

PLAN COMMISSION

INTER-DEPARTMENTAL REVIEW COMMITTEE REPORT

CONDITIONAL USE FOR A PLANNED DEVELOPMENT – YORKTOWN COMMONS

NOVEMBER 23, 2015

Title

PC 15-27C

Property Owner

KRE YTC Venture LLC
203 Yorktown
Lombard, IL 60148

Petitioner – Consultant

Rosanova & Whitaker, Ltd.
c/o Russell Whitaker
30 W. Jefferson St., Ste. 200
Naperville, IL 60540

Property Location

Northeast corner of Butterfield
Rd. and Highland Ave.
Trustee District #3

Zoning

B3PD Community Shopping
District Planned Development

Existing Land Use

Regional Shopping Center

Comprehensive Plan

Regional Commercial

Approval Sought

Approve a conditional use for a
planned development with
companion deviations, use
exceptions, and variations.

Prepared By

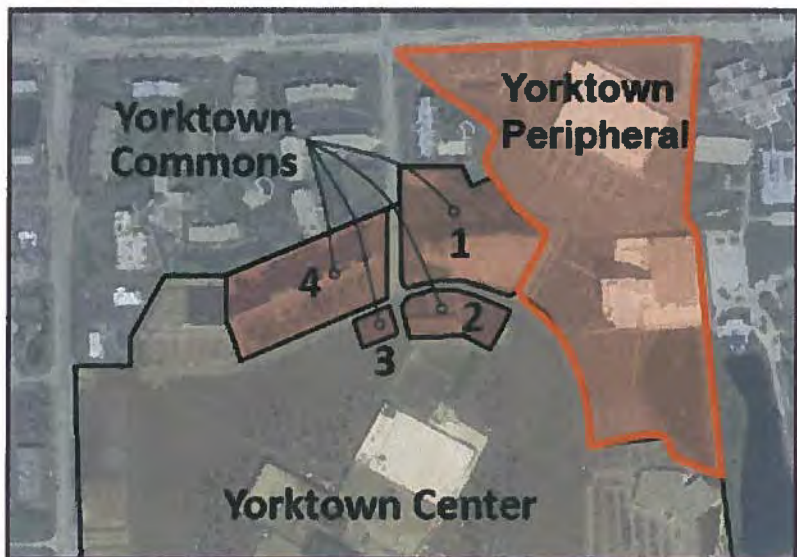
Matt Panfil, AICP
Senior Planner



LOCATION MAP

DESCRIPTION

Now that Parcels 1 through 4 have been removed from their respective planned developments, see PC 15-27A and PC 15-27B, the petitioner requests approval of a conditional use for an approximately fifteen (15) acre, mixed-use planned development entitled Yorktown Commons.



There are no specific physical developments associated with this request. Similar to previously approved planned development such as Highlands of Lombard and Fountain Square, the petitioner is only seeking to establish the terms for which development may occur.

PROJECT STATS

Planned Development Size

Parcel 1:	5.9 acres
Parcel 2:	1.8 acres
Parcel 3:	0.5 acres
Parcel 4:	6.0 acres

Total: 14.2 acres

Submittals

1. Petitions for public hearings, dated October 19, 2015;
2. Three (3) separate project narratives with responses to standards corresponding to each item, PC 15-27 A through C, dated October 19, 2015;
3. Yorktown Commons PD Design Guidelines, prepared by Torti Gallas + Partners, and dated October 19, 2015;
4. Yorktown Commons Traffic Study and Appendix, prepared by Eriksson Engineering Associates, Ltd., and dated October 19, 2015;
5. Parking Memorandum from Stephen B. Corcoran, P.E., PTOE, Dir. of Traffic Engineering for Eriksson Engineering, Associated, Ltd., dated October 15, 2015;
6. Natural Resource Review prepared by the Illinois Dept. of Natural Resources at the request of V3 Companies, Ltd., and dated July 21, 2015;
7. Civil Design Guidelines, prepared by V3 Companies, Ltd., and dated October 19, 2015;
8. Prelim. Stormwater Analysis, prepared by V3 Companies, Ltd., and dated October 19, 2015;

This approach will allow for the property to be developed competitively with other properties within the Yorktown Shopping Center and the Butterfield Road Corridor. While no definitive plans, structures, or tenants are being presented at this time, lot, bulk, and design standards and guidelines are enumerated within the attached "Yorktown Commons Planned Development Design Guidelines" (Exhibit B), which is essentially a regulatory tool for future development.

Design Standards:

Critical to the land use planning of this proposal is the incorporation of said design guidelines which are presented in a format which is commonly referred to as a "form-based code."

By utilizing a form-based approach, the petitioner maintains a certain degree of market flexibility in regards to the various combinations of development that may occur within the subject property, and the Village has a level of predictability of how the subject area will look and function through the establishment of certain lot, bulk, and design standards. The concept plans depicting building type and location included within the Yorktown Commons Planned Development Design Guidelines document are only illustrative and could be adjusted in any number of ways to meet market conditions or aesthetic / design preferences.

Flexibility to the market is important to the petitioner as the nature of regional shopping centers continues to evolve from their automobile-dominated, post-war origins. The petitioner's team has conducted extensive research that suggests an increased desire on behalf of both businesses and residents for a more traditional mixed-use type of environment where housing, work, and shopping and entertainment are all in close proximity. The new supply of residents will also benefit the surrounding commercial uses by expanding their customer base.

Review and Approval Process:

As development of the property occurs, the Plan Commission will maintain oversight of the development through the "Administration" provisions established in Section IV of the Yorktown Commons Planned Development Design Guidelines. The process will allow for each separate parcel to apply for Site Plan Approval as a distinct phase of the overall development. The Director of Community Development will determine if the plan conforms substantially to the preliminary plan established within PC 15-27C or if it constitutes either a minor change or major change to the preliminary plan, as defined by Section IV (E) and (F) of the Yorktown Commons Planned Development Design Guidelines.

Submittals (cont.)

9. Wetland Delineation and Assessment Report, prepared by V3 Companies, Ltd., and dated August 12, 2015;
10. Prelim. Plat of Subdivision, prepared by V3 Companies, Ltd., and dated October 14, 2015; and
11. Five (5) ALTA/ACSM Land Title and Topographic Surveys, one each for Parcels One through Three and both an east and west survey for Parcel Four, prepared by V3 Companies, Ltd., and dated October 5, 2015.

If a proposal conforms substantially to the preliminary plan, no public hearing is required and the proposal will appear before the Plan Commission for final Site Plan Approval. If the proposal represents a major change to the preliminary plan, then the item will require a public hearing before the Plan Commission, who will make a recommendation to the Village Board for final consideration.

Development Schedule / Phasing:

Although the anticipated phases are not an obligation and can change depending on market conditions, etc., pursuant to Section 155.507 (B)(3)(h) the petitioner has indicated that the anticipated development schedule is the same as the numbers assigned to each relevant parcel, with Parcel 1 being anticipated to be developed first and Parcel 4 anticipated to develop last.

APPROVAL(S) REQUIRED

Pursuant to Section 155.502 (F)(3) of the Lombard Ordinance, the petitioner requests approval of a conditional use for a planned development with the following companion deviations, use exceptions, and variations:

1. Deviations and variations from the B3 Community Shopping District's lot and bulk requirements, Sections 155.415 (D), (E), (F), (G), (H), (I), (J), and (K) of the Lombard Zoning Ordinance, to allow for the establishment of a form-based code predicated upon lot, bulk, and design standards and guidelines as set forth within the Yorktown Commons Planned Development Design Guidelines included within the petitioner's submission;
2. Approve the Yorktown Commons Planned Development Design Guidelines as the preliminary plan for the subject property and to extend the timeframe for the final plan approval as set forth in the Yorktown Commons Planned Development Design Guidelines;
3. A variation from Section 155.508 (B)(3) of the Lombard Zoning Ordinance to allow for use exceptions, ground-floor residential dwelling units, for more than forty percent (40%) of the site are or more than forty percent (40%) of the total floor area, whichever is less;
4. A variation from Section 155.707 of the Lombard Zoning Ordinance to waive transitional landscape yard requirements where residential uses are proposed in the B3 Community Shopping District; and

5. A variation from the Planned Development Standards, Article IX of the Lombard Zoning Ordinance, to establish the process for administration of the Yorktown Commons Planned Development as set forth in Section IV of the Yorktown Commons Planned Development Design Guidelines and granting site plan approval authority to the Plan Commission, pursuant to Section 155.511 of the Lombard Zoning Ordinance.

EXISTING CONDITIONS

As summarized in the PC 15-27A and PC 15-27B IDRC Reports, the geographic area of the proposed Yorktown Commons Planned Development is as follows:

- Parcel 1: Vacant parcel with a constructed ditch and remnant parking lot from the Yorktown Condos temporary sales office / model showroom (circa 1973).
- Parcel 2: Vacant structure (former Bamboo Room).
- Parcel 3: Surface parking for JC Penny's Department Store.
- Parcel 4: Eastern part of the Yorktown Convenience Center.

INTER-DEPARTMENTAL REVIEW

Building Division:

The Building Division has been involved with numerous conversations with the petitioner regarding building type and previously approved Building Code amendments. A full Building Code review will be conducted for any and all future development submissions.

Fire Department:

As the proposal involves only the preliminary stages of an overall redevelopment plan for the area, the Fire Department has no issues or concerns regarding the project at this time.

Private Engineering Services (PES):

Private Engineer Services has the following comments regarding the proposed project:

1. Within the Preliminary Stormwater Analysis (Exhibit C), PES requests additional language to be included within the document acknowledging that the assessment is based upon the current Village of Lombard and DuPage County Stormwater Ordinances with effective dates in 2013. Future development permit applications will be required to follow the stormwater ordinances in effect at the time of their submission; and
2. The Wetland Delineation and Assessment Report (Exhibit D) verifies that no wetland areas, Waters of DuPage, or Waters of the U.S. were delineated on the subject property.

The report also found that eastern cottonwood and common buckthorn were the most dominant plant species within the subject area, which are not plant species inherently worthy of preservation at this site.

Public Works:

The Department of Public Works has the following comments regarding the proposed project:

1. Public Works supports the concept of changing the street lighting standard from the commonly used cobra-arm commercial lighting to the Village's specified residential lighting standards. The specific locations and lighting to be used will be reviewed as part of any future development;
2. The right-of-way cross section referenced within the Yorktown Commons Planned Development Design Guidelines is consistent with the concepts being promoted within the Department of Public Works' bicycle and pedestrian planning efforts currently underway. However, the final design of right-of-way cross sections is still subject to Public Works' review at the time of the submission of any development.

Also, the Village will require these right-of-way improvements as part of the development of either Parcel 1 or Parcel 4, whichever occurs first; and

3. Although not under the purview of the Plan Commission, the Department of Public Works also notes that they have engaged a consultant to determine whether the existing sanitary lift station and force main serving the subject area has adequate capacity for the proposed additional effluent. A report with the results of this study is expected by January 2016.

At this time, it remains possible that the proposed planned development will need to either increase the capacity of the existing lift station or construct a new sanitary lift station, and possibly also install a new force main. Please note that the submitted Civil Design Guidelines (Exhibit E), which include parcel specific recommendations for demolition, layout and paving, sanitary sewer, water main, stormwater management, lighting, and private utilities, do not mention this issue.

Planning Services Division:

The Planning Services Division notes the following:

1. Surrounding Zoning & Land Use Compatibility

	Zoning	Land Use
North	B3PD & R5PD	Yorktown Peripheral PD, Yorktown Apartments, and Yorktown Condos
South	B3PD	Yorktown Shopping Center Planned Development
East	B3PD	Yorktown Shopping Center and Yorktown Peripheral Planned Developments
West	B3PD	Yorktown Shopping Center Planned Development

In consideration that the nearest adjacent properties were either once part of the same development (Yorktown Shopping Center and Yorktown Peripheral planned developments) or are themselves multi-family residential, staff finds the proposed Yorktown Commons Planned Development and associated preliminary plan for redevelopment as either mixed or residential-only uses is consistent with the zoning and land uses of surrounding properties.

2. Comprehensive Plan Compatibility

As the petitioner identifies in the project narrative for PC 15-27C, the Lombard Comprehensive Plan emphasizes a focus on, "the improvement and maintenance of existing development and encouraging

compatible development and redevelopment.” Furthermore, as part of the vision of the Comprehensive Plan, Vision 3 specifically identifies, “Lombard will create and maintain viable commercial districts throughout the Village.” Among the recommended actions to achieve Vision 3, the Comprehensive Plan includes the following:

1. Design and facilitate a Village-wide system of commercial development providing for neighborhood, community and regional commercial areas;
3. Ensure that commercial and retail development is compatible with neighboring land use areas; and
5. Ensure that new development and redevelopment of private properties is in scale with and designed to be complementary to existing development.

Taking into consideration Yorktown Shopping Center’s status as a prominent existing development, the proposal is consistent with efforts to maintain existing development and encourage compatible redevelopment. Also, the proposed Yorktown Center Planned Development Design Guidelines ensure that future development will be compatible with and consistent in scale and design with neighboring land uses.

While the proposal is for predominantly residential uses, the Comprehensive Plan defines regional commercial as, “a larger, more intensively developed mixed-use commercial area that serves as a key center for the region.” Staff finds that the mixed-use proposal is consistent with the regional commercial future land use designation for the property. Furthermore, as identified within the project narrative, the subject property is a poor location for retail-only land uses due to its poor visibility.

3. *Zoning Ordinance Compatibility*

For the Plan Commission’s convenience, a color-coded table comparing the proposed zoning standards with the underlying B3 Community Shopping District and existing Yorktown Shopping Center / Yorktown Peripheral planned developments has been included as Exhibit F. Within the table, items are coded into one of three (3) categories:

- | | |
|---------|--|
| Blue: | The proposed planned development is more permissive than the underlying zoning, but roughly equivalent to the existing planned developments. |
| Orange: | The proposed planned development is more permissive than the underlying zoning, but stricter than the existing planned developments. |
| Green: | The proposed planned development is more permissive than both of the existing planned developments. |

A. Land Uses:

Within Section VI of the Yorktown Commons Planned Development Design Guidelines, the petitioner has identified all permitted uses in the B3 Community Shopping District as permitted uses within the Yorktown Commons Planned Development. Other specifically listed permitted uses include: various types of residential uses, hotels, daycare as an accessory use, and mixed-use buildings that incorporate any of the permitted uses.

This approach to land use is similar to that within the existing Yorktown Shopping Center Planned Development, except the Yorktown Shopping Center PD allows for any permitted or conditional use within a "B" classified zoning district as a permitted use. While the petitioner has not enumerated conditional uses within the Yorktown Commons Planned Development Design Guidelines, staff requests specific language be added that all conditional uses in the B3 Community Shopping District be conditional uses within the Yorktown Commons Planned Development.

In regards to the requested variation from Section 155.508 (B)(3) of the Lombard Zoning Ordinance to allow for ground-floor residential dwelling units as use exceptions, staff notes that the Lombard Zoning Ordinance does specifically permit, "dwelling units, located above the first floor, provided: at least fifty percent (50%) of the square footage of the building footprint on the ground floor is devoted to uses permitted (permitted uses and/or approved conditional uses)" within the B3 Community Shopping District. Therefore, if the underlying zoning were in effect, the petitioner already has the right to construct mixed-use buildings within the subject property. However, such mixed-use buildings would be limited by the maximum building height limitation of two (2) stories or thirty feet (30'), whichever is less.

Staff can support the requested use exception variation based on the fact that ground floor residential dwelling units would be permitted by right in up to forty-nine percent (49%) of the ground floor area. While the petitioner intends on including ground floor commercial space, the demand for commercial space within the subject area has shown to be less than what would be required by a fifty percent (50%) ground floor commercial space requirement.

In summary, the uses within the Yorktown Commons Planned Development are more limiting than the neighboring Yorktown Shopping Center and Yorktown Peripheral planned developments; however, the proposed planned development is more flexible in housing type and the amount of ground-floor residential permitted. With the condition that the B3 Community Shopping District conditional uses identified in Section 155.415 (C) be specifically included within the Yorktown Commons Planned Development Design Guidelines, staff can support the proposed land uses associated with the project.

In regards to the petitioner's request for a variation from Section 155.415 (I) of the Lombard Zoning Ordinance, staff can support the variation provided the petitioner provide language within the Yorktown Commons Planned Development Design Guidelines that while outdoor service areas (including outdoor dining) are permitted, outside storage is not permitted within the planned development.

B. Lot Bulk:

Although the petitioner has requested variations for both lot area and lot width, Sections 155.415 (D) and (E) of the Lombard Zoning Ordinance respectively, none of the four (4) proposed parcels are less than the minimum required 20,000 square feet area of area or one-hundred foot (100') width requirements. However, it is anticipated that future resubdivisions of the subject parcels may result in substandard lot area and/or lot width. Staff can support the requested variations in order to allow maximum flexibility in building type and layout for future development. The other design standards within the Yorktown Commons Planned Development Design Guidelines will further assure quality development, regardless of lot area or lot width.

C. Setbacks:

In lieu of the traditional building setbacks within the B3 Community Shopping District, thirty feet (30') for front, rear, and corner side yards and ten feet (10') for interior side yards, the petitioner is proposing to utilize "build-to-lines" and "frontage occupancy" standards which instead of mandating a specific minimum distance a building is to be from a property line, require a certain percentage of a structure to be built within a certain distance of the property line. Staff finds that the proposed build-to-lines and frontage occupancy requirements will help establish a more pedestrian-oriented street-wall where buildings are closer to each other and their entrances are closer to sidewalks.

In consideration that the proposed build-to-lines and frontage occupancy requirement further the vision of the Yorktown Commons Planned Development, and that said regulations are generally more restrictive than the existing Yorktown Shopping Center and Yorktown Peripheral planned developments, staff supports the requested setback variations associated with Section 155.415 (F) of the Lombard Zoning Ordinance.

D. Building Height:

The petitioner has requested a variation from Section 155.415 (G) to allow for structures on Parcel 1, Parcel 2, and Parcel 4 to be six (6) stories or one-hundred feet (100'), whichever is less. The Lombard Zoning Ordinance allows a maximum height of two (2) stories or thirty feet (30'), whichever is less. The requested height variation is similar in scope to the maximum structure height permitted within the Yorktown Shopping Center and Yorktown Peripheral planned developments, which essentially allowed for four (4) buildings to be up to approximately one-hundred and five feet (105') to one-hundred and twenty-five feet (125') in height. The range in height varies because the height is to be measured from sea level and the topography varies by up to twenty-feet throughout the site.

