown Commons Architectural Review Board prior to application for permit. The Yorktown Commons Architectural Review Board may approve signs that do not meet these requirements if it is determined that they meet the intent of the Design Guidelines.
2. The Community Development Director may approve a sign that is not consistent with the regulations of this section when the sign is designed as a site-specific piece of civic art if it is determined that such sign is consistent with the intent of the PD Design Guidelines.

D. Configurations

1. Attached Signs – Four types of attached signs are permitted:
 - i. The Band Sign: The band sign consists of a band of lettering across the entire width of the building. If lit, band signs shall be front-lit. Band signs shall be a maximum of 36" tall, and the bottom of the band sign shall not be installed less than 10 ft. above the sidewalk. See **FIGURE 39.**

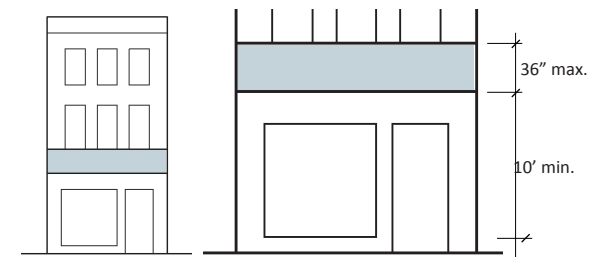


FIGURE 39: Diagram of Band Sign Dimension



FIGURE 39: Illustrative Example of Band Sign

- ii. The Board Sign: The board sign consists of painted or vinyl graphics on a signboard attached flush with the building wall. See **FIGURE 40**.
- iii. Window Signs: The window sign is located behind the glass or is comprised of gold leaf or vinyl applied directly to the glass. Window signs shall not be mounted on opaque signboards. The height of any window sign is limited to one-third the height of the glass in the sash where the sign is installed, excluding muntins.

The width of any window sign is limited to 90 percent of the width of the glass in the sash where the sign is installed. Signs may not be affixed with tape or other temporary means to the exterior nor to the interior of the glass surfaces. Neon signs shall be prohibited. Decals shall not be affixed to glass. See **FIGURE 41**.

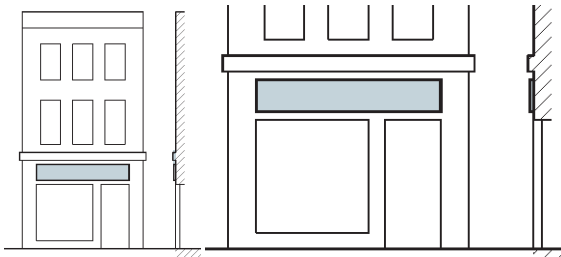


FIGURE 40: Diagram of Board Sign Dimension

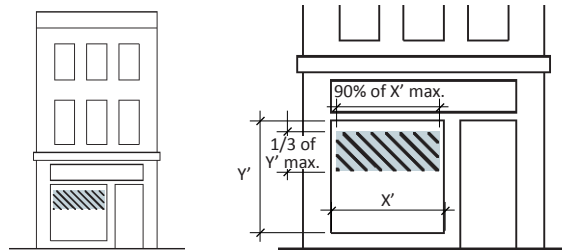


FIGURE 41: Diagram of Window Sign Dimension



FIGURE 40: Illustrative Example of Board Sign
(Credit: flickr K-FREE)



FIGURE 41: Illustrative Example of Window Sign

Configurations

2. Projecting Signs – Two types of projecting signs are permitted:

- i. Blade Signs: Blade signs hung from an architectural element shall be centered on that element. Blade signs projecting from the wall may project a maximum of 5'. The top of the blade sign shall be between 9' and 12' above the sidewalk. The blade sign shall be 32 in. tall maximum. Blade signs shall be no more than 4 ft. wide nor project more than 5 ft. from the wall. No blade sign shall exceed 6 sq. ft. Brackets or other suspension device shall match the architectural style of the building and shall not be computed as part of the allowable size of the sign.

See **FIGURE 42**.

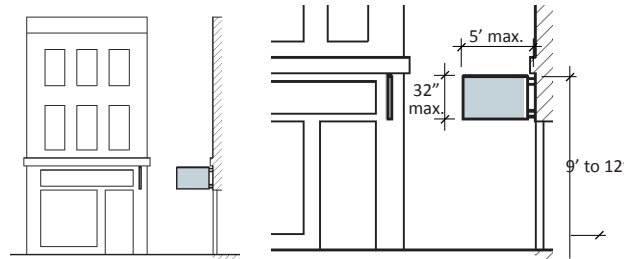


FIGURE 42: Diagram of Blade Sign Dimension



FIGURE 42: Illustrative Example of Blade Sign

- ii. Vertical Signs: Vertical corner signs are permitted at the corners of blocks. They may project perpendicular from one side of the building or at a 45 degree angle to the corner. Vertical corner signs may be lit with projecting lights. Vertical corner signs shall be mounted a minimum of 12' in height from the sidewalk, measured to the bottom of the sign. The height of the sign shall not exceed the first-storey wall height. Vertical corner signs shall be mounted 12" maximum away from the exterior face of the building and shall be a maximum of 3 ft. wide. See **FIGURE 43**.

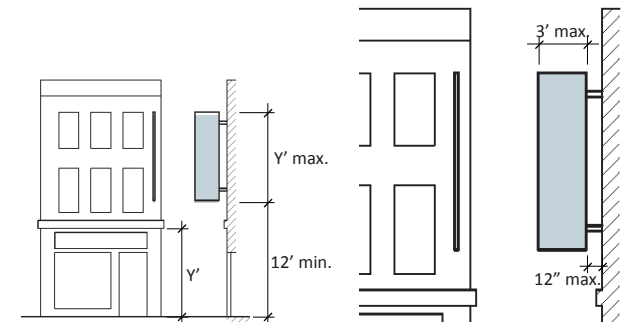


FIGURE 43: Diagram of Vertical Sign Dimension



FIGURE 43: Illustrative Example of Vertical Sign
(Credit: flickr Kansas Sebastian)

3. Ground Signs – Sculptural and A-frame sign boards placed on the sidewalk shall be permitted if they are temporary and removed during non-operating hours. See **FIGURE 44.**
4. Awning Signs – : Signage may be painted either on the fringe of an awning or in the center of the body of the awning. Awning signs shall be painted directly on canvas. Back lit awnings are prohibited. Signs that occupy the fringe of the awning may fill the entire height and width of the fringe up to a maximum fringe height of 9".
5. Security Signs – One sign providing notice of a security system is permitted at each frontage and shall be affixed to a building.
6. Real Estate “For Sale” and “For Rent” Signs – One real estate sign advertising a property for sale or rent may be displayed at each frontage.
7. Freestanding, off-site and detached signs are prohibited unless noted otherwise.
8. Signs may encroach into setbacks and across R.O.W. lines, but not across private lot lines.
9. Temporary Signs shall be permitted for openings or special events for a maximum time period of 72 hours.
10. Project Identification signs shall be permitted if approved by the Yorktown Center Architecture Review Board.



FIGURE 44: Illustrative Example of A-Frame Ground Sign
(Credit: flickr kellan)

Architectural Standards: All Buildings

Orientation

Entries

Windows

Walls

Mechanical

Equipment

Site Utilities

Materials

Townhouse String

Configuration

Section X. – Architecture Standards: All Buildings

To promote the creation of walkable neighborhoods, and in recognition of the architectural history of the Village of Lombard, building designs should provide sufficient articulation to create human-scaled architecture and be compatible with the historic character. Architectural design shall be subject to review by the Yorktown Commons Architectural Review Board and the following architecture standards shall apply to all buildings within the Yorktown Commons PD.

A. Orientation

1. Primary Building Facades shall be oriented to the street. A building located on a corner lot shall have two Primary Building Facades.

B. Entries

1. The primary entry shall be located along the Primary Building Façade and shall be designed with architectural features and details that clearly communicate the entry.
2. Service entrances shall be located at the rear of the building.