As the proposed six (6) stories or one-hundred feet (100') height maximum is consistent with the surrounding multi-family residential structures and the building heights allowed within the adjacent planned development, staff can support the requested building height variation.

E. Open Space:

The petitioner is proposing a minimum of twelve and one-half percent (12.5%) open space, which is consistent with the minimum open space requirement for planned developments with deviations, as established in Section 155.508 (C)(7) of the Lombard Zoning Ordinance. However, the petitioner's definition of open space includes hardscape uses such as courtyards, squares, pedestrian paths, etc., which means that the amount of open space, as defined by the Lombard Zoning Ordinance as, "that portion of a lot or property maintained as lawn, garden, field, woods, wetland, or other natural landscape area and is free of buildings, structures and impervious surfaces" can be zero percent (0%).

While the proposed open space requirement is less restrictive than the underlying zoning, it is much more restrictive than the Yorktown Shopping Center and Yorktown Peripheral planned development requirements which stipulate only up to thirty percent (30%) of the site to be occupied for building purposes provided the remaining area shall remain as open spaces. However, the definition of open spaces applied in the original Yorktown planned development includes space which may be utilized for parking, loading berths, signs, traffic lanes, and pedestrian walkways and

landscaping. While the amount of open space, as defined by the Lombard Zoning Ordinance, can also be zero percent (0%) in this scenario, the Yorktown Commons requires a certain percentage of open space to be utilized for pedestrian amenities, not just parking or drive aisles.

Therefore, staff can support the requested variation from Section 155.415 (H) in consideration that it furthers the vision of the Yorktown Commons Planned Development as a more “urban,” pedestrian-oriented environment, and that the proposed open space requirements are more beneficial to the public than those within the adjacent planned developments. It is important to note that any impervious area would be subject to Village stormwater regulations.

F. Transitional Yards:

Section 155.415 (J) of the Lombard Zoning Ordinance requires that wherever a rear or interior side yard of a lot in the B3 Community Shopping District abuts a lot in the CR Conservation Recreation District or a residence district, all principal and accessory buildings and structures shall be set back from the abutting lot line a distance of forty feet (40'). Staff concurs with the petitioner's request to waive this requirement because the proposed Yorktown Common Planned Development is predominantly residential in nature. Also, the multi-family residential zoning districts within the Lombard Zoning Ordinance (R4, R5, and R6) do not require transitional building setbacks when a multi-family residential use is adjacent to another multi-family residential use.

Similar to the transitional building setback discussed above, Section 155.415 (K) of the Lombard Zoning Ordinance also requires a thirty foot (30') wide transitional landscape yard wherever a rear or interior side yard of a lot in the B3 Community Shopping District abuts a lot in the CR Conservation Recreation District or a residence district. Staff can support the requested transitional landscape yard variation based on the same reasoning as in the case above; the proposed planned development is predominantly residential in nature and the Lombard Zoning Ordinance does not require transitional landscape yards when a multi-family residential use is adjacent to another multi-family residential use.

Section 155.707 of the Lombard Zoning Ordinance establishes the size and required improvement for transitional landscape yards. While the granting of a variation to Section 155.415 (K) would eliminate the necessity for a landscape yard, both staff and the petitioner felt it was in the best interest to request a variation from Section 155.707 for the sake of clarity.

G. Signage:

In combination with the underlying signage regulations established within the Lombard Sign Ordinance, the petitioner has proposed additional, context-sensitive, signage regulations found within Section IX of the Yorktown Commons Planned Development Design Guidelines. The proposed signage regulations differ from the Lombard Sign Ordinance requirements in the following:

1. Sign design is subject to review by the proposed Yorktown Commons Architectural Review Board (i.e., owner) prior to submission of a building permit application to the Village;

2. The Community Development Director is permitted to approve a sign that is not consistent with the design guidelines when the sign is designed as a site-specific piece of civic art, if it is determined that such sign is consistent with the intent of the planned development (assuming Village Code compliance);
3. Wall signs are limited in typology to a maximum three foot (3') tall band sign or board sign which consists of painted or vinyl graphics on a signboard;
4. Instead of regulation window sign size by limiting the total area to a maximum of twenty percent (20%) of the total window surface area, the petitioner proposes that the height of window signs be limited to one-third ($1/3^{\text{rd}}$) the height and the width of window signs be limited to ninety percent (90%) of the width of the glass in the sash where the sign is installed;
5. In regards to project signs, the petitioner proposes that the maximum distance from which a projecting sign may project from the wall to be five feet (5') instead of the Lombard Sign Ordinance requirement that projecting signs project no more than two-thirds ($2/3^{\text{rd}}$) the width of the public sidewalk or a maximum of four feet (4') from the supporting wall, whichever is less. In order to be consistent with the Lombard Sign Ordinance, staff asks that the five foot (5') maximum projection be reduced to four feet (4'). If a further extension is warranted, an application can be made for a signage deviation in association with a Site Plan Approval application. Please note that the Plan Commission has final authority to grant signage deviations associated with Site Plan Approval and such a signage deviation would not require Village Board approval.

The petitioner proposes that such a sign may only be up to thirty-two inches (32") in height, whereas the Lombard Sign Ordinance has no such requirement. Also, the petitioner requires project signs to be a minimum of nine feet (9') above the sidewalk, whereas the Lombard Sign Ordinance requires a minimum of eight feet (8') above the sidewalk;

6. The petitioner requests that sculptural and A-frame sign boards, referred to as sandwich board signs within the Lombard Sign Ordinance, to be permitted to be placed on the sidewalk if they are temporary and removed during non-operating hours. Staff can support this request, provided additional language is provided within the Yorktown Commons Planned Development Design Guidelines that specifies that sandwich board signs are to comply with Section 153.234 of the Lombard Sign Ordinance;
7. In regards to awnings, the petitioner has specified that advertising on the valance may be up to nine inches (9") in height, whereas the Lombard Sign Ordinance allows for advertising up to ten inches (10") in height.
8. In regards to real estate signs, staff requests that the petitioner provide additional language that real estate signs are to comply with Section 153.234 of the Lombard Sign Ordinance;

9. The petitioner has specified that temporary signs be permitted for a maximum time period of seventy-two (72) hours, which is shorter in duration than any of the permitted temporary signs within the Lombard Sign Ordinance.

H. Parking:

In combination with the underlying parking regulations established within Section 155.600 of the Lombard Zoning Ordinance, the petitioner has proposed additional, context-sensitive, parking regulations found within Section VIII (C) of the Yorktown Commons Planned Development Design Guidelines. The proposed parking regulations differ from the underlying zoning in the following:

1. The petitioner proposes a system of three (3) layers in which parking is regulated. The first layer is located between the lot line along the primary frontage and the build-to-line. The second layer is located between the build-to-line and a twenty foot (20') setback from the build-to-line. The third layer is located in the remaining portion of the lot behind the second layer. Using this system, the petitioner is proposing to restrict off-street parking within the third layer along primary streets and within the second or third layer along secondary streets.

The petitioner is proposing to require townhouse parking to be access via an alley or rear drive and garages for townhome units shall not have direct access from Grace Street or the Yorktown Center ring road. Also, single-family and two-family residential dwelling units shall be accessed via an alley.

Furthermore, the petitioner requests that off-site parking may fulfill the requirements for off-street parking if the off-street parking spaces are within five-hundred feet (500') of the building or use served by the off-street parking area. Such parking is permitted by right in the Yorktown Shopping Center Planned Development.

Staff can support these proposals as they are more restrictive than the Lombard Zoning Ordinance and should help shape a pedestrian-oriented environment that is consistent with the vision of the planned development.

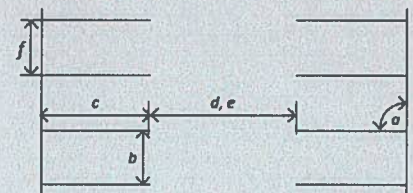
2. To further provide a more pedestrian-oriented environment, the petitioner proposes to require all multi-level parking structures to be designed with a "liner building" facing the street(s). A liner building is defined within Section XII of the Yorktown Commons Planned Development Design Guidelines as, "a building that conceals a separately constructed garage and that is designed for occupancy by residential, commercial, or mixed-uses."
3. While Section VIII (C)(3)(ii)(a) of the Yorktown Commons Planned Development Design Guidelines stipulates that there shall be no fewer than one (1) parking space per unit, the petitioner has agreed that the minimum parking spaces requirement be consistent with Section 155.602 Table 6.3 of the Lombard Zoning Ordinance, which requires a minimum of one and one-half (1½) parking spaces for one and two bedroom multi-family dwelling units.

4. In Figure 37 of the Yorktown Commons Planned Development Design Guidelines (right), the petitioner proposes a parallel parking space width of seven feet (7') and a compact parking space width of eight feet (8'). The Village recently completed an update, which was researched and evaluated against modern industry standards, to the Lombard Zoning Ordinance parking dimension standards at the behest of the current property owner (see PC 14-02). The recently adopted dimensions specify a parallel parking space to be a minimum of nine feet (9') in width and the narrowest, compact parking space, to be a minimum of eight foot three inches (8'3") in width. The proposed dimensions which are more permissive in the Yorktown Commons Planned Development Design Guidelines than the existing dimensions in the Lombard Zoning Ordinance are highlighted in red.

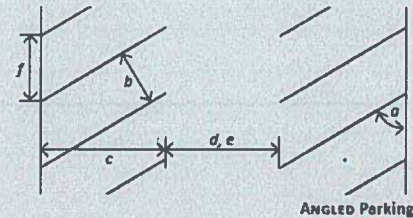
While staff understands the pedestrian-oriented reasoning behind the requested dimension reductions, if such reduced parking space dimensions are required for future development, it is in the best interest of the Village that such dimensions be specifically identified and reviewed on the merits of the specific development. Therefore, staff requests that the petitioner amend the parking lot dimensions highlighted in red to reflect the current Lombard Zoning Ordinance standards.

Parking Lot Dimensions

Space Angle a	Space Width b	Space Length c	Aisle 1-Way d	Aisle 2-Way e	Curb Width f
90°	9'	18'	24'	24'	9'
60°	9'	19'	18'	20'	10'-5"
45°	9'	18'	15'	20'	12'-9"
Parallel	7'	22'	12'	24'	N/A
Compact Space Dimensions					
90°	8'	16'	24'	24'	8'
60°	8'	17'-10"	18'	20'	9'-3"
45°	8'	17'	15'	20'	11'-4"



PERPENDICULAR Parking



ANGLED Parking

FIGURE 37: Parking Dimensions

4. Site Design

A. Yorktown Commons Planned Development Design Guidelines – Site Plan Approval

While the petitioner has included a preliminary plan with illustrative scenarios of how development may occur within the planned development, the ultimate development plans will be submitted at a later date. To facilitate an efficient review and approval process, and pursuant to Section 155.511 of the Lombard Zoning Ordinance, the petitioner requests, with staff's support, Site Plan Approval authority be granted to the Plan Commission.

B. Density:

Section VI (D) of the Yorktown Commons Planned Development Design Guidelines identifies 970 residential dwelling units, 28,000 square feet of retail space, and 100,000 square feet of other commercial space as the maximum buildout scenario for Yorktown Commons. In this scenario, the ratio of dwelling units per acre (DUs/acre) is approximately 65 DUs/acre. This ratio is very

similar to the 63 DUs/acre that was the maximum permitted density for the Yorktown Apartments Planned Development, as established by Ordinance No. 1323 in 1972.

Lombard Multi-Family Residential Developments							
Case No. or Year Built	Name	Address	Zoning	Height	# of Units	Size (Acres)	DU/Acre
Yorktown Apartments (1967 Approval)							
1998	Liberty Square Condos	2240-60 S. Grace St.	RSPD	4 stories	112	4.42	25.34
1972, SPA 14-03	Yorktown Apartments	2233, -39, and -55 S. Highland Ave.	RSPD	5-15 stories	374	7.13	52.45
		2233 S. Highland Ave.	RSPD	15 stories	236		
		2239 S. Highland Ave.	RSPD	7 stories	78		
		2255 S. Highland Ave.	RSPD	5 stories	60		
1972	Yorktown Apartments	2200-10 S. Grace St.	RSPD	6 stories	106	3.54	29.94
1969	Yorktown Green Condos	2201-22 S. Highland Ave.	RSPD	6 stories	264	12.21	21.62
Totals					856	27.30	31.36
Max. Allowed by Ord. No. 1323					1722	27.30	63.08
Unit Surplus					866		
Max. Yorktown Commons Units					970	15.00	64.67

Also, should Yorktown Commons be approved and built out to the maximum 970 dwelling units, the total number of dwelling units within the Yorktown Apartment and Yorktown Commons planned developments would be approximately 1,826, or just 104 more dwelling units than are permitted for just the geographic area of Yorktown Apartments.

While the Yorktown Apartments were never constructed to their maximum allowable density, staff finds that the proposed maximum density for Yorktown Commons is consistent with previous approvals and surrounding development. Exhibit G identifies the densities for various other multi-family developments throughout the Village.

C. Traffic & Circulation:

Eriksson Engineering Associates, Ltd. prepared the Traffic Study (Exhibit H) based upon the Year 2021 traffic conditions which projects traffic beyond the anticipated construction of the development and also incorporates a regional growth rate as well as the recently approved Yorktown Apartments expansion (SPA 14-03), Highlands of Lombard apartment project (PC 15-04), and Sam's Club project (PC 15-21). The Traffic Study also was based upon the most intense buildout scenario as identified within the Yorktown Commons Planned Development Design Guidelines; 970 residential units, and 28,000 square feet of retail space, and 100,000 square feet of other commercial space.

The study found that the proposed development and its associated traffic volumes can be accommodated by the existing area road network. Also, with the modifications approved for the Butterfield Road entrance to Yorktown Center that were recently completed and the proposed improvements for the Sam's Club (PC 15-21) project, the total project traffic volumes will work well at the intersection of Fairfield Avenue, Yorktown Center ring road, and Butterfield Road.

Key recommendations within the study include, but are not limited to:

1. The intersection of the Yorktown Center ring road and the north-south access drive immediately east of Highland Avenue (near McDonald's and Carson's Furniture Gallery) should be modified to correct existing deficiencies by providing better striping and signage to guide drivers;

2. Each parcel should provide sidewalks along their frontage with Grace Street, the Yorktown Center ring road, and the access routes to JC Penny's, Target, and the Westin; and
3. As the existing and proposed traffic volumes do not warrant Grace Street to be four (4) lanes wide, it should be modified by converting the existing curb travel lanes into a bike lane, approximately five feet (5') wide, and on street-parking, approximately seven feet (7') wide.

The Village's traffic consultant, Kenig, Lindgren, O'Hara, Aboona, Inc. (KLOA), reviewed the Traffic Study and provided comments (Exhibit I). Based on KLOA's review, the proposed Yorktown Commons Planned Development can be accommodated by the Yorktown Shopping Center's internal roadway and external intersections. There is sufficient reserve capacity at the studied intersections to accommodate the projected traffic volumes and continue operating at acceptable levels of service.

KLOA concurs with the recommendation to provide sidewalks along the frontage of each parcel on Grace Street and Yorktown Center ring road. Lastly, KLOA also concurs with the recommended striping and signage modifications at the intersection of the Yorktown Center ring road and the north-south access drive immediately east of Highland Ave (near McDonald's and Carson's Furniture Gallery). However, it is suggested that a preliminary striping plan be provided for further review.

In addition to KLOA's comments, staff notes:

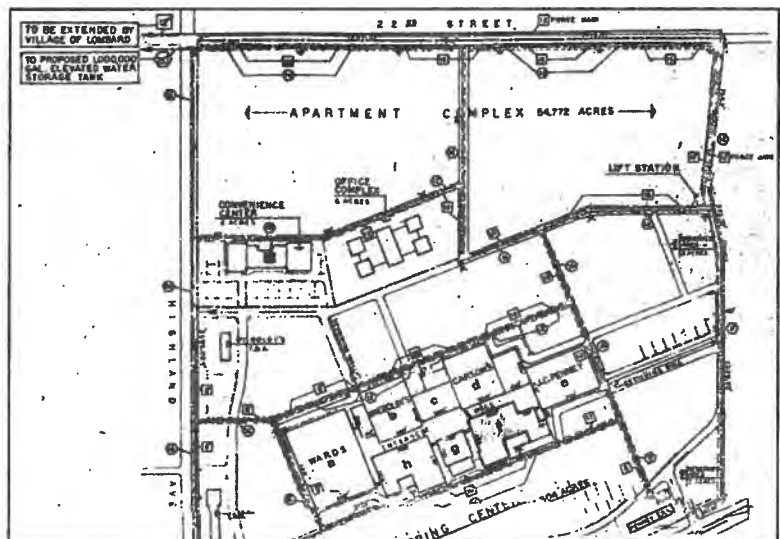
1. The traffic volume estimates established in Section III, Table 2 of the Traffic Study are to serve as a "trip bank" to be drawn upon as each site plan is approved; and
2. Bike and pedestrian connections from the surrounding residential uses to Parcel 4 are to be identified prior to, or at the time of development of said parcel.

SITE HISTORY (NON SIGN-RELATED)

1966:

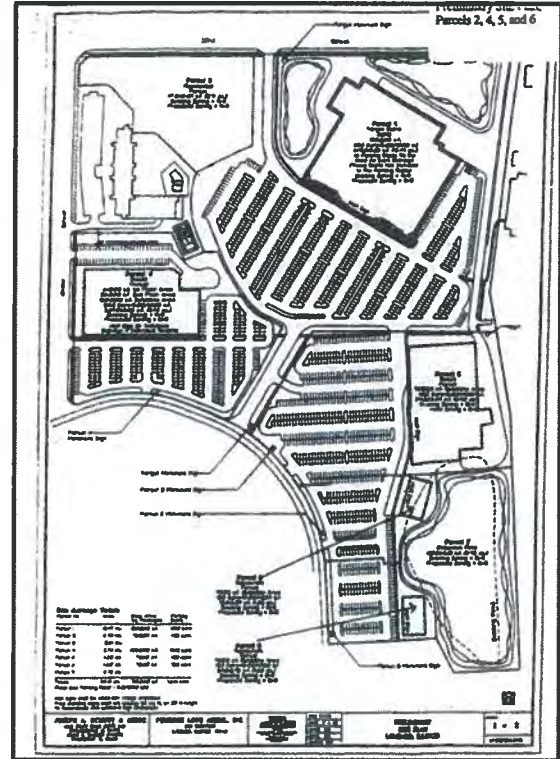
Yorktown Shopping Center Planned Development

Established of the Yorktown Shopping Center Planned Development via Ordinance No. 1172. Please note within the originally approved site plan (below) that Parcel 1 was identified for multi-family residential development and Parcel 4 was designated for use as an office complex.



PC 94-14: Yorktown Peripheral Planned Development

Amendment to the geographic boundaries of the Yorktown Shopping Center Planned Development to remove approximately 15.6 acres to establish the Yorktown Peripheral Planned Development. Please note within the originally approved site plan (lower right) that Parcel 1 was identified for use as a 100,000 square foot retail structure.



FINDINGS & RECOMMENDATIONS

Based on the above findings, the Inter-Departmental Review Committee has reviewed the petition and finds that it meets the standards for a conditional use and standards for a planned development with deviations, as established by the Lombard Zoning Ordinance. As such, the Inter-Departmental Review Committee recommends that the Plan Commission make the following motion recommending **approval** of this petition:

Based on the submitted petition and testimony presented, the proposed conditional use for a planned development with companion deviations, use exceptions, and variations **complies** with the standards required by the Village of Lombard Zoning Ordinance; and, therefore, I move that the Plan Commission accept the findings of the Inter-Departmental Review Committee Report as the findings of the Plan Commission and I recommend to the Corporate Authorities **approval** of PC 15-27C, subject to the following conditions:

1. That this relief is valid only with the approval of both PC 15-27A and PC 15-27B;
2. That this relief is limited to the establishment of a planned development with a preliminary plan and design guidelines. Any physical site improvements or alterations require approval through the Village through the established Site Plan Approval process or through a separate zoning public hearing petition, whichever is applicable; and
3. That the petitioner shall satisfactorily address all comments noted within the Inter-Departmental Review Committee Report that pertain to the proposed form-based code regulations prior to final consideration of the petition by the Village Board.

Inter-Departmental Review Committee Report approved by:

A handwritten signature in black ink, appearing to read 'William J. Heniff', is written over a horizontal line.

William J. Heniff, AICP
Director of Community Development

c. Petitioner

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EXHIBIT A – PROJECT NARRATIVE WITH RESPONSE TO STANDARDS

STATE OF ILLINOIS)
)
COUNTY OF DUPAGE)
)
VILLAGE OF LOMBARD)

**PETITION TO THE VILLAGE OF LOMBARD FOR AMENDMENT TO
THE YORKTOWN SHOPPING CENTER PLANNED DEVELOPMENT**

YTC Mall Owner, LLC and YTC Land Owner, LLC (the “Petitioner”), as the owners of certain property as more specifically set forth below, respectfully petition the Village of Lombard (the “Village”) to:

1. Revoke the Yorktown Shopping Center Planned Development with respect to the property hereafter described as the Yorktown Commons Property;
2. Amend the Yorktown Shopping Center Planned Development to reduce the required parking; and
3. Subdivide certain property as generally depicted on **Exhibit A**, hereinafter the “**Plat of Subdivision.**”

In support of the requested relief, Petitioner hereby represents to the Village as follows:

1. The Petitioner, YTC Mall Owner, LLC and YTC Land Owner, LLC, both Delaware limited liability companies, are the owners of the respective properties legally described on **Exhibit B** (the “**YTC Property**”).

2. J.C. Penney Company, a Delaware Corporation (“JCP”), is the owner of the property legally described on **Exhibit C** (the “**JCP Property**”).

3. The YTC Property and the JCP Property are collectively part of the larger Yorktown Shopping Center Planned Development (the “**Yorktown PD**”), which planned

development was approved by the Village in 1966 pursuant to Ordinance No. 1172 and facilitated the original development of the Yorktown Shopping Center.

4. The Yorktown Shopping Center Planned Development was subsequently amended in 1995 in order to remove certain properties from the original Yorktown Shopping Center Planned Development and to facilitate development of the Target property under a new planned development known as the Yorktown Peripheral Planned Development (the “**Peripheral PD**”).

5. The collective properties currently within the Yorktown PD are legally described in **Exhibit D** (collectively the “**Yorktown PD Properties**”) and the boundaries of the Yorktown PD and the Peripheral PD are generally depicted on the zoning map attached hereto as **Exhibit E**.

6. Petitioner hereby requests that the YTC Property and the JCP Property be subdivided as generally depicted on the Plat of Subdivision, as may be hereafter amended.

7. Petitioner hereby requests the Yorktown PD be revoked with respect to the properties depicted on the Plat of Subdivision as Parcel 1, Parcel 2, Parcel 3 and Parcel 4 (collectively the “**Yorktown Commons Property**”).

8. Petitioner hereby requests that the Yorktown PD be amended to reduce the required parking supply from 7,500 spaces to 6,800 spaces.

9. The proposed amendment to the Yorktown PD is necessary and appropriate to facilitate the continued success of the Yorktown Shopping Center and meets the applicable standards for a planned development with deviations as follows:

A. Any reduction in the requirements of this Chapter is in the public interest.

The actual parking requirement for the Yorktown PD and the relationship of that

parking requirement to the original planned development requirement of 7,500 spaces is relatively difficult to track. As outlined above, the area within the Yorktown PD has morphed over time, with the total area following removal of the Yorktown Commons Property being at least 50 acres less than originally approved as part of the Yorktown PD. The individual users within the Yorktown PD and the area utilized by those users are regularly changing as individual leases are executed, modified and/or terminated. Moreover, traffic and other improvements around the perimeter of the shopping center have seemingly modified the parking supply on a yearly basis. These modifications or improvements in and around the Yorktown PD have kept the shopping center relevant as part of ongoing efforts to reshape the asset consistent with industry standards and the needs/wants/desires of the general public that utilizes the shopping center. The parking study submitted concurrent with this application details the resulting parking supply for the Yorktown PD and substantiates the supply as sufficient to meet user demands based on actual parking counts conducted during peak shopping center operations. Accordingly, the proposed deviation is in the best interest of the public as it facilitates the continued improvement of the shopping center property consistent with modern trends while maintaining sufficient parking supply to meet user demand at peak operation.

B. The proposed deviation would not adversely impact the value or use of any other property.

The proposed deviation would not adversely impact the value or use of any other property as the parking study substantiates that the proposed parking supply is sufficient to meet and exceed, by a comfortable margin, the peak parking demand at the Yorktown PD.

C. That such deviations are solely for the purpose of promoting better development which will be beneficial to the residents or occupants of the planned development as well as those of the surrounding properties.

The proposed deviations will facilitate the elimination of existing parking fields that serve no practical purpose and are underutilized the vast majority, if not all of the year. As noted above, the area within the Yorktown PD has, or will shrink, by approximately 50 acres. The proposed removal of the Yorktown Commons Property from the Yorktown PD will result in a gross reduction of overall building square footage of approximately 80,000 square feet. In light of the Dunkin Donuts project and anticipated development of a parcel along Highland Avenue, we still anticipate a net reduction in building square footage of approximately 65,000 square feet. Between historic parking demand as set forth in the parking study and the reduction of usable building area, the proposed parking deviation will promote better and more efficient development between the Yorktown PD and surrounding properties.

D. That the overall floor area of the planned development shall not exceed by more than 40 percent the maximum floor area permitted for the individual uses in each applicable district.

Petitioner does not require relief to increase the permissible floor area.

E. That in residential planned developments the maximum number of dwelling units allowed shall not exceed by more than 40 percent the number of dwelling units permitted in the underlying district.

Petitioner does not propose any residential development within the Yorktown PD.

F. That all buildings are located within the planned development in such a way as to dissipate any adverse impact on adjoining buildings and shall not invade the privacy of the occupants of such buildings and shall conform to the following:

Petitioner does not propose any new buildings for the Yorktown PD.

- a. The front, side or rear yard setbacks on the perimeter of the development shall not be less than that required in the abutting zoning district(s) or the zoning district underlying the subject site, whichever is greater.*

All Yard requirements will be met under the proposed changes to the Yorktown PD.

- b. All transitional yards and transitional landscape yards of the underlying zoning district are met.*

All Yard requirements will be met under the proposed changes to the Yorktown PD.

- c. If required transitional yards and transitional landscape yards are not adequate to protect the privacy of property adjacent to the development, the Plan Commission shall recommend either or both of the following requirements:*

The proposed yards are sufficient to protect privacy of property adjacent to the development.

- i. All structures located on the perimeter of the planned development must set back by a distance sufficient to protect the privacy and amenity of adjacent existing uses;*
- ii. All structures located along the entire perimeter of the planned*

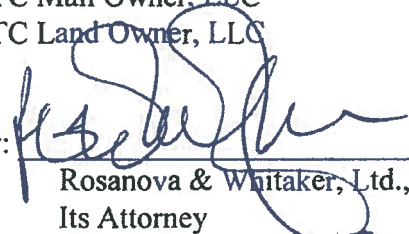
development must be permanently screened with sight-proof screening in a manner which is sufficient to protect the privacy and amenity of adjacent existing uses.

WHEREFORE, by reason of the foregoing, the undersigned Petitioner requests the Village Board and Plan Commission take the necessary steps to grant the aforementioned zoning and subdivision relief and such other relief as may be necessary and appropriate based on the request outlined above to: 1) revoke the Yorktown PD with respect to the Yorktown Commons Property; 2) to subdivide the property as depicted on the Plat of Subdivision; and 3) to grant a deviation to reduce the required parking for the Yorktown PD from 7,500 spaces to 6,800 spaces, all pursuant to the provisions of the Lombard Municipal Code.

RESPECTFULLY SUBMITTED this 19th day of October, 2015.

PETITIONER:

YTC Mall Owner, LLC
YTC Land Owner, LLC

By: 
Rosanova & Whitaker, Ltd.,
Its Attorney

SUBSCRIBED and SWORN to before me
this 19th day of October, 2015.

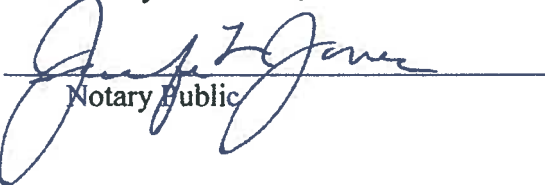

Notary Public



Exhibit List:

Exhibit A: Plat of Subdivision

Exhibit B: Legal Description of YTC Property

Exhibit C: Legal Description of JCP Property

Exhibit D: Legal Description of Yorktown PD Property

Exhibit E: Zoning Map

Exhibit A

Plat of Subdivision

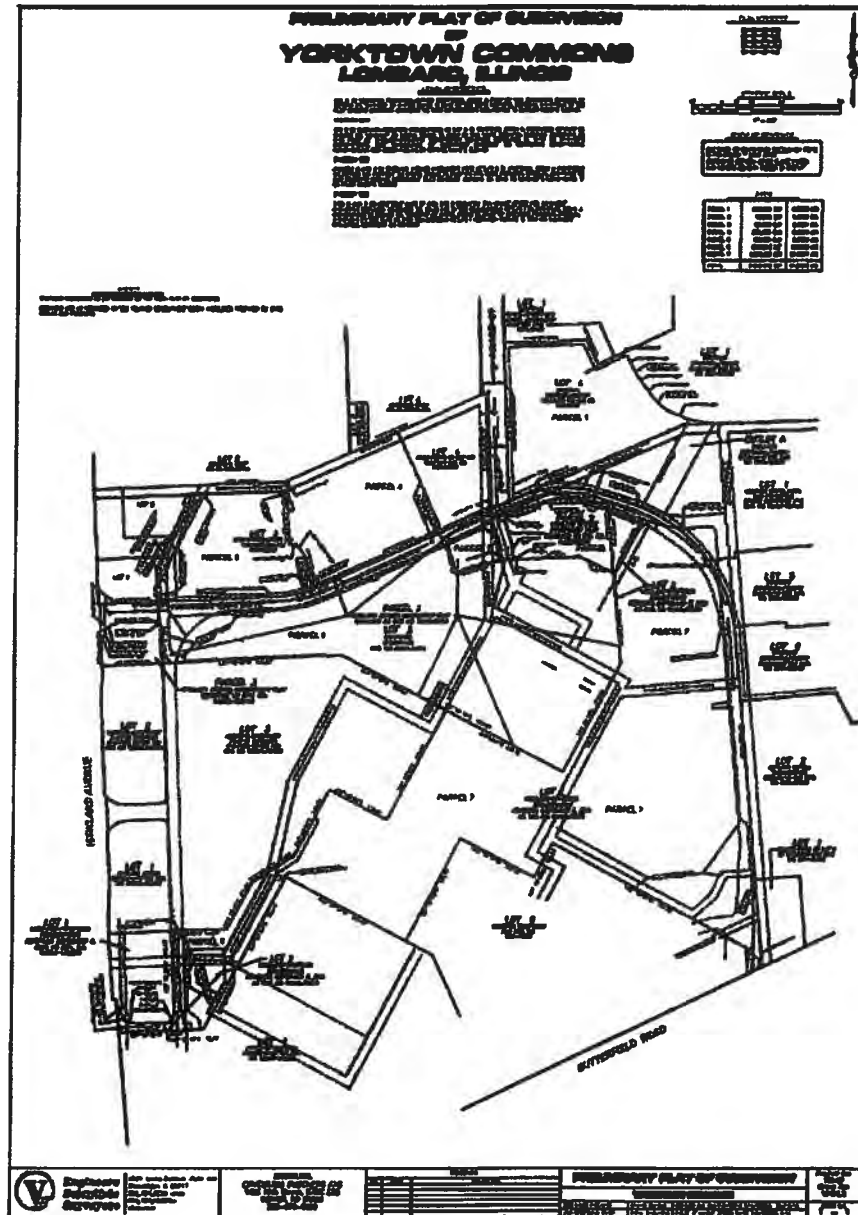


Exhibit B

YTC Property Legal Description

Bamboo Room Parcel:

Tract 1: Parcel 3 in Highland Avenue Assessment Plat of Lot 2 in Yorktown, being a subdivision of Section 29, Township 39 North, Range 11, East of the Third Principal Meridian, according to said Highland Avenue Assessment Plat recorded January 30, 2012 as document R2012-12175, in Du Page County, Illinois

Mall Parcel:

Parcel 1: Lot 2 in Highland-Yorktown Resubdivision of Lot 4 in Yorktown, being a subdivision of part of Section 29, Township 39 North, Range 11, East of the Third Principal Meridian, according to said Plat of Highland-Yorktown Resubdivision recorded September 5, 2006 as document R2006-170876, and certificates of correction recorded as documents R2006-191219, R2006-219358 and R2008-138794,

(except that part of said Lot 2 described as Commencing at a Southwest corner of said Lot 2, being a point on the Northwestern right-of-way line of Old Butterfield Road; thence North 2 degrees 58 minutes 03 seconds West, along a Westerly line of said Lot 2, a distance of 78.22 feet to an angle corner in said Lot 2; thence North 60 degrees 00 minutes 00 seconds West, along the Southwesterly line of Lot 2 aforesaid, 757.443 feet to an angle corner in said Lot 2; thence South 30 degrees 00 minutes 00 seconds West, along a Southeasterly line of Lot 2 aforesaid, 130.252 feet to an angle corner in said Lot 2, said point being also on the Southwesterly line of said Lot 2; thence North 60 degrees 00 minutes 00 seconds West, along said Southwesterly line, 282.665 feet to another angle corner in said Lot 2, said point being also the point of beginning of the parcel of air space herein described; thence North 30 degrees 00 minutes 00 seconds East, perpendicular to the last described line, 17.69 feet; thence South 60 degrees 00 minutes 00 seconds East, 160.98 feet; thence South 30 degrees 00 minutes 00 seconds West, perpendicular to the last described line, 17.69 feet to the point of intersection with the Southwesterly line of Lot 2 aforesaid; thence North 60 degrees 00 minutes 00 seconds West, along said Southwesterly line, 160.98 feet to the hereinabove designated point of beginning, said parcel having as a lower limit a horizontal plane of elevation +758.58 feet (National Geodetic Survey Datum) and having as an upper limit a horizontal plane of elevation +775.58 feet (National Geodetic Survey Datum))

also except that part of the following described tract lying within said Lot 2 described as follows: That part of Lots 2 and 4 in Yorktown, according to the plat thereof recorded September 27, 1968 as document R68-44972, described as follows: Commencing at the Southwest corner of Lot 2 in Pehrson Second Resubdivision of Lots 2 and 3 in the Resubdivision of Lot 6 of Yorktown (aforesaid) according to the plat thereof recorded September 3, 1976 as document R76-62393; Thence North 2 degrees 58 minutes 03 seconds West along the West line of the aforesaid Lot 2 in Pehrson Second Resubdivision 217.86 feet to the point of beginning of the herein described tract; Thence 611.59 feet Northwesterly along the arc of a circle tangent to the last described line convex Northeasterly having a radius of 518.00 feet and whose chord bears North 35 degrees 46 minutes 29 seconds West 575.58 feet; Thence North 70 degrees 36 minutes 56 seconds West along a line tangent to the last described curve 119.25 feet; Thence 265.51 feet Westerly along the arc of a circle tangent to the last described line, convex Northerly having a radius of 368.00 feet and whose chord bears South 88 degrees 42 minutes 54 seconds West 259.79 feet to a point on the Northwesterly line of the aforesaid Lot 2 in Yorktown, said point being 105.99 feet Northeasterly of the Southeast corner of Lot 4 in the Resubdivision of Lot 1 in Yorktown (aforesaid) according to the plat thereof recorded August 3, 1971 as document R71-37751; Thence North 68 degrees 02 minutes 44 seconds East along a line tangent to the last described curve and being the Northwesterly line of the aforesaid Lots 2 and 4 in Yorktown 604.01 feet to a corner in the Northerly line of Lot 4 in Yorktown; Thence South 89 degrees 57 minutes 16 seconds East along the North line of said Lot 4 a distance of 120.00 feet to the Northeast corner of said Lot 1; Thence South 2 degrees 58 minutes 03 seconds East along the East line of said Lot 4 a distance of 722.27 feet to the hereinabove described point of beginning)

also except that part of said Lot 2 in Highland-Yorktown Resubdivision aforesaid described as beginning at a point on the East right of way line of Highland Avenue as dedicated by document R68-10768 and the South line of said Lot 2; Thence Easterly along said South line having an Illinois East Zong Grid bearing

of North 87 degrees 35 minutes 17 seconds East 20.02 feet; Thence North 04 degrees 57 minutes 24 seconds West 49.21 feet; Thence North 29 degrees 57 minutes 24 seconds West 47.32 feet to a point on said East right of way line of Highland Avenue; Thence South 04 degrees 57 minutes 24 seconds East 91.21 feet along said East right of way line to the point of beginning)

all in Du Page County, Illinois

Convenience Center Parcel:

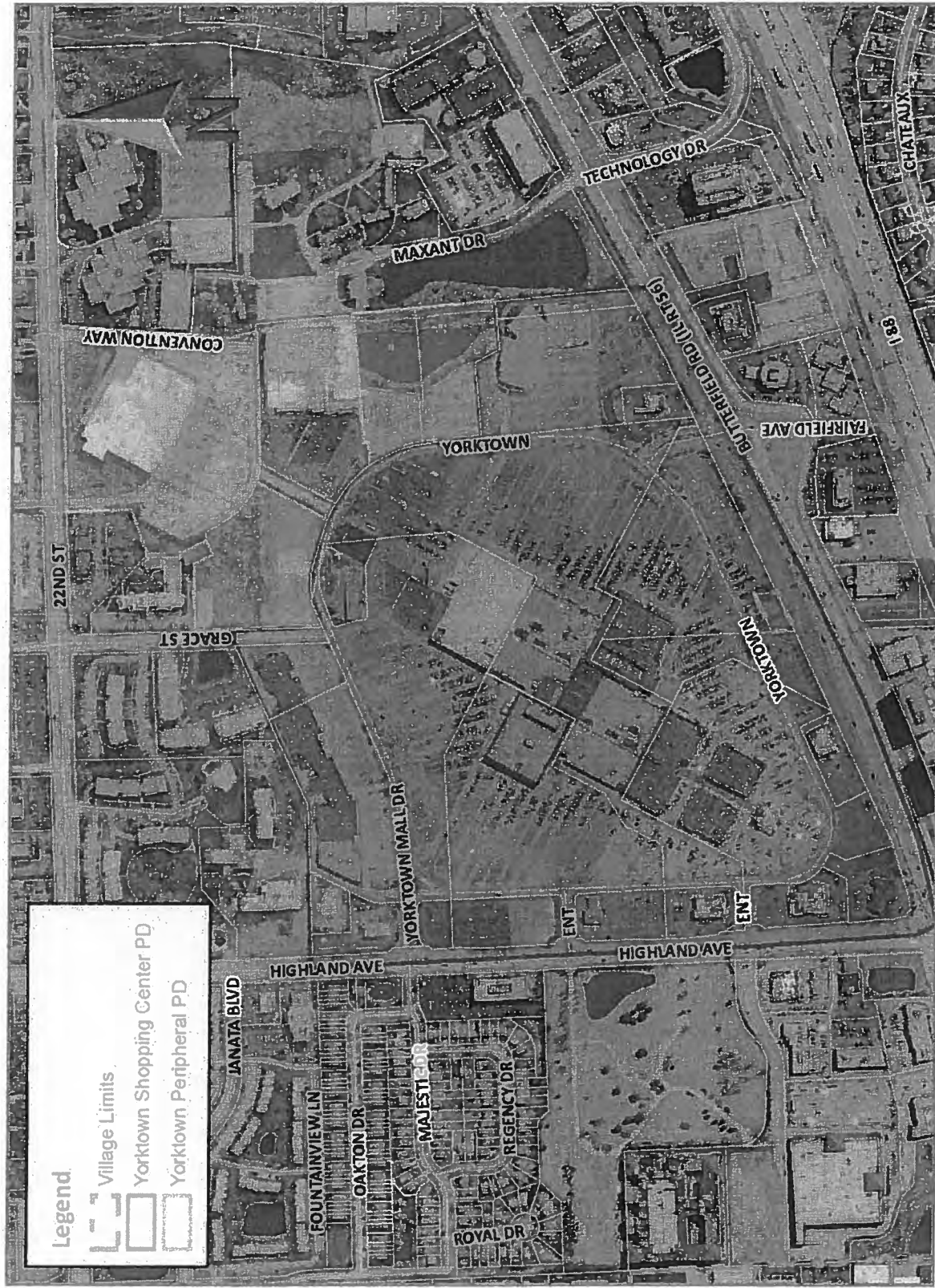
Parcel 1: Lots 3 and 4 (except that part of said Lot 4 dedicated for Grace Street per document R72-57164 recorded September 20, 1972) in the Resubdivision of Lot 1 in Yorktown, being a resubdivision in Section 29, Township 39 North, Range 11, East of the Third Principal Meridian, according to the Plat of said Resubdivision of Lot 1 recorded August 4, 1971 as document R71-37751, in Du Page County, Illinois

Exhibit C

JCP Property Legal Description

Tract 1: Parcel 2 in Highland Avenue Assessment Plat of Lot 2 in Yorktown, being a subdivision of Section 29, Township 39 North, Range 11, East of the Third Principal Meridian, according to said Highland Avenue Assessment Plat recorded January 30, 2012 as document R2012-12175, in Du Page County, Illinois

Exhibit E
Zoning Map



Planned Development Boundaries

Figure 1

**EXHIBIT B – “YORKTOWN COMMONS PLANNED DEVELOPMENT DESIGN GUIDELINES –
AS REVISED BY PLAN COMMISSION AND VILLAGE STAFF”**

12/23/2015

Yorktown Commons

Planned Development Design Guidelines



Credits



KRE YTC Venture LLC

203 Yorktown Center
Lombard, IL 60148

Continuum Partners, LLC

1400 16th Street, Suite 320
Denver, CO 80202
303-573-0050

Torti Gallas + Partners, Inc.

1300 Spring Street, 4th Floor
Silver Spring, Maryland 20910
301-588-4800
www.tortigallas.com

Rosanova & Whitaker, Ltd.

30 West Jefferson Street, Suite 200
Naperville, IL 60540
630-355-4600

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Intent and Applicability

Intent

Applicability

Relationship to Zoning Ordinance

Design Guidelines

General Framework

Supersession of the

Village Code

Applicability to Other Parts of the Village Code

Applicable Zoning

Building Code

Section I. – Intent and Applicability

A. Intent

The Yorktown Commons Planned Development establishes the vision for the parcels located along the northern edge of the Yorktown Shopping Center. The vision is to establish a pedestrian friendly mixed-use neighborhood that acts as a gateway to the Shopping Center from the north.

The Yorktown Commons PD Design Guidelines have been developed and approved in order to ensure the implementation of the vision while also allowing Land Use flexibility.

B. Applicability

The Yorktown Commons PD Design Guidelines are the mandatory development standards approved as a part of the Yorktown Commons PD. All development within the Yorktown Commons PD boundaries is subject to the following:

- ▶ PD Ordinance Regulations;
- ▶ Yorktown Commons PD Design Guidelines;
- ▶ Yorktown Commons Architectural Review Board; and
- ▶ Standard Village Site Plan Approval process.

The Yorktown Commons PD is comprised of four Parcels as depicted in **FIGURE 01**. Collectively, the four parcels comprising the Yorktown Commons PD may be referred to as the Property or the Parcels. Individually, each parcel may be referred to generally as a Parcel or specifically by Parcel number (i.e. Parcel 1).

The Yorktown Commons PD Design Guidelines regulate:

1. Permitted Land Uses;
2. Site layout standards (such as Build-to-Lines, Frontage Occupancy, parking, Block standards, and Lot standards);
3. Site three-dimensional standards (such as Building heights, special urban design features, and frontage types);
4. Street Sections;
5. Open Space; and
6. Architectural Standards.



FIGURE 01: Yorktown Commons PD Location

Section II. – Relationship to Zoning Ordinance and Subdivision Regulations

A. Design Guidelines General Framework

The Design Guidelines are comprised of a Regulating Plan and development standards contained in this document.

B. Supersession of the Village Code

The provisions of these guidelines supersede the development standards of the Village of Lombard Zoning Ordinance, Sign Ordinance, and Subdivision Ordinance. Where these guidelines are silent about any matter relating to Land Use, Building, or development, applicable provisions of the Village Code that do not conflict with these guidelines shall apply.

Section III. – Applicability of Other Parts of the Village Code

A. Applicable Zoning

The Property is zoned Community Shopping District Planned Development. The Property has historically been governed by either the original Yorktown Shopping Center Planned Development (Ordinance 1172) or the Yorktown Peripheral Planned Development (Ordinance 3962). The terms of this Yorktown Commons Planned Development shall hereafter govern. The terms of the Yorktown Commons Planned Development and these Design Guidelines shall supplant and prevail over, in their entirety, the underlying B3 Community Shopping District Regulations. The terms of the Yorktown Commons Planned Development shall prevail over other conflicting provision of the Village's Municipal Code, but other generally applicable provisions of the Municipal Code shall apply where not in direct conflict with the terms of the Planned Development.

B. Building Code

The provisions of the Yorktown Commons Planned Development are not intended, in any way, to modify or amend the provisions of the Building Code or other applicable life safety ordinances and laws. In any case where Building or life safety regulations are in conflict with the Yorktown Commons Planned Development, the Building or life safety regulations shall apply.

Administration

Preliminary Plan

Approval

Final Plan Approval

Interpretations

Severability

Major Changes

Minor Changes

Section IV. – Administration

A. Preliminary Plan Approval

The provisions of these Design Guidelines and associated documents approved pursuant to the Planned Development for Yorktown Commons shall comprise the “Preliminary Plan” for the Property pursuant to Section 155.503(B) of the Village Code. The Preliminary Plan shall provide the owner of the Property the right to develop the Property consistent with the requirements of the Preliminary Plan.

B. Final Plan Approval

The “Final Plan” for the Property or any individual parcel shall be submitted and processed consistent with the requirements of Section 155.503(C)(2) of the Village Code, except follows:

1. There shall be no time limit for submittal of a Final Plan;
2. The Director of Community Development shall determine whether the Final Plan conforms substantially to the Preliminary Plan (i.e. is consistent with the requirement of the Design Guidelines). If the Final Plan substantially conforms to the Preliminary Plan, no public hearing shall be required and the Final Plan shall be approved pursuant to the Site Plan approval process set forth in Section 155.511 of the Village Code. If the Final Plan does not substantially conform to the Preliminary Plan, the Final Plan shall be processed as a major change or minor change, as may be applicable, pursuant to Section 155.504 of the Village Code.

3. Notwithstanding anything herein or in Section 155.511 or 155.503(C)(2) to the contrary, the Plan Commission shall have final authority to approve a Final Plan that substantially conforms to the Preliminary Plan. The Plan Commission approval of a Final Plan shall be subject only to the technical compliance with the applicable requirements of Section 155.507(C) and said approval shall be evidenced by recordation of a Planned Development Plat executed by the chairman of the Plan Commission.

C. Interpretations

If uncertainty exists regarding the interpretation of terms, development standards, architectural standards, or any other regulation contained within these guidelines, the Community Development Director shall determine the intent of the provision at question.

D. Severability

If any section, subsection, sentence, clause, or phrase of this code or its application to any Person or circumstance is held invalid by the decision of any court of competent jurisdiction, the remainder of this code, or the application of the provision to other Persons or circumstances is in effect and shall remain in full force and effect.

E. Major Changes

Major Changes shall be those changes which:

1) are inconsistent with the Vision set forth in Section V of these Design Guidelines; 2) seek approval of a Use not specifically permitted or substantially similar to a Use permitted in Section VI of these Design Guidelines; 3) require a greater than ten percent (10%) modification of the standards set forth in Sections VII, VIII and IX of these Design Guidelines, except that the maximum Building height shall not increase; or 4) require a Deviation or variance from an applicable provision of the Municipal Code.

Major Changes shall require a public hearing before the Plan Commission and ordinance approval by the Village Board pursuant to the following standards:

1. That the establishment, maintenance, or operation of the Major Change will not be detrimental to, or endanger the public health, safety, morals, comfort, or general welfare;
2. That the Major Change will not be injurious to the Uses and enjoyment of other property in the immediate vicinity for the purposes already permitted, nor substantially diminish and impair property values within the neighborhood in which it is to be located;
3. That the establishment of the Major Change will not impede the normal and orderly development and improvement of the surrounding property for Uses permitted in the Yorktown Commons Planned Development;

4. That adequate public utilities, access roads, drainage and/or necessary facilities have been or will be provided;
5. That adequate measures have been or will be taken to provide ingress and egress so designed as to minimize traffic congestion in the public Streets;
6. That the proposed Major Change is not contrary to the objectives of the Yorktown Commons Planned Development; and
7. That the Major Change shall, in all other respects, conform to the applicable regulations of the District in which it is located, except as such regulations may, in each instance, be modified pursuant to the recommendations of the Plan Commission.

Submittal for a Major Change shall be consistent with the requirements of Section 155.507(C).

Approval of a major Change shall satisfy the Final Plan requirements.

F. Minor Changes

Minor Changes are changes which are not Major Changes. A Minor Change shall not require a new public hearing and may be approved by the Director of Community Development after receiving comments and recommendations from the Inter-Departmental Review Committee. The Director of Community Development may elect to process a Minor Change consistent with the Site Plan approval process set forth in Section 155.511 of the Village Code.



Vision

Description of the Project

Section V. – Vision

A. Description of the Project

Today's communities and markets are seeking walkable, sustainable, and healthy communities that provide a diverse mix of uses and a strong identity. Yorktown Center has a vision to reposition itself from an automobile dominated landscape to one that includes a mix of uses and a strong residential component – a neighborhood where one can live, walk along tree-lined streets, spend time in a neighborhood square, and even walk to get a cup of coffee. This new neighborhood extends the existing residential to the north, builds a unique identity with a strong appeal to the non-traditional suburban buyer or renter, while connecting to, and supporting, the economic well-being of the mall. The key elements of the plan are:

1. A new entry from the north along Grace Street with residential buildings lining the street, sidewalks, and parkway trees;
2. Yorktown Ring Road is transformed into a tree-lined residential street proximate to Yorktown Commons;
3. A new public urban space at the intersection of Yorktown Ring Road and Grace Street with placemaking elements, such as retail storefronts, a small plaza, and seating;
4. New residential uses with high quality architecture that contribute to a desirable neighborhood; and
5. New neighborhood open spaces that provide opportunities for walking, passive recreation, and seating.

The illustrative site plans included here are intended to show a few of the many possible development scenarios for the Yorktown Commons PD. Consistent with the vision of creating a high-quality walkable realm, they illustrate site plans and designs which might be built according to the Design Guidelines. They should not, however, be interpreted as representing a specific required site plan or design.

The Regulating Plan and standards create a framework that will ensure implementation of the Vision, but will also allow for flexibility in the development layout and program. On the following three pages possible alternative development scenarios are illustrated. Other configurations are possible and permitted so long as they adhere to the standards of the Guidelines.



Aerial view, looking south, of a possible development scenario for the Yorktown Commons PD

Vision

Sustainability

B. Sustainability

The Yorktown Commons PD provides the following elements to foster environmental sustainability within a neighborhood design context. The sustainable features of the project generally fall into two categories:

1. Smart Location:

- i. Building on previously-developed sites and infill locations uses land efficiently and preserves open space, ecological areas, and agricultural land around cities. It also utilizes existing infrastructure and clusters housing, jobs, stores, and public spaces together. By putting these conveniences within easy reach, public transit, biking, and walking are more feasible and the length of car trips are reduced; and
- ii. The project is located in a way that is sensitive to its natural setting and protects local environmental quality.

2. Neighborhood Pattern and Design:

- i. Streets designed for walkability include Buildings that face the sidewalk and have architectural diversity along their path. Continuous sidewalks, low-speed traffic, and on-street parking provides a buffer between the sidewalk and the street to increase pedestrian comfort and safety. Walking has cross-cutting benefits for public health, environmental sustainability, and neighborhood vitality, and

further unlocks the advantages of neighborhoods with smart locations, a mix of uses, and compact development;

- ii. Parkway trees make pedestrians more comfortable by providing shade and contact with nature. Native and drought-resistant species should be favored. Monocultures should be avoided in order to provide ecological diversity and resistance to diseases;
- iii. The proposed mix of residential and commercial land uses has many benefits. The sustainable and diverse uses of a blended neighborhood tend to support each other and reinforce a sense of neighborhood character, while decreasing the need to travel long distances for goods, services, or work. This reduces car trips and facilitates walking, which contributes to health and fitness;
- iv. Connections for pedestrians, cyclists, and vehicles – both within the neighborhood and to surrounding areas – are essential. Bike parking near transit stops is encouraged. Connections to existing public transportation are safe and pedestrian friendly; and
- v. Local housing near public transit increases the likelihood that people will take transit or walk rather than drive. It offers an alternative to automobile use and captures many of the other benefits of infill development.



View looking West down Yorktown Ring Road of a possible development scenario.

Vision

Illustrative Scenario 1

This scenario illustrates multi-family in the north-east quadrant and town houses in the north-west quadrant. Additional streets would be added in this scenario that have sidewalks and parkway trees where homes front the street and parking is relegated to the back to create a pedestrian friendly environment. A common green is dedicated as a recreational amenity for townhouse residents.





Illustrative Scenario 2

This scenario illustrates multi-family in both the north-east quadrant and north-west quadrant. In this scenario the linear green space to the east of Grace Street continues west of Grace Street to a green space leading to Yorktown Ring Road creating a pedestrian loop that alternates between urban Streetscape, courtyards, and linear greens.

Vision

Illustrative Scenario 3

This scenario illustrates multi-family in the north-west quadrant and townhouses in the north-east quadrant. From the north-west pedestrian loop and Grace Street, a formal linear green space leads into the north-east quadrant, terminated by a garden structure. The open space changes character into a more informal, natural landscape and connects back out to the street on the east. Additional interior streets and alleys allow the buildings to form and shape the open space into a beautiful, pedestrian-friendly realm.





Possible development scenario with a View of Plaza at corner of Yorktown Ring Road and Grace Street

Land Use

Permitted Uses

Required Uses

Accessory Uses

Density

Home Occupations

Section VI. – Land Use

A. Permitted Uses

The underlying zoning for the Yorktown Commons PD is B3 Community Shopping District. The permitted Uses for Yorktown Commons shall be:

1. All permitted Uses in the B3 Community Shopping District (See Section 155.415(B))
2. Residential, including the following types:
 - i. Single-Family
 - ii. Two-Family
 - iii. Townhouse / Attached Single-Family
 - iv. Stacked Townhouse
 - v. Multi-Family (fee simple, condominium, and/or rental)
3. Hotel.
4. Daycare as an Accessory Use.
5. Mixed-Use Buildings that incorporate any of the Uses noted in this Land Use section shall be permitted.