C. Windows Along Street Frontages

1. Primary Building Facades shall have windows.
2. Windows may be clear glass or may have a low-emissivity coating. No mirrored or tinted glass shall be permitted.
3. Windows shall have a vertical proportion meaning that they are taller than they are wide (see **FIGURE 45**).
4. For residential buildings, windows in a facade shall be regularly spaced and the same size, except for accent windows.

D. Walls

1. Designs and materials shall be consistent on all Primary Facades.
2. Where real or simulated brick, stone, stucco, or similar materials are used on the Primary Facade(s), the material shall continue along the secondary facades a minimum of 16" in depth measured from the face of the Primary Facade.

E. Mechanical Equipment

1. Utility meters shall be located so as not to be visible from the primary frontage. Such equipment shall be located to the interior of the block behind buildings.

F. Site Utilities

1. Site utility structures, such as transformers, shall be located to the interior of blocks (behind buildings) or along the side of the buildings.

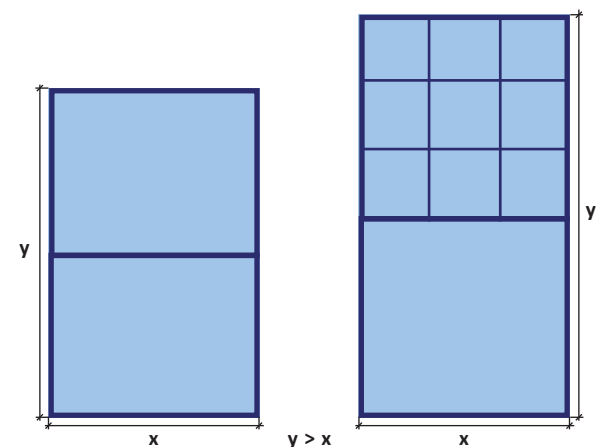


FIGURE XX: Vertical Proportions

G. Materials

1. Vinyl siding shall not be permitted on any facade for all buildings.
2. Membrane roof material shall only be permitted for flat roofs.

H. Townhouse String Configuration

1. String shall be designed as a composed building and shall not be designed as individual independent facades (see **FIGURE 46**).
2. Breaks and recesses between units in the string shall be designed to occur as a part of the string composition and shall not alternate back and forth between every dwelling unit (see **FIGURE 47**).
3. Changes in material shall occur to reinforce the string composition and shall not occur with each dwelling unit.
4. Changes in color or material from one townhouse unit to another shall only occur when there is either a change in facade plane or vertical break in the facade plane.

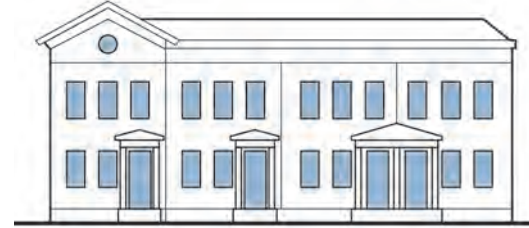


FIGURE 46: Townhouse String Designed as a Building

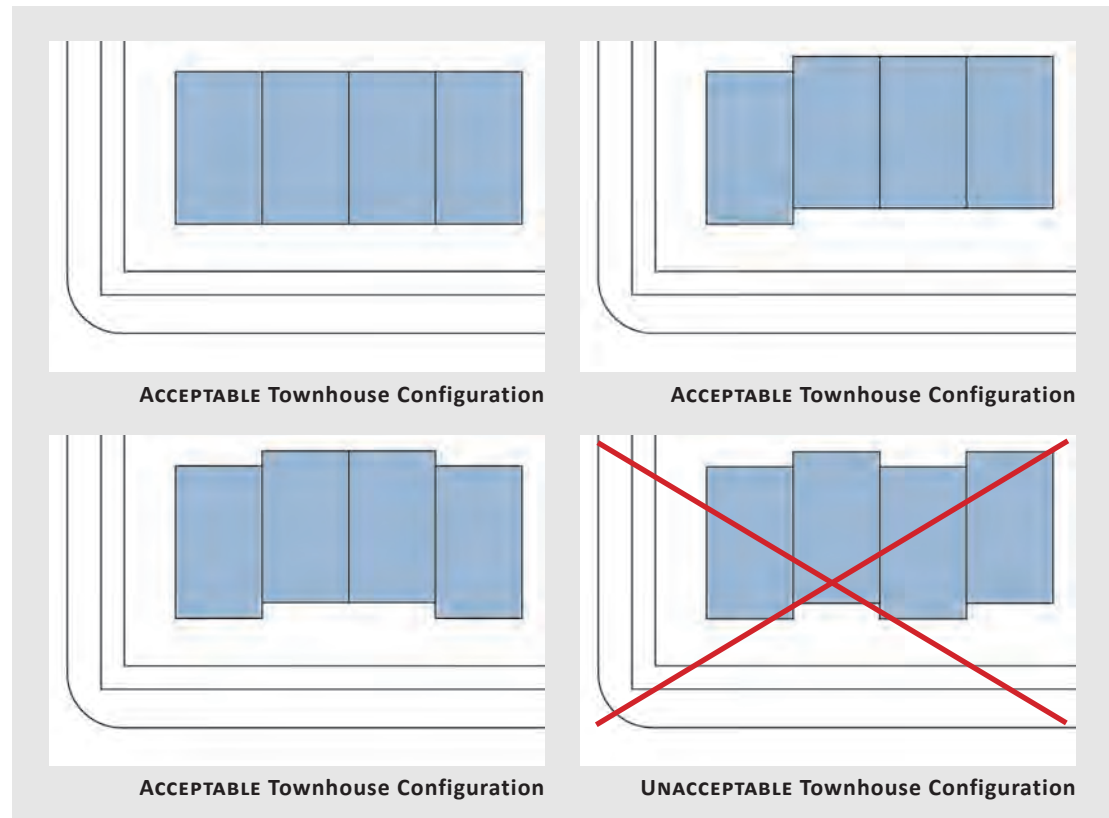


FIGURE 47: Horizontal Breaks in Townhouse Strings

Architectural Standards: Commercial, Mixed-Use, and Large Multifamily

Configuration

Windows

Mechanical

Equipment

Materials

Entries

Section XI. – Additional Architecture Standards: Commercial, Mixed-Use, and Large Multifamily Buildings

In order to reinforce the idea of a pedestrian scale and to have the varied look of a number of buildings built over time, the following requirements apply to all Commercial and Mixed-Use buildings:

A. Configuration

1. The minimum building height shall be eighteen (18) feet.
2. Blank walls shall not face streets. Liner buildings with primary building facades shall be used to screen blank walls from the street.
3. Facades longer than 250' shall be designed to look like more than one building. For buildings that are 250' or longer, no section of building longer than 200' shall look like one building. Each section of building shall be different in color and/or material than the other sections and shall have different base heights, cornice heights, and window sizes.

B. Windows

1. The minimum percentage of facade surface along Streets that shall be glazed (window surface area) is forty (40) percent of the first floor facade. The area of the first floor facade is calculated as the length of facade multiplied by the average height, as measured from the ground to the elevation of the second floor (see **FIGURE 48**).
2. The maximum height of sill above sidewalk shall be twenty (20) inches.

3. Solid metal security gates, solid roll-down windows, link, grill or other similar security devices shall only be permitted when installed from the inside of a building within the window or frame.
4. The maximum percentage of glass on the first floor that may be blocked with interior fixture, paint, paper signs, paper, or similar material shall be 20% and may not exceed a height of four (4) feet above the exterior ground line.