B. Required Uses

1. Retail shall be required along Building faces where shown on the Regulating Plan and as illustrated in **FIGURE 02**.

C. Recommended Uses

1. Street-activating Uses – such as Retail, leasing offices, club rooms, fitness rooms, lobbies, and the like – are encouraged on the Ground floor of Multi-Family Buildings, if present, where illustrated on **FIGURE 02**.

D. Conditional Uses

1. All Conditional Uses in the B3 Community Shopping District, except where Conditional Uses enumerated in the zoning ordinance have been specifically approved as permitted Uses as part of the Yorktown Commons PD pursuant to Section VI(A) of these Design Guidelines.

E. Accessory Uses

1. Accessory Uses shall comply with all development standards of the Yorktown Commons PD Design Guidelines

F. Density

1. The maximum number of Residential Dwelling units shall be 970.
2. The maximum area of Retail Uses shall be 28,000 square feet.
3. The maximum area of other commercial Uses shall be 100,000 square feet.
4. The hotel maximum density is 150 rooms with no limit on ancillary Retail associated with the hotel.

G. Home Occupations

Home occupations shall be permitted according to Section 155.211 of the Village Code of Ordinances.

Legend

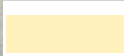


	All Uses per Section VI
	Retail Storefront Required on Ground Floor
	Street Activating Uses Encouraged on Ground Floor



FIGURE 02: Land Use 17

Regulating Plan

Establishment

Public and Private

Street Alignments

Section VII. – Regulating Plan

A. Establishment

1. The Regulating Plan establishes the framework for the neighborhood by establishing:
 - i. Public and private Street alignments;
 - ii. Required Open Space;
 - iii. Build-to-Lines;
 - iv. Frontage Occupancy;
 - v. Special Urban Design Features;
 - vi. Public and Private Front Yard Types;
 - vii. Building Height Requirements; and
 - viii. Required Retail Storefront Locations.
2. The Regulating Plan is shown in **FIGURE 03**. For ease of use, each Regulating Plan element is described and illustrated in the following pages.
3. The lines and boundaries depicted in the Regulating Plan are based on, and dimensioned from, the Street Right-of-way or Property line as described on the following pages.

B. Public and Private Street Alignments

1. Public and private Street alignments and locations shall be consistent with the Regulating Plan and additional Streets or drives shall be consistent with the standards contained in these guidelines.

Legend

---	Property Line
—XX'—	Build-to-Line
←X%-X%→	Frontage Occupancy
X Max	Maximum Building Height
Type	Frontyard Type
	Required Retail Frontage
	Prominent Corner Element
	Massing and Articulation Rules Apply
	Facade Standards Apply

	Open Space Required at Location Shown
	Open Space Required Somewhere in Block

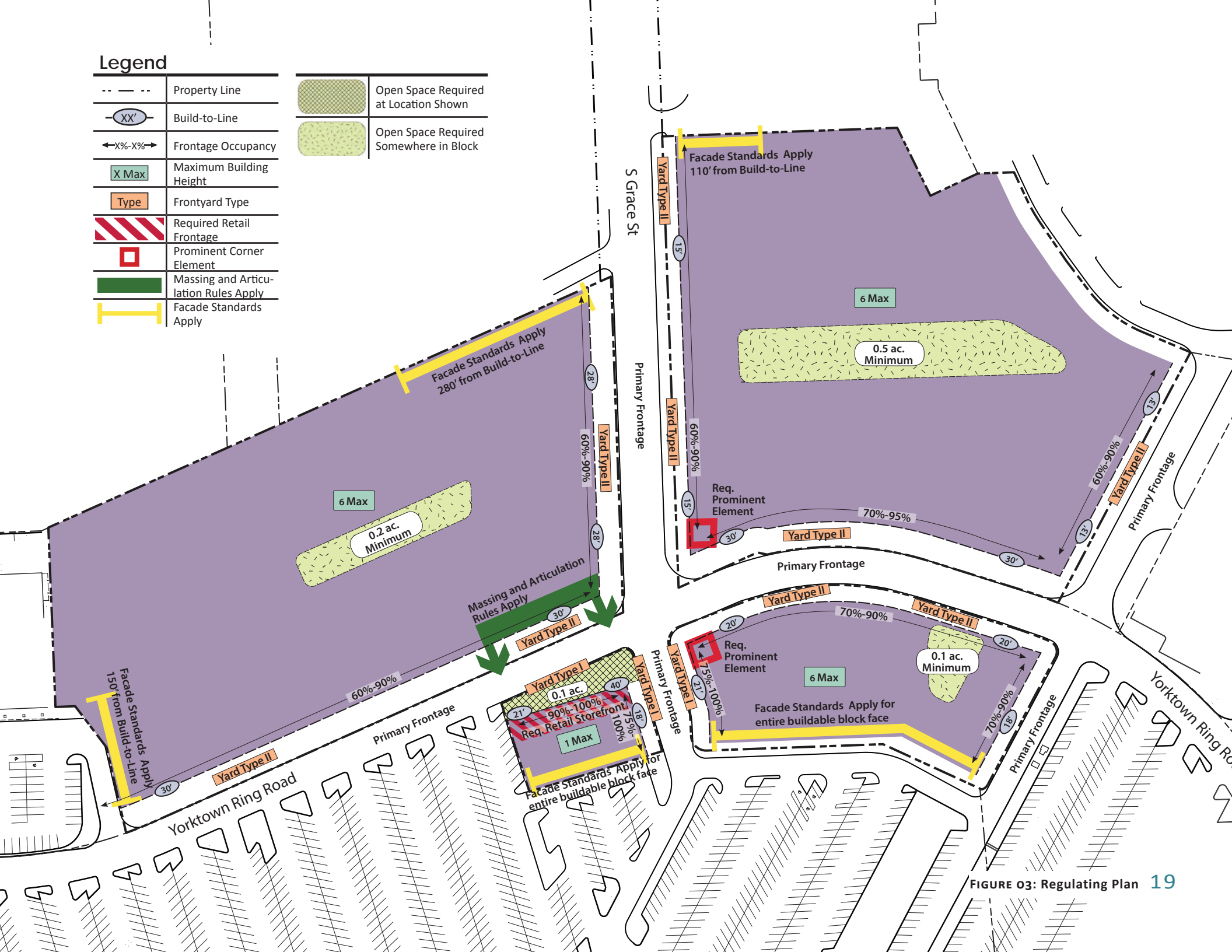


FIGURE 03: Regulating Plan 19

Regulating Plan

Open Space

Required Open

Spaces

C. Open Space

1. The required minimum Open space shall be 12.5% of the total site area.
2. Open space shall be defined as land or water that is open to the air and used for active or passive recreation, including:
 - i. Lawn;
 - ii. Garden;
 - iii. Courtyard;
 - iv. Square;
 - v. Plaza;
 - vi. Park;
 - vii. Linear Green;
 - viii. Trails and Pedestrian Paths;
 - ix. Water Features and Landscaped Stormwater Management Features; and
 - x. Garden Structures, or similar.
3. Open space shall not include private Townhouse and/or single-family Yards.
4. Usable Open space provided on a roof or a podium may be counted.
5. Open space may be retained as part of a parcel where ownership is not divided, but shall be conveyed to an owners' association where ownership of a parcel will be divided. In lieu of dedication to an owners association, the developer may convey Open space to a public entity with the consent of the Village.
6. There shall be no separate requirements for Common Open Space as defined in the Village Code and the provisions of Section 155.510 shall not be applicable to the Yorktown Commons Planned Development.

D. Required Open Spaces

1. In accordance with the intent and Vision of the Yorktown Commons Planned Development, the creation of a specific Open space and additional Open spaces shall be required.
2. Required Open spaces are shown on the Regulating Plan and in **FIGURE 04** in two categories:
 - i. In the required size and specific location as shown in the Regulating Plan; and
 - ii. In the required minimum quantity (may be composed of multiple Open spaces) but not specifically located in the Regulating Plan.
3. The Open space may include walkways, Courtyards, and other essential hardscape features that shall be counted toward the calculation of the total Open space, but on Parcels 1 and 4, it is anticipated that the Open space for each Parcel shall be predominately green vegetated areas.
4. Additional Open space located in the interior of a Block and behind Buildings with no frontage on a Thoroughfare shall be permitted.
5. Additional Open space shall not occur at Block corners.

Legend



Space Required at Location Shown



Minimum Quantity Required;
Location within Block Not Specified



FIGURE 04: Open Space 21

Regulating Plan

Build-to-Line

E. Build-to-Line

1. Buildings and their Primary Facades shall be located along the Build-to-Line for all stories of a Building (see **FIGURE 07**).
2. In order to provide for Facade articulation, Building Facades may step back a maximum of two (2) feet from the Build-to-Line for a maximum of 70% of the Facade and may step forward a maximum of one (1) foot beyond the Build-to-Line for a maximum of 25% of the Facade along a Block Face.
3. Where site geometry creates corners with acute angles, Buildings may chamfer or pull back from the corner to create reasonable footprints.
4. Accessory Uses, Accessory Buildings, and accessory Structures on corner Lots.
 - i. Detached Garages shall be built at the Build-to-Line on the Secondary Street;
 - ii. Privacy Fences shall be built at the Build-to-Line along the Secondary Street; and
 - iii. Except for approved projections, all other Accessory Uses, Buildings, and Structures shall be built a minimum of two (2) feet back from the Build-to-Line.
5. If a Build-to-Line Encroaches into a Clear Sight Area, then the Build-to-Line along each Block Face shall start at the edge of any Clear Sight Area.
6. Projections beyond the Build-to-Line
 - i. Porches, stoops, balconies, second-Story bay windows, towers, arcades, Awnings, Marquees, and signs may Encroach beyond the Build-to-Line, within the private front Yard, to a maximum dimension of ten (10) feet (see **FIGURE 05**);
 - ii. In Commercial front Yard types, at-Grade projections with raised platform shall not be permitted to Encroach beyond the Build-to-Line (see **FIGURE 06**); and
 - iii. The minimum vertical clearance of projections shall not be less than eight (8) feet above Grade over a walkway, or less than fifteen (15) feet above Grade over a driveway, interior access drive, or Alley.

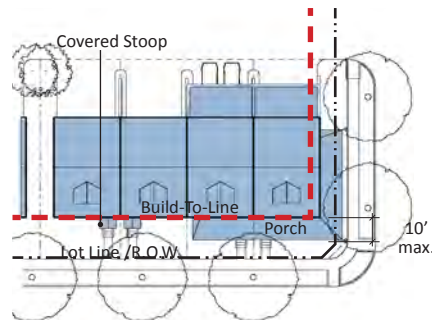


FIGURE 05: Build-To-Line, Residential Building

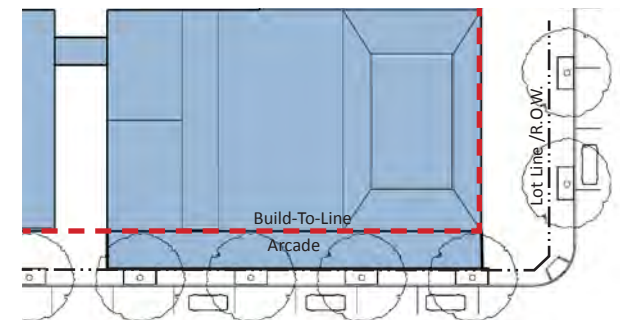

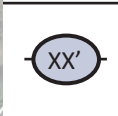


FIGURE 06: Build-To-Line, Mixed-Use Building

Legend

	Build-to-Line
	Dimension from ROW / Property Line to the Build-to-Line

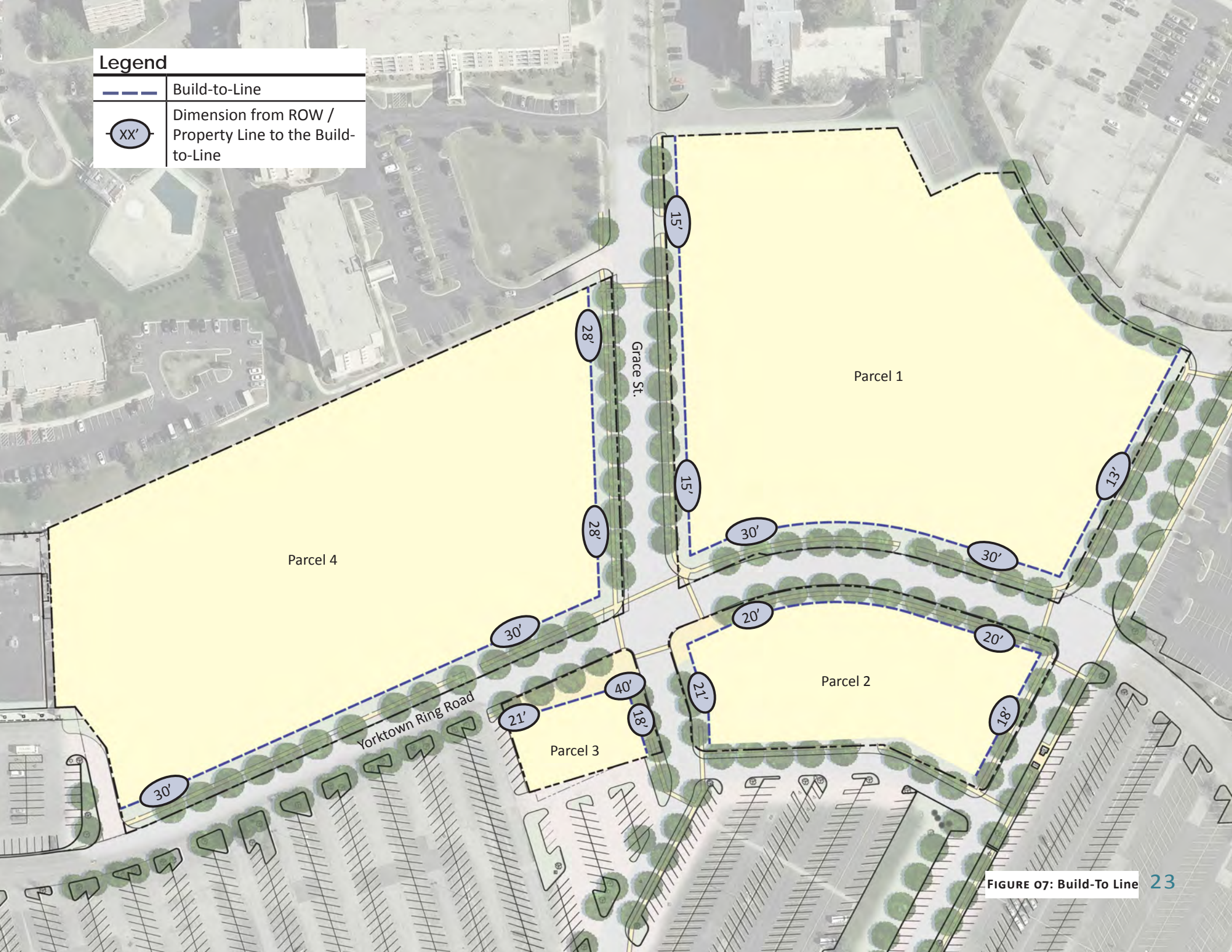


FIGURE 07: Build-To Line 23

Regulating Plan

Special Conditions for Single-family, Two-family, and Townhouse Buildings

7. Special Conditions for Single-family and Two-family Dwellings:
 - i. On the concave side of a curving Street, Buildings—or groups of attached Buildings—shall meet the Build-to-Line at their corners (**FIGURE 08**);
 - ii. On the convex side of a curving Street, Buildings—or groups of attached Buildings—shall meet the Build-to-Line at the center of the Facade (**FIGURE 09**);
 - iii. On corners not equal to 90 degrees, Buildings shall have one of their Facades located on the Build-to-Line and intersect the other Build-to-Line at the corner (**FIGURES 10 AND 11**); and
 - iv. In all cases listed above, the length of Facade projected out to the Build-to-Line shall be measured for purposes of Frontage Occupancy requirements.