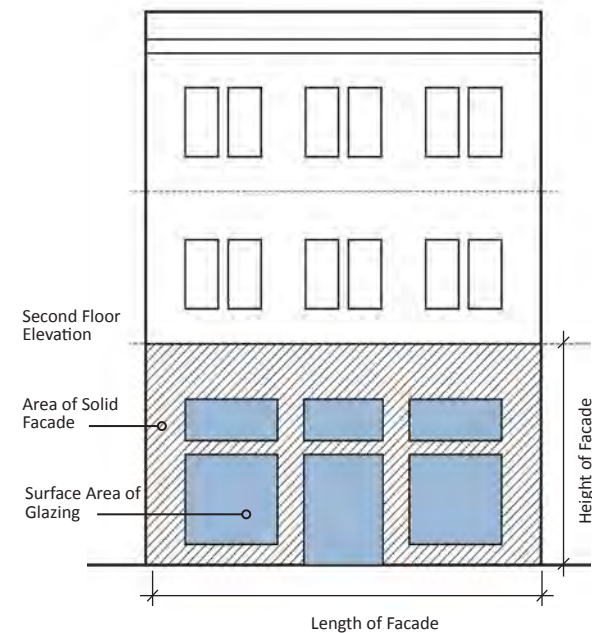


FIGURE 48: Percentage of Window Surface Area

C. Mechanical Equipment

1. Window air conditioning units shall not be permitted on Primary Facades.

D. Materials

1. A maximum of two primary materials shall be used on a facade or a facade section as it is described Section 11 (A) (3) of this document. A primary material is one that occupies one-third or more of the facade (excluding windows) for 3-story buildings or one-fourth or more of the Primary Facade (excluding windows) for 4-story and taller buildings. A maximum of one primary material shall be used on a Primary Facade for 2-story buildings unless a Craftsman style is used where the proportions of materials shall be consistent with the historical style.
2. Exterior light fixtures shall match the architectural style of the building.

E. Entries (FIGURES 49 AND 50)

1. Mixed-use buildings shall have an entrance to access upper story uses located along the Primary Facade. Additional entrances to access upper story uses may be located elsewhere on the building.
2. Entries along sloping streets shall provide safe, flat entryways.
3. Residential style doors shall be prohibited.



FIGURE 49: Sloping Street



FIGURE 50: Level Street

Definitions

Section XII. – Definitions

Except where specifically defined in the Design Guidelines or the Village Code of Ordinances, all words used carry their customary meanings. For terms not defined in this section and that have been defined in other sections or chapters of the Code of Ordinances, the definitions in those other sections or chapters apply. In the event that a definition in this section conflicts with a definition in another part of the Code of Ordinances, the definitions in these Design Guidelines shall control.

Words used in the present tense include the future. Words used in the singular number include the plural, and words used in the plural include the singular, unless the context clearly indicates the contrary. The word “shall” is always mandatory. The word “may” is permissive. The word “should” indicates a recommendation but not a legal requirement. Building or structure includes any part thereof. The word “lot” includes the word “plot” or “parcel.” The word “person” includes an individual person, a firm, a corporation, a partnership, and any other agency of voluntary action.” The words “include,” “includes,” and “including” shall be interpreted as though followed by the phrase “without limitation” or “but not limited to.” The phrase “used for” includes “arranged for,” “designed for,” “intended for,” “maintained for,” and “occupied for.” The official designated to make an interpretation of all terms used here is the Community Development Director.

A

Alley Entrance: The location along a street where an alley is accessed.

Alley Loaded: A house or garage where parking spaces or parking garages are accessed from an alley.

B

Balcony: An accessory area to a Dwelling, with one or more sides permanently open to the exterior except for a railing or parapet not exceeding four feet in height.

Block Face: A single side of a block, including all primary facades on that block.

Build-to-Line: The line shown on the regulating plan along which the principal vertical plane of all primary facades must be erected (except as otherwise provided in this Part). The Build-to-Line location is measured from—and is generally parallel to—the parcel line, lot line, or Right-of-Way. To accommodate irregular small jogs in parcel lines, the Build-to-Line may be regularized as long as it is consistent with the intent of the regulations contained in these Guidelines.

C

Civic Building: A Building designed specifically for a Civic use.

Civic Use: A use that is open to the public at least some of the time and provides a focal point

for community interaction and fosters citizen participation in civic activities, including churches, temples, synagogues, mosques, and other religious facilities; lodges; college or university facilities; exhibition halls and art galleries; grade schools; library; meeting halls; museum or similar facilities; performance theaters; post office; fire house; public administration offices; trade or specialty school facilities; or similar uses.

Commercial: A generic term for the use of land or buildings for commerce or work, other than for civic uses. Examples of commercial uses include personal service businesses, retail businesses, offices for businesses and non-profit organizations, medical offices, professional offices, private gyms, privately owned entertainment facilities, repair shops, and industrial uses.

Commercial, Other: *Any commercial use other than retail, gas station, industrial, utility (other than for local distribution or collection) and any use with a drive-through.*

Context: The surroundings and their character – This includes both the Physical Context which is the physical surroundings and the Historical Context which is the historic character of a region.

Corner Tower: A tower that is at the corner of a building, located at the corner of a block, mews, or pedestrian paseo.

Courtyard: A space enclosed on three or four sides by buildings.

Covered Stoop: A raised landing located at a building entry with a roof covering the landing.

Cul-de-sac: A dead-end street with only one entry point and a turnaround.

D

Density: The quantity of development as a proportion of the parcel area measured in Floor Area Ratio (FAR). See “Floor Area Ratio.”

Design Review: A comprehensive analysis of a building or structure to determine if the design meets the intent and regulations of this document.

Dooryard: A private frontage type with a shallow setback and front garden with a low fence at the public right-of-way line.

Duplex: A building type consisting of a single structure occupied by two dwelling units that may have either shared or separate entrances for each unit and are architecturally presented as a single-family house.

Definitions

E

Encroach: To break the plane of a vertical or horizontal regulatory limit with a structural or architectural element, so that it extends into a required setback area or private frontage, or above a height limit.

Encroachment: A structural or architectural element that breaks the plane of a vertical or horizontal regulatory limit, extending into a required setback area or private frontage, or above a height limit.

Entrance, Principal: the main point of access of pedestrians into a building, facing a street.

F

Facade: The exterior wall of a building.

Form-Based Code: A type of land use regulation that significantly regulates the form of the built environment and public realm rather than the focusing on the separation of land uses as commonly found in conventional “Euclidean” zoning.

Frontage Line: The property line at the street right-of-way dividing the right-of-way from the private frontage area.

Frontage Occupancy: The minimum length of primary façade that shall be built along a build-to-line.

G

Garden Wall: A wall no greater than 48” in height that defines the frontage line and/or the perimeter of a property. A garden wall has ornamental qualities and may be solid or comprised of piers and a knee wall with pickets between piers.

Ground Cover: A low growing dense growth of plants other than grass that entirely covers the ground, such as pachysandra.

H

Hydraulic Lift: A parking method in which mechanized lifts are used to lift one car above the ground so that another car can park underneath.

I

Industrial: Uses involving manufacturing, assembling, fabrication, warehousing, wholesale, and servicing of motor vehicles or industrial equipment.

L

Land Use: The purpose for which a land or a structure is designed, arranged, or intended to be occupied or used, or for which it is occupied, maintained, rented, or leased.

Layer: A range of depth within a lot used to regulate the location of permitted uses and structures.

Liner Building: A building that conceals a separately constructed garage and that is designed for occupancy by residential, commercial, or mixed-uses.

M

Mews: A linear public or private open space lined on both sides by buildings, primary facades, and front doors.

Mixed-Use: A building or parcel with multiple uses.

Mixed-Use Building: A building designed for and containing more than one use.

Multi-Family Building: A condominium or rental building designed for occupancy by three or more families living independently of each other in separate dwelling units, other than townhouses and stacked townhouses.

O

Office: A place of business where professional or clerical duties are performed in either for-profit or not-for-profit entities.

P

Parcel Line: (Syn: Lot Line)

Parti: The basic scheme or concept for a building as represented in a simple diagram.

Paseo: A public walk open to the sky located between two buildings that is designed for comfortable and safe pedestrian passage from one location to another and for pedestrian amenities such as seating, lighting, and landscaping.

Passage: A connecting public walkway that enables pedestrians to pass under a building at ground level.

Paver: A pre-cast paving material consisting of blocks that can be lifted by a person without mechanical assistance.

Pervious Paving: Paving material that permits water to infiltrate into the ground.

Primary Building: The building or buildings on a lot that contain the principal use or uses.

Primary Entrance: (Syn: Principal Entrance)

Definitions

Primary Façade: The façade of a building that faces the street. When a building is located on a corner lot, both façades that face streets and/or a mews are primary facades. When a building faces a mews, the façade facing the mews is the primary façade.