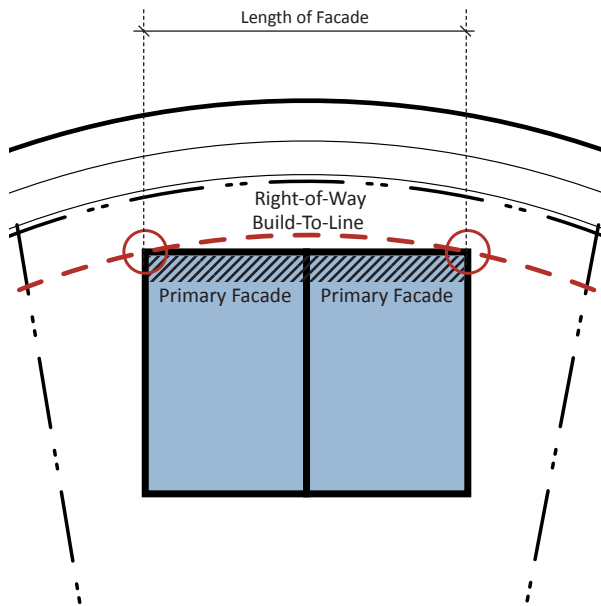


FIGURE 08: Build-to-Line on Curved Street, Concave

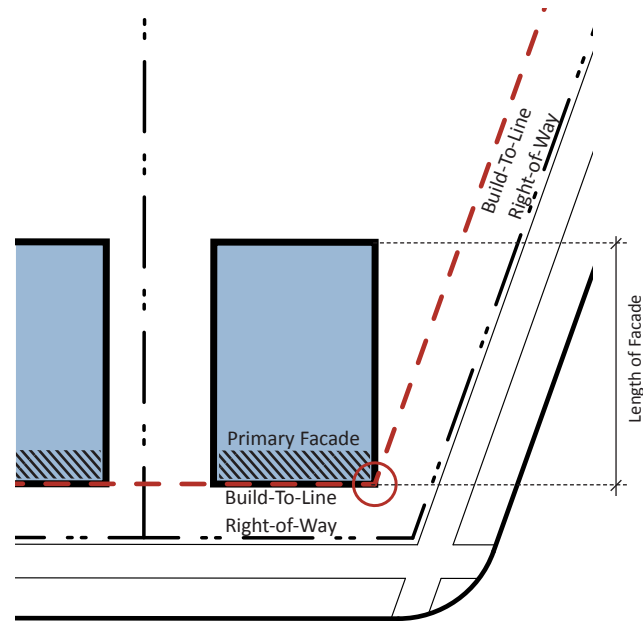


FIGURE 10: Build-to-Line on Corners Greater than 90°

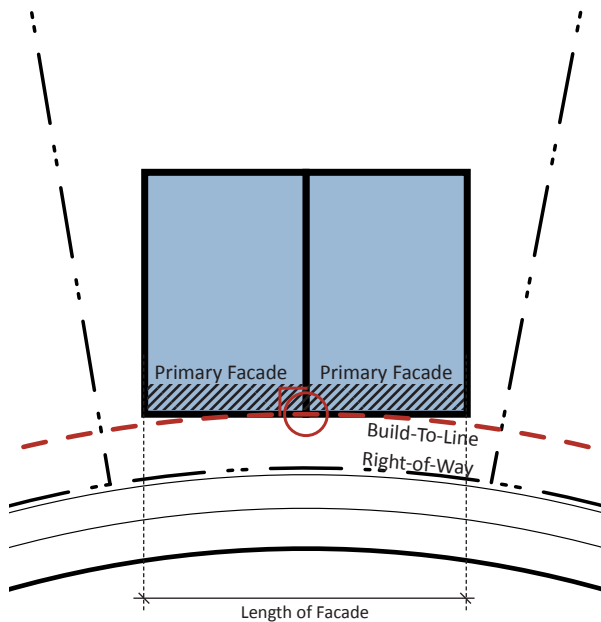


FIGURE 09: Build-to-Line on Curved Street, Convex

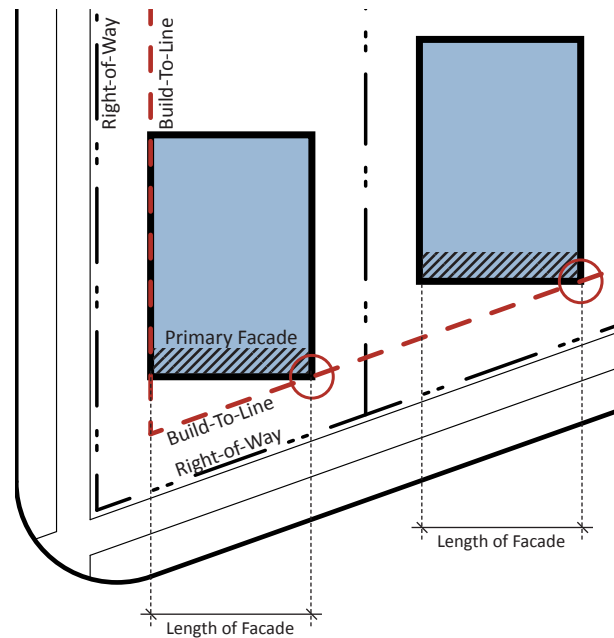


FIGURE 11: Build-to-Line on Corners Less than 90°

Regulating Plan

Frontage Occupancy

F. Frontage Occupancy

1. Frontage Occupancy is the minimum length of Primary Facade that shall be built along a Build-to-Line (see **FIGURE 13**).
2. Frontage Occupancy requirements shall apply to all stories of a Building (excluding occupied or unoccupied space in roofs).
3. Buildings and Primary Facades shall be located at Block corners. The permitted void (what is left after subtracting the minimum Frontage Occupancy from the length of the Block Face) shall not be located within twenty (20) feet of a Block corner.
4. Buildings located at Block corners shall have two Primary Facades, one facing each public or private Street (see **FIGURE 12**).
5. Detached Garages shall count toward Secondary Frontage Occupancy requirements when located on corner Lots.
6. Buildings that face a public or private Street with a Courtyard above the first floor that is no more than seventy (70) feet wide shall be permitted to count the Courtyard width along the Build-to-Line as occupied Frontage.
7. Multi-Family Buildings that face a public or private Street with a Courtyard no more than 70' wide shall be permitted to count the Courtyard width along the Build-to-Line as occupied Frontage.
8. Buildings that form a Courtyard facing a public or private Street on the first floor are prohibited along Retail front Yards.
9. Single-family detached and two-family Building Frontage Occupancy shall be regulated by the Lot standards instead of the Regulating Plan.
10. Lots which do not occupy an entire Block Face shall meet the minimum Frontage Occupancy requirements within the individual Lot.
11. Where Retail is required in the Regulating Plan, Buildings shall be designed to accommodate Retail Uses on the Ground floor. However, other Uses may occupy the Retail space, provided that the changes made to accommodate such other Uses shall in no way prevent a transition to Retail Use.
12. Mixed-Use, multi-family, and Townhouse Buildings facing curving Streets may be faceted to follow the curve along the Build-to-Line. Facades located within nine (9) feet of the Build-to-Line in this condition shall be counted as occupied Frontage.

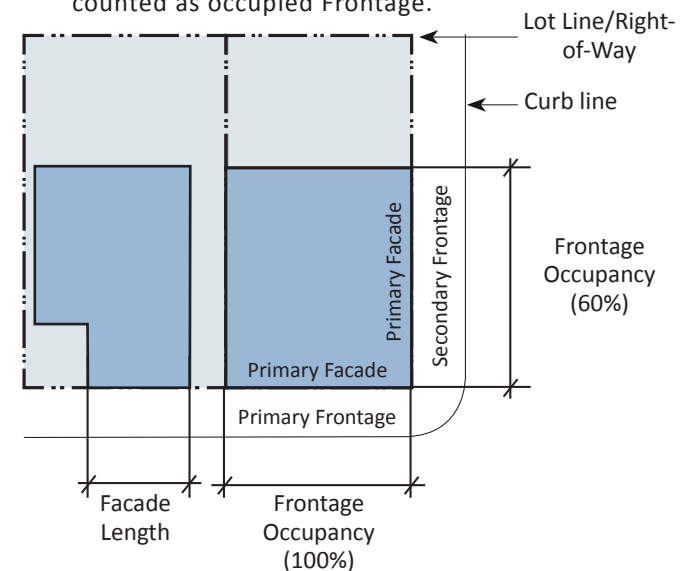

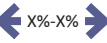


FIGURE 12: Example of Frontage Occupancy at Block Corners

Legend

	Build-to-Line (see pg. 23)
	Occupied Percentage of Build-to-Line (Range)

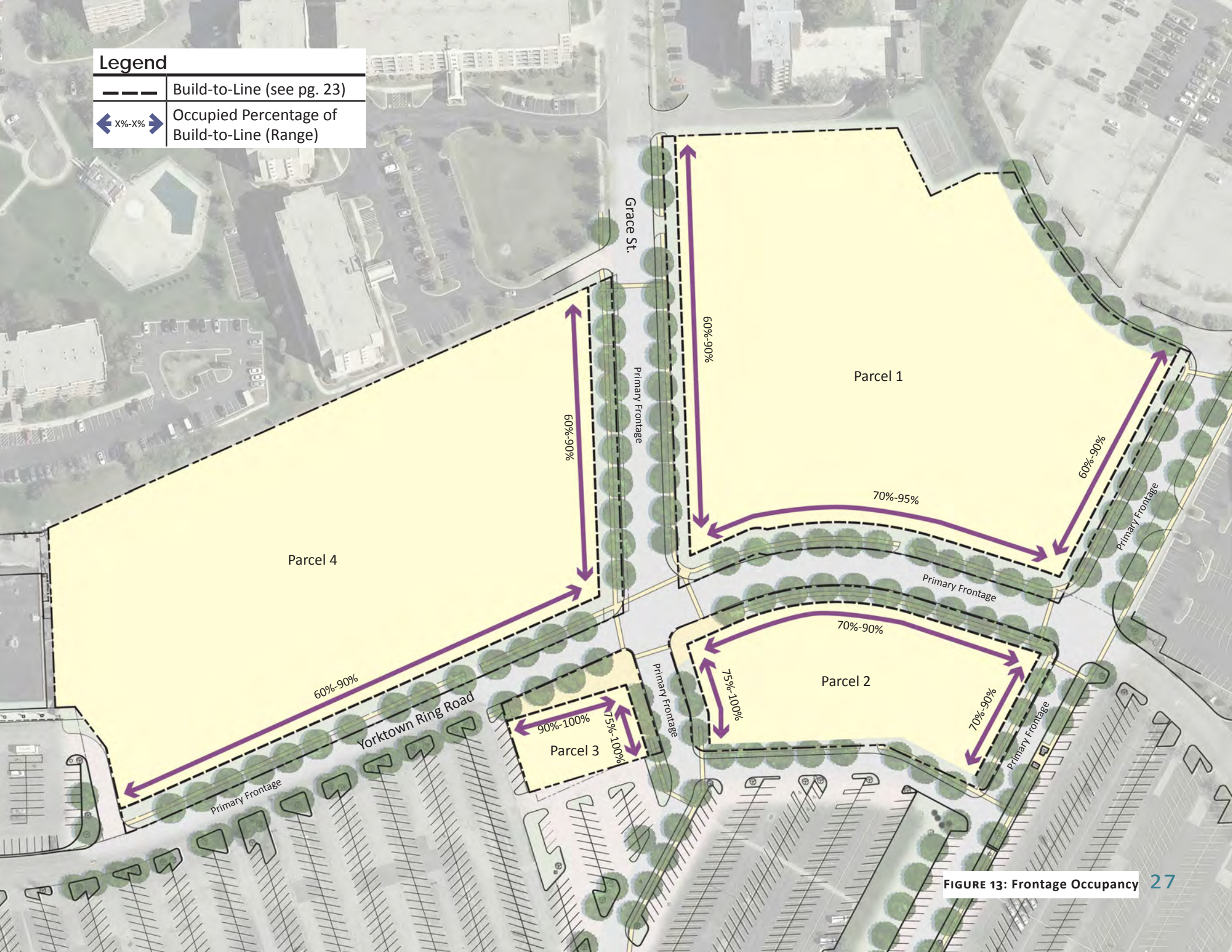


FIGURE 13: Frontage Occupancy 27

Regulating Plan

Building Height

G. Building Height

1. Maximum permitted Building heights, in number of stories, shown in **FIGURE 14**.
2. Stories permitted are in addition to a Basement, if present.
3. The maximum height of a Story on the first floor, as measured at the horizontal midpoint of the Facade, shall not exceed thirty (30) feet for non-Residential or Mixed-Use Buildings, or twenty-five (25) feet for Residential Buildings. All other floors shall not exceed fourteen (14) feet in height.
4. Maximum Building height shall not exceed one hundred (100) feet.

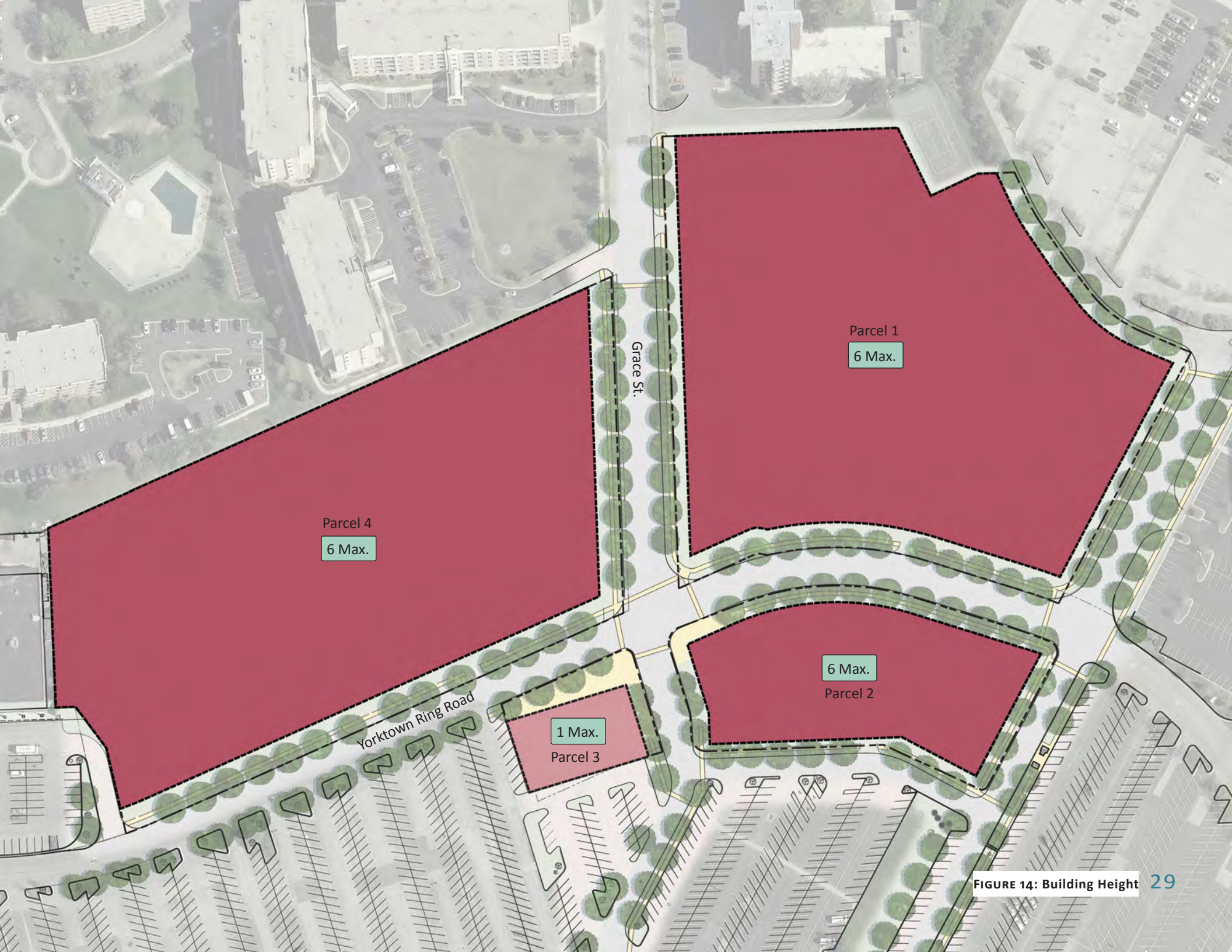


FIGURE 14: Building Height 29

Regulating Plan

Special Urban Design and Architectural Features

H. Special Urban Design and Architectural Features

1. **FIGURE 19** represents the urban design and architectural features that shall be required.
2. Vista Terminations:
When Lots or Buildings are situated such that the Centerline of a Street or Open space intersects with said Lot or Building, the Building Facade shall be located and designed as a Vista Termination, i.e., designed to visually attract a viewer's attention at the end of a visual axis and designed to terminate a view with a significant architectural feature such as a tower, Corner Tower, symmetrical Facade centered on a visual axis, an architecturally embellished entry, or similar distinctive architectural devices.
3. Building design shall incorporate special urban design features as noted in the Regulating Plan. Special urban design features include:
 - i. Prominent Corner Element (**FIGURE 15**);
 - ii. Symmetrical Facade on Axis of Street (**FIGURE 16**);
 - iii. Prominent Building Element on Axis (see **FIGURE 17**);

- iv. Composition, Massing, and Articulation of Building (or Buildings) Correlates and Responds to Public Space. For example, a Multi-family Building could have a section of Facade that projects or recesses beyond the typical Facade line for the length of the corresponding space, or a group of Townhouses the length of the corresponding space could be designed to look like a single building or all be of the same material and color (**FIGURE 18**); and
- v. Secondary Building Facades design to Primary Facade Standards (doors not required).

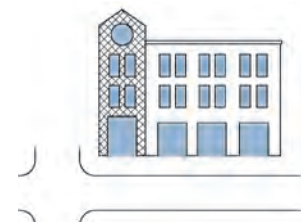


FIGURE 15: Example of Prominent Corner Element



FIGURE 16: Symmetrical Facade Element on Axis



FIGURE 17: Prominent Building Element on Axis

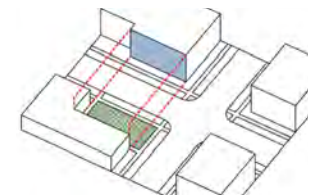


FIGURE 18: Building Massing and Articulation Correlates to Public Space

Legend





	Prominent Corner Element
	Axis of Public Space
	Massing and Articulation Must Correlate to Public Space
	Secondary Facades designed to Primary Facade Standards



FIGURE 19: Architectural Features 31

Regulating Plan

Public and Private Front Yards

I. Public Front Yards and Private Front Yards

1. A public front Yard is the space that occurs between the Street curb and the Right-of-way line (i.e., the front Lot line). Where existing parcel/Lot lines are located at the curb, the public front Yard shall be between the Street curb and back of sidewalk.
2. A private front Yard is the space that occurs between the public Right-of-way (i.e. the front Lot line) and the Primary Building Facade. Where existing parcel/Lot lines are located at the curb, the private front Yard shall be from the back of the sidewalk to the Primary Building face.
3. Public front Yard and private front Yard types shall be applied as designated in the Regulating Plan for required Streets.
4. Public front Yard and private front Yard types for additional Streets shall be selected from the types listed below.
5. Front Yard types are established as:
 - i. Tree Pit and Hardscape;
 - ii. Tree Lawn and Hedge; and
 - iii. Tree Lawn and Stoop
6. The description of front Yard types are as shown in **FIGURES 20 – 22**.
7. The location of require front Yard types are shown on **FIGURE 23**.
8. Hardscape front Yards shall be implemented with a consistent paving material from curb to Building face appearing as a single sidewalk.

FRONT Yard Type I. – Tree Pit and Hardscape

The tree pit and hardscape front Yard type shall provide:

- a. A hardscape surface from curb to Building face designed to appear as a single sidewalk;
- b. A tree pit with a minimum size of 4' long and 4' wide; and
- c. Parkway trees shall be planted as required in Section XIII(F)(1).

FRONT Yard Type II. – Planting Strip and Hedge

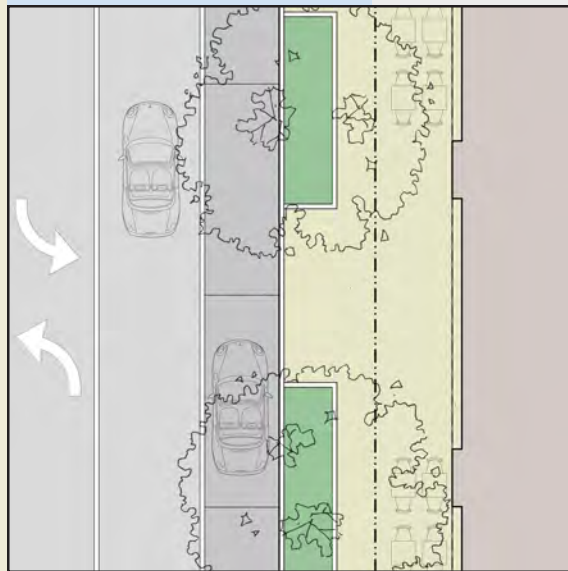
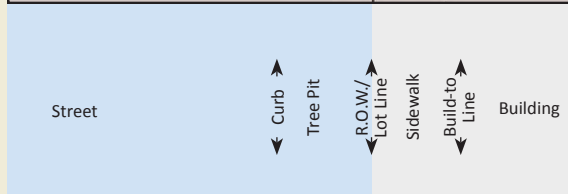
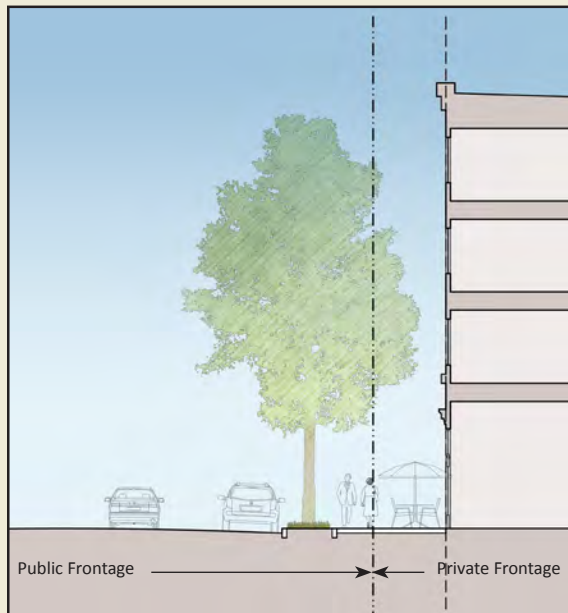
The tree lawn and Hedge front Yard type shall provide:

- a. A 5' minimum tree lawn, immediately adjacent to the curb;
- b. A 5' minimum sidewalk;
- c. A Hedge, immediately adjacent to the sidewalk, with 24" spacing at time of planting with a maintained height of 36" and maintained as a solid screen and not pruned as individual plants; and
- d. Parkway trees shall be planted as required in Section XIII(F)(1).

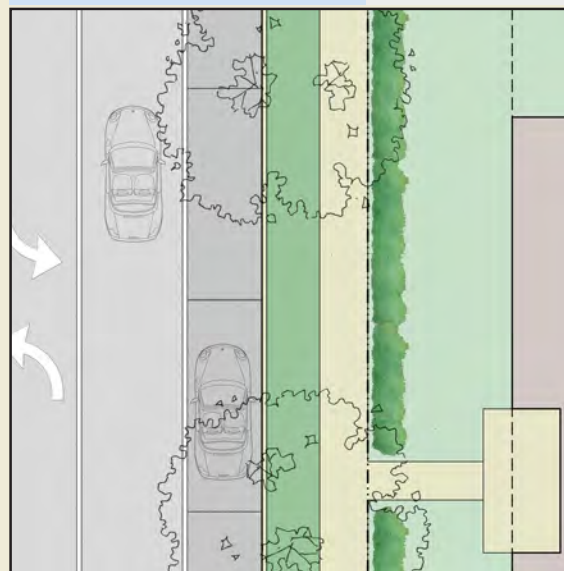
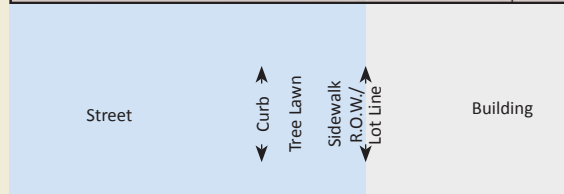
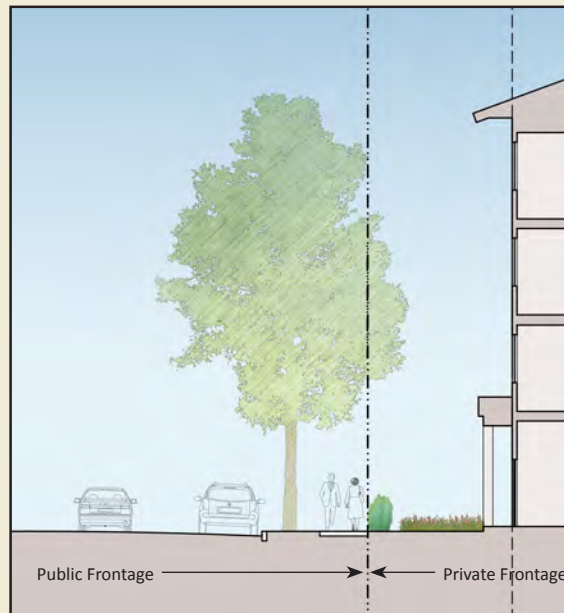
FRONT Yard Type III. – Tree Lawn and Stoop

The tree lawn and lawn front Yard type shall provide:

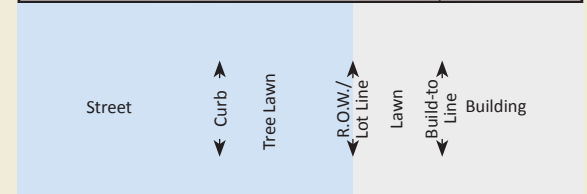
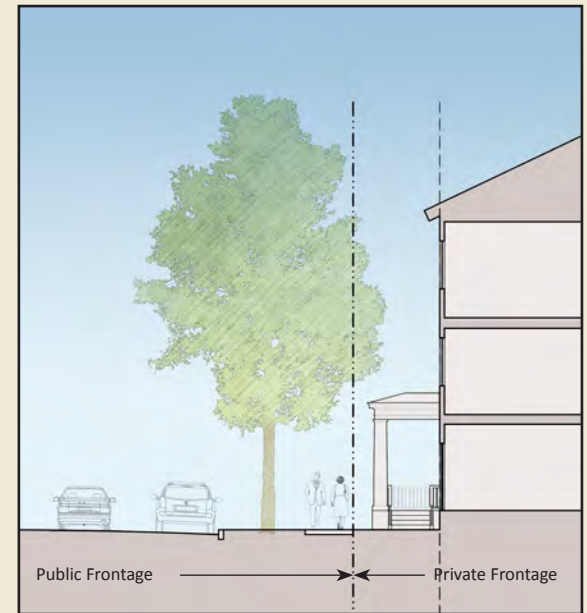
- a. A 5' minimum tree lawn, immediately adjacent to the curb;
- b. A 5' minimum sidewalk; and
- c. Parkway trees shall be planted as required in Section XIII(F)(1).



**FIGURE 20: Front Yard Type I. –
Tree Pit and Hardscape**



**FIGURE 21: Front Yard Type II. –
Planting Strip and Hedge**



**FIGURE 22: Front Yard Type III. –
Tree Lawn and Stoop**

Regulating Plan

Public and Private

Front Yards

9. Front Yard element minimum dimensions:
 - i. Sidewalk – 5' min.;
 - ii. Tree Lawn – 5' min.;
 - iii. Sidewalk contiguous with curb – 6';
 - iv. Tree Pit – 4' min. width and length; and
 - v. Hedge – 24" spacing at time of planting with a maintained height of 36" and maintained as a solid screen and not pruned as individual plants.
10. Outdoor dining areas, including temporary tables, chairs, and umbrellas, shall be permitted. Outdoor dining areas may Encroach into the public Right-of-way. The following regulations shall also apply:
 - i. Outdoor dining areas shall be permitted only along the Business' tenant bay or storefront Facade; and
 - ii. Shall not block an entrance or exit to or from the Business or Building.
11. Outdoor display or sales of merchandise shall be permitted provided a minimum usable sidewalk width of four (4) feet is maintained. Outdoor display or sales of merchandise may Encroach into the public Right-of-way. The following regulations shall also apply:
 - i. Outdoor display or sales of merchandise shall be permitted only along the Business' tenant bay or storefront Facade;
 - ii. Shall not block an entrance or exit to or from the Business or Building;
 - iii. Shall not exceed 10 percent (10%) of the Gross Floor Area of each non-related and separately operated Use; and
 - iv. Shall be permitted only during the hours of operation of the Business and shall be removed at the close of each Business day.
12. Temporary commercial signs (i.e. sandwich board, etc.) shall be permitted during operating hours provided they comply with the Lombard Sign Ordinance.

Legend



Front Yard Type I:
Tree Pit and Hardscape



Front Yard Type II:
Planting Strip and Hedge



Front Yard Type I or II



Front Yard Type III:
Tree Lawn and Stoop*

**To be determined at time of site
plan development.*



FIGURE 23: Front Yard Types 35

Regulating Plan

Street Sections

J. Street Sections

To facilitate bicycle and pedestrian connections to the Yorktown Commons PD and Yorktown Shopping Center, Grace Street—from E 22nd Street in the North to the Yorktown Ring Road in the south—shall be reconfigured to include on-street parking and bike lanes. The typical condition is illustrated in **FIGURE 24**; intersection designs at E 22nd Street and Yorktown Ring Road will vary.

In the case of a parcel being developed with a single-family, two-family, or townhouse program, all units shall be arranged facing either a Street (public or private) or an Open space, and shall be accessed from an Alley or rear drive. New Streets shall follow the standards listed in this section as appropriate to the particular layout.

The sections for new interior Streets are:

1. Two-way Traffic, Park One Side (**FIGURE 25**);
2. Two-way Traffic, Park Two Sides (**FIGURE 26**);
3. Alley (**FIGURE 27**); and
4. Center Green, Park One Side (**FIGURE 28**)

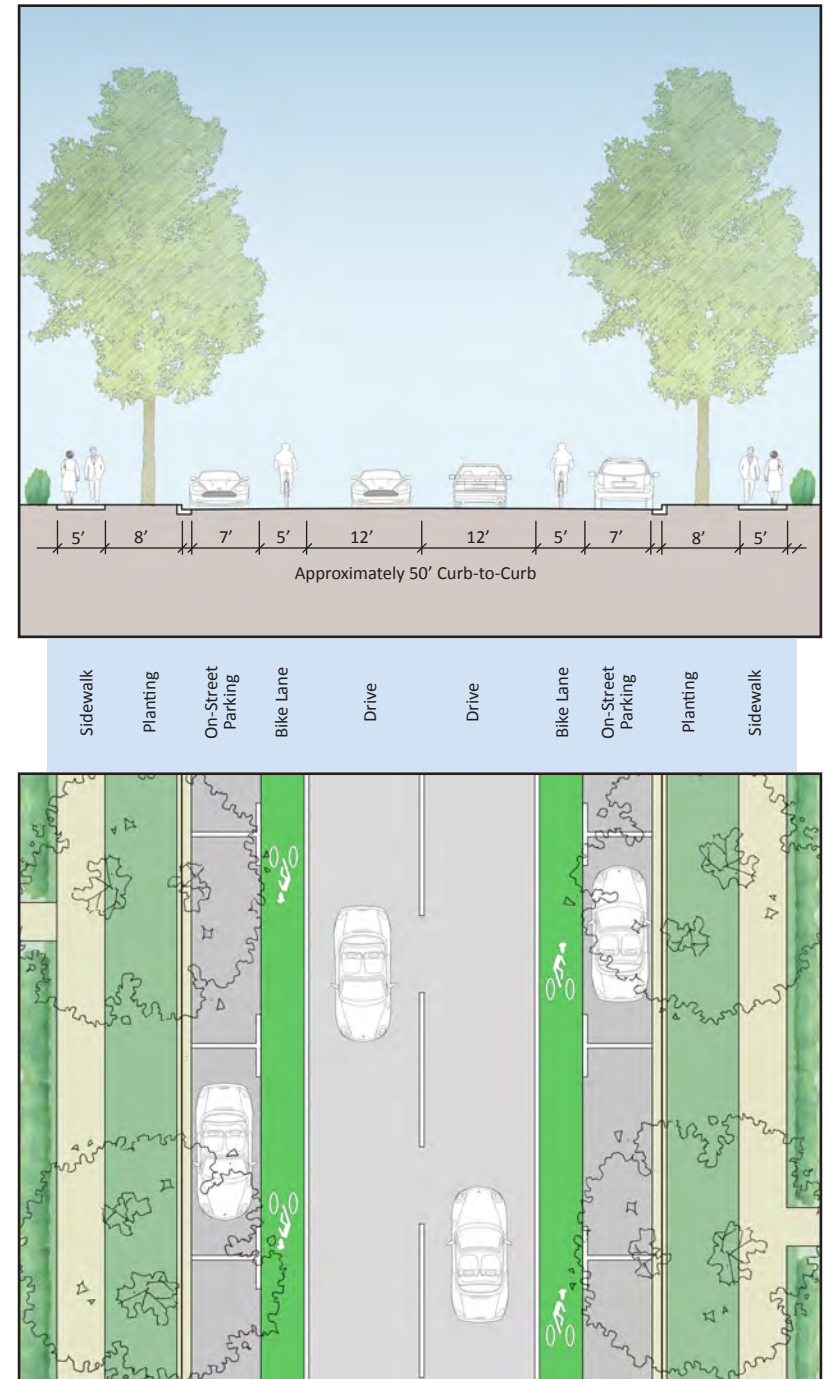


FIGURE 24: Street Sections – Illustrative Typical Section of Grace Street

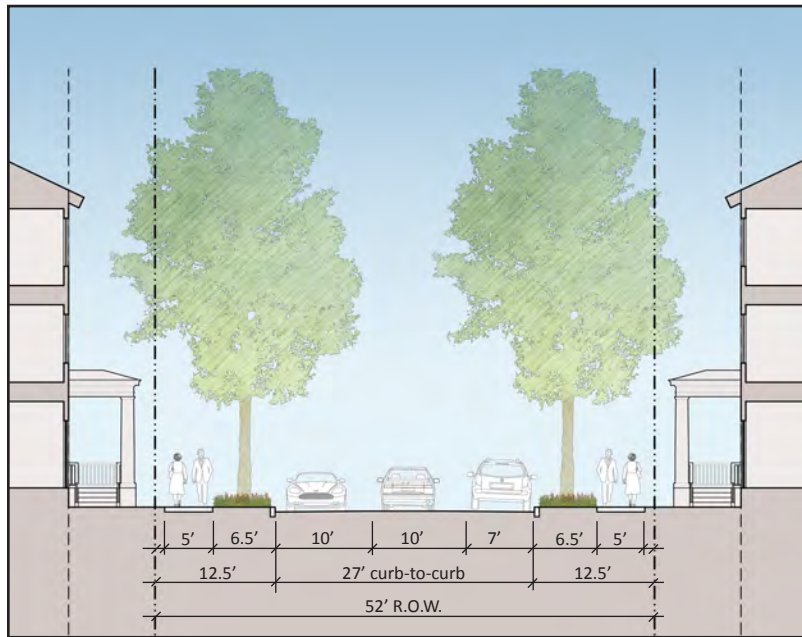


FIGURE 25: Street Sections – Park One Side

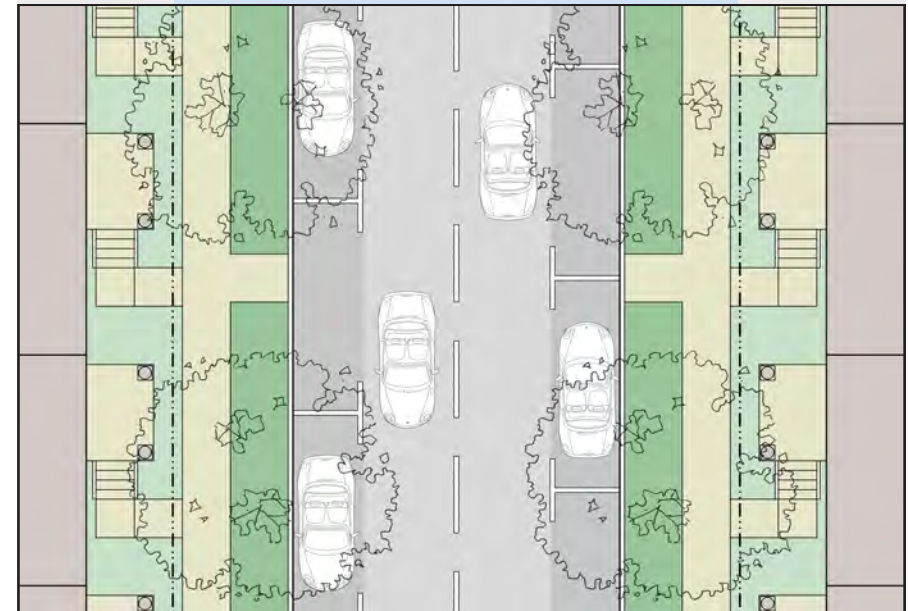
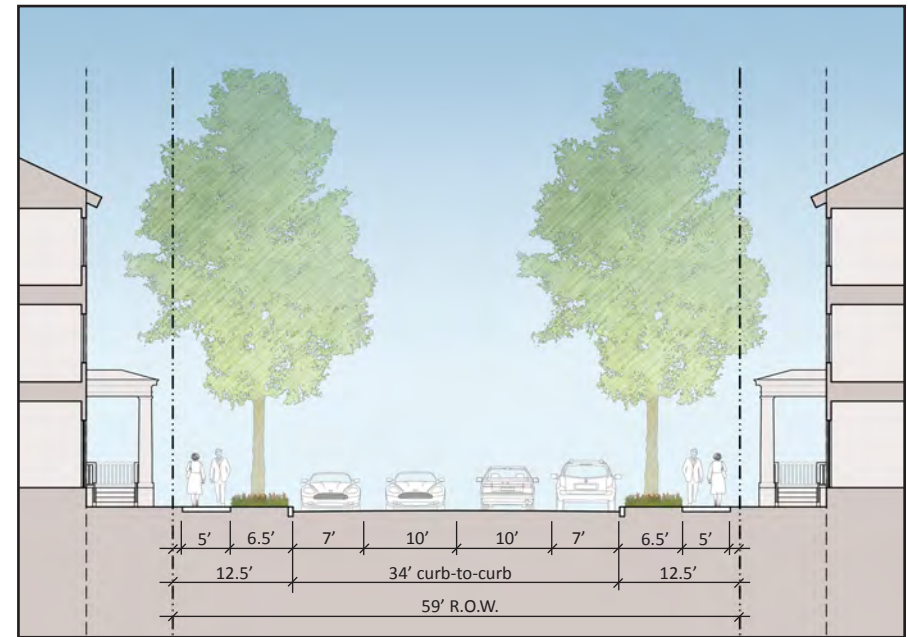