Primary Frontage: The side of a lot facing a street designated on the Regulating Plan as a primary street.

Primary Street: A street designated on the regulating plan as a primary street. Typically, a primary street has more vehicular circulation, a greater mix of uses, and more pedestrian accommodation than a secondary street.

Principal Entrance: The main point of access of pedestrians into a building facing a street.

Privacy Fence: A fence with a height greater than 48”.

Private Frontage: The space that occurs between the public right-of-way and the primary façade.

Public Frontage: The space that occurs between the street curb and the frontage line (right-of-way line).

Public Space: Land so designated on a Regulating Plan that is generally not developed with buildings and is dedicated and available to the general public for active or passive recreation and for civic uses. Public spaces may include civic uses and civic buildings within them.

R

Regulating Plan: A regulatory instrument in the form of a scalable, dimensionally correct, plan that is a key element of most form-based codes.

Retail: Sale of goods and provision of personal services directly to consumers. Where a Retail use is required in these Design Guidelines, a Restaurant shall also be considered as a Retail use for these purposes.

Retail Frontage: A frontage specifically designed to support pedestrian oriented retail activity.

S

Secondary Street: A street designated on the regulating plan as a secondary street. Typically, a secondary street is a street with less pedestrian circulation than a Primary Street and has a lesser degree of mixed uses than primary streets.

Secondary Frontage: The side of a lot facing a street designated as a secondary street.

Shared Parking: A system in which available parking is shared between two or more uses resulting in a reduction in the total number of spaces needed due to differing peak demand times.

Sharrow: A lane on a street that is shared by cars and bicycles and that has painted markings designating the use of bicycles.

Shopfront, Private Frontage: A private frontage type with an entirely paved ground plane, designed to accommodate first-floor commercial and retail uses with high pedestrian volumes.

Single-Family Building : A building designed for and occupied exclusively as a home or residence for not more than one family.

Stacked Town House Building: A fee-simple, condominium, or rental dwelling unit configured such that one two-story unit is generally located over another two-story unit and both units have separate entrances facing a street or mews. Each pair of stacked units is designed to look like a single townhouse and each pair of stacked units is attached to another pair of stacked units on one or two sides by a common party wall so that there are a minimum of four dwelling units configured together.

Streetscape: All of the elements that are located between the street curb and the primary facade when the sidewalk extends from the street curb to the primary façade, and all of the elements that are located between the street curb and right-of-way when the sidewalk does not extend to the primary façade.

T

Tandem Parking: A parking configuration in which a car is parked in front of another, requiring the second car to be moved in order to provide egress for the first car.

Thoroughfare: The area contained in a right-of-way of a public or private street.

Tower, Architectural: A stand-alone structure that is significantly taller than it is wide, or a portion of a building that is significantly taller than it is wide and typically has more detail than the surrounding building(s). When a tower is a portion of a building, the tower eave or cornice is taller than the remainder of the building eave or cornice height and one or more of the tower facades is located forward of the remaining building façade.

Townhouse Building: A single-family fee-simple, condominium, or rental dwelling unit that is attached on one or two sides of the building by a common party wall where there are a minimum of three units and each unit has a separate entrance facing a street or mews.

Tuck-under Townhouse/Tuck-under Duplex: A town house or duplex building where parking is accommodated within the building footprint and where the parking is accessed from an alley and where garage doors hide the parking spaces.

Definitions

V

Verge: The linear grass or planted area located between the sidewalk and the curb of a street.

Vista Termination: A building, structure, or portion of a building or structure, specifically designed to visually attract a viewer's attention at the end of a visual axis, i.e. to terminate a view. Vista terminations may include towers, corner towers, symmetrical facades centered on a visual axis, an architecturally embellished entry, or similar distinctive architectural devices.

W

Screen Wall: A masonry upright structure serving to enclose, divide, or protect an area.

Wing Wall: A solid wall extending from the back of a building to the alley rear property line setback line having a height equal to the first story of the building.

Y

Yard (Private Frontage): A private frontage type where the building is not set close to the street, the lot frontage is not defined by a fence, and the ground plane is primarily either grass or ground cover.



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