FIGURE 26: Street Sections – Park Two Sides

Regulating Plan

Street Sections

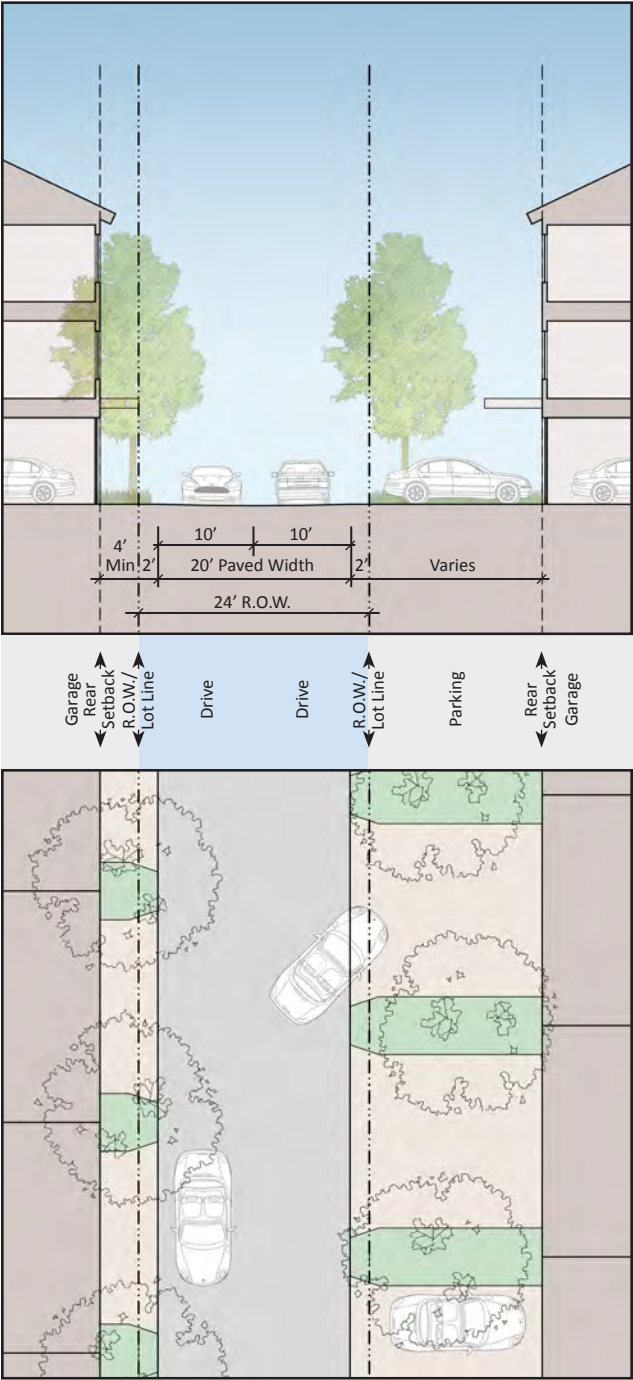


FIGURE 27: Street Sections - Alley

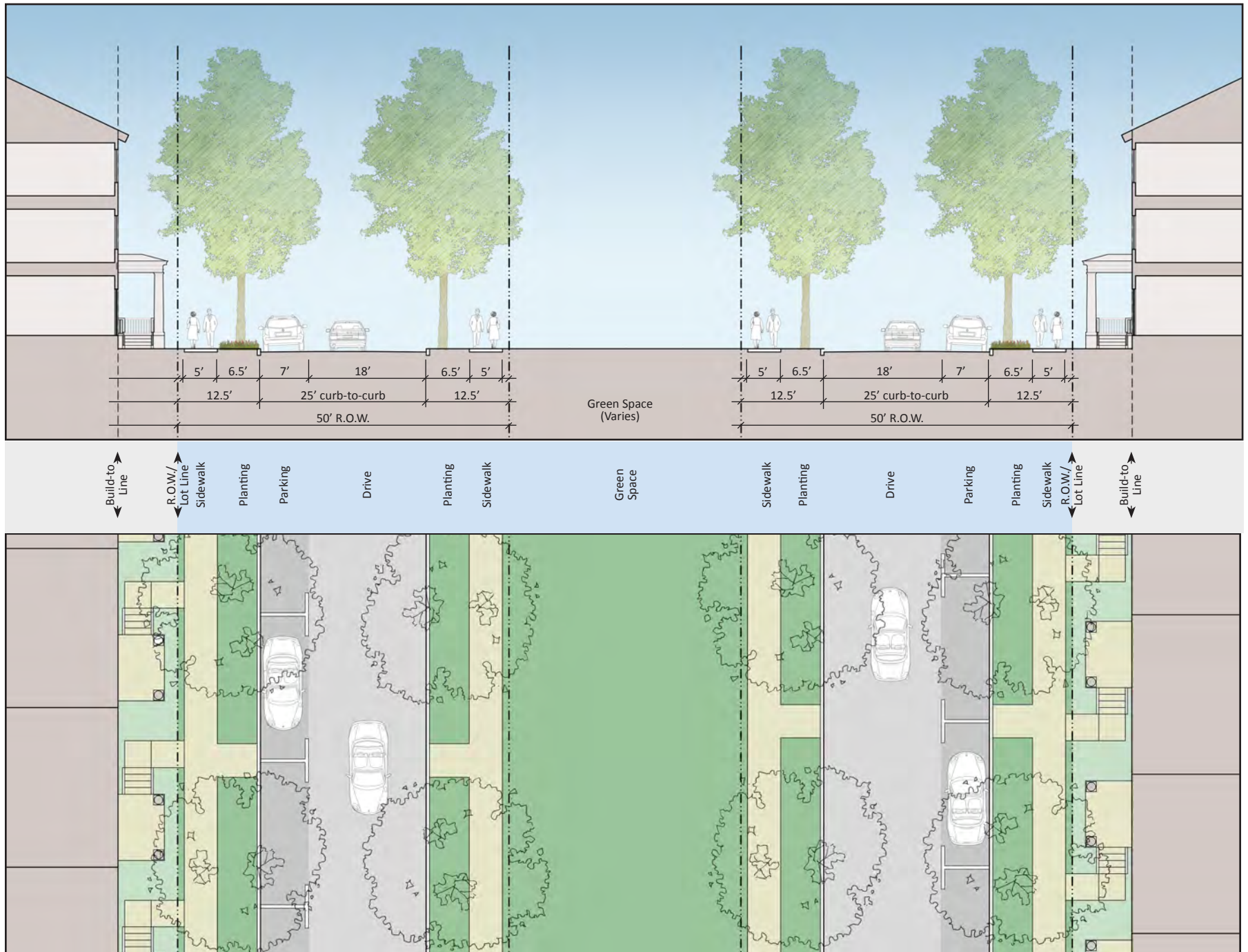


FIGURE 28: Street Sections – One-Way Pair

Development Standards

Block Configuration Lot Standards

Section VIII. – Development Standards

A. Block Configuration

1. Blocks shall be configured as shown in the Regulating Plan.
2. Additional Streets may be added to the interiors of the Blocks shown on the Regulating Plan.
3. Blocks shall meet the following standards:
 - i. The maximum length of a Block Face shall be 650' (see **FIGURE 29**);
 - ii. The maximum perimeter of any Block shall be 2,500', provided, however, the maximum perimeter length of a single-family only, two-family only, or Townhouse only Block (or combination) shall be 1,520' (see **FIGURE 30**);
 - iii. Blocks may be defined by either public Streets or private Streets;
 - iv. Block perimeter lengths shall be measured between: (i) intersecting public Street Right-of-way lines; or (ii) the boundary width of where a private Right-of-way line, Easement, or edge of pavement would be for private Streets/drives; and
 - v. When a Block backs up to an adjacent existing parcel where no Street interrupts its length, there shall be no maximum Block length parallel to the adjacent existing parcel and there shall be no maximum perimeter length (see **FIGURE 31**).
4. Cul-de-sac Streets shall be prohibited. Turnarounds for service drives and Alleys are permitted.

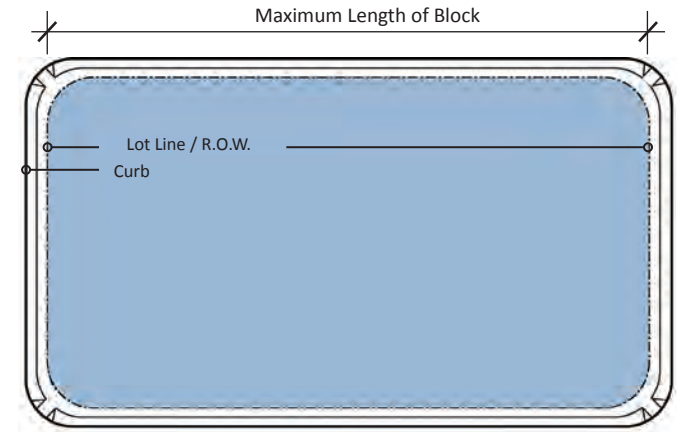


FIGURE 29: Maximum Length of a Block.

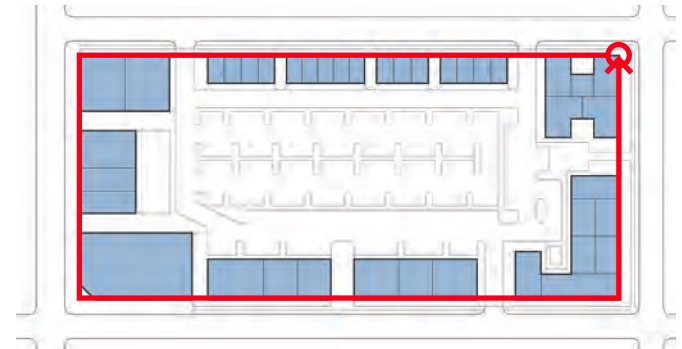


FIGURE 30: Maximum Perimeter of a Block.

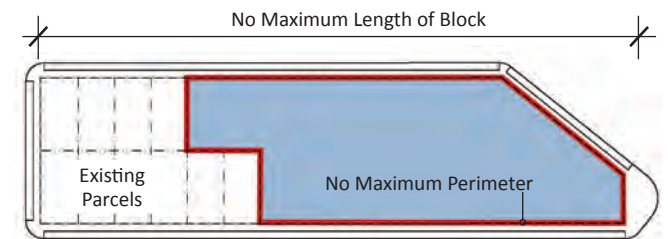


FIGURE 31: No Requirement for Abutting Existing Blocks

B. Lot Standards

1. Applicability:

- i. Minimum rear Yard, side Yard, and Frontage Occupancy standards for Single-Family, Two-Family, Townhouse, Stacked Townhouse, and similar shall comply with **FIGURE 32.**

2. Access:

- i. All Single-Family, Two-Family,
- ii. Townhouse, Stacked Townhouse, and similar Lots shall access parking from an Alley or rear drive. "Front loaded" conditions shall not be permitted.

Lot Type	Front Build-to-Line	Secondary Build-to-Line	Minimum Rear Yard Setback	Minimum Side Yard Setback	Primary Frontage Occupancy	Secondary Frontage Occupancy
Reference dimension locations in FIGURES 33 – 35	A	B	C	D	E	F
Single-family & Two Family	9' or 12' or 15'*	Consistent with front Build-to-Line	4'†	6'; 0' for Attached Side	50% min.	40% min.
Townhouse, Stacked-townhouse, and similar	6' or 9' or 12'*	Consistent with front Build-to-Line	4'†	5'; 0' for Attached Side of End Unit and Middle Units	80% min.	83% min.
Multi-family, Mixed-Use, and Commercial	See Regulating Plan	See Regulating Plan	15'	5'; 0' for a Party Wall	See Regulating Plan	See Regulating Plan

* A front Porch or Covered Stoop shall be required when the build-to-Line is 12' or 15'.

† Measured from Alley Right-of-Way

Note: There is no minimum requirement for Lot width.

FIGURE 32: Lot Dimensional Standards

Legend

— · — · — ·	Property Line	Buildable Area
- - - - -	Build-to-Line	

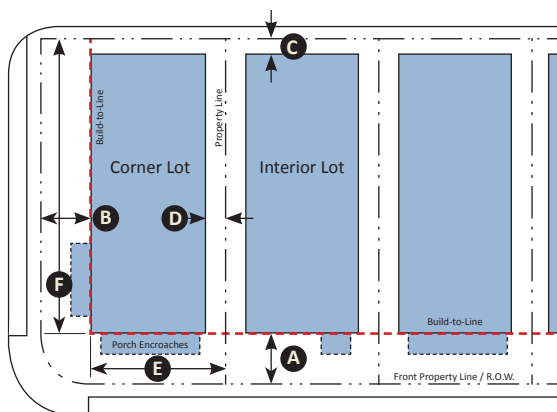


FIGURE 33: Single-family; Two-family on one Lot

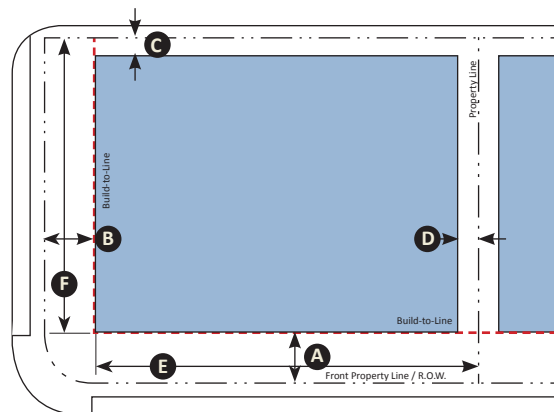


FIGURE 34: Multi-family; Mixed-Use; Commercial

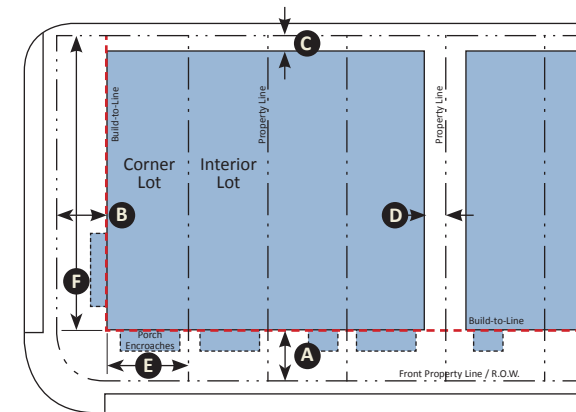


FIGURE 35: Two-family, semidetached; Townhouse Lot

Development Standards

Parking and Loading

C. Parking and Loading

Section 155.600 of the Village Code shall apply except as modified and noted herein.

1. Permitted Parking Locations

Permitted parking locations shall be governed by Layers (see **FIGURE 36**). The first Layer is located between the Lot line along the Primary Frontage and the Build-to-Line. The second Layer is located between the Build-to-Line and a 20 foot setback from the Build-to-Line. The third Layer is the remaining portion of the Lot located behind the second Layer.

- i. Off-Street parking areas shall be located within the third Layer along Primary Streets;
- ii. Off-Street parking areas shall be located within the second or third Layer along Secondary Streets;
- iii. Structured parking without a Liner Building shall be prohibited from occupying the Build-to-Line;
- iv. Townhouse parking shall be accessed via an Alley or rear drive. Garages for individual Townhouse Dwellings shall not have direct Garage access from a public Street;
- v. Single-family and two-family parking shall be accessed via an Alley; and

vi. Off-site parking may fulfill the requirements for off-Street parking if it meets the following conditions:

- a. The off-site parking lot is located within five hundred (500) feet of the Building, Structure, or Use served by the off-Street parking area.

2. Parking Configuration

- i. All multi-level parking Garages/decks shall be designed with a Liner Building facing the Streets. This does not apply to driveways or drive aisles in parking lots; and
- ii. Individual surface parking lots on adjacent parcels shall provide a vehicular connection between each other. The connection must be built at the time of construction of each parking lot.

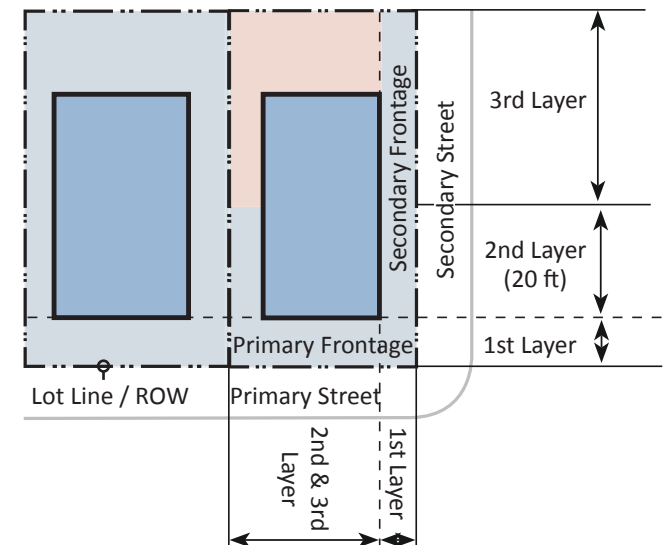


FIGURE 36: Parking Layers

3. Required Parking

- i. Accessible parking shall be provided as established in Section 155.602 Table 6.1 of the Lombard Village Code.
- ii. Required off-Street Parking spaces shall be as established in Section 155.602 Table 6.3 of the Lombard Village Code except as noted below:
 - a. On-Street Parking spaces immediately adjacent to the parcel shall count toward off-Street parking requirements if there is no current plan for Street widening.
 - b. 30 Percent of Parking spaces provided may be built to “compact” standards.
 - c. Bicycle parking shall be provided for multi-family and commercial Uses.
 - d. Short-term bicycle parking shall be located in a convenient and visible area, at least as close as the closest accessible automobile Parking space.

4. Parking Lot Dimensional Standards

- i. Parking space and aisle standards shall be as found in **FIGURE 37**.

5. Loading

Off-street loading shall be provided consistent with the requirements of Section 155.603 of the Village Code.

Parking Lot Dimensions

Space Angle a	Space Width b	Space Length c	Aisle 1-way d	Aisle 2-Way e	Curb Width f
90°	9'	18'	24'	24'	9'
60°	9'	19'	18'	20'	10'-5"
45°	9'	18'	15'	20'	12'-9"
Parallel	9'	24'	12'	24'	N/A
Compact Space Dimensions					
90°	8'-3"	16'-6"	24'	25'	8'-3"
60°	8'-3"	18"	18'	20'	9'-6"
45°	8'-3"	17'	15'	20'	11'-9"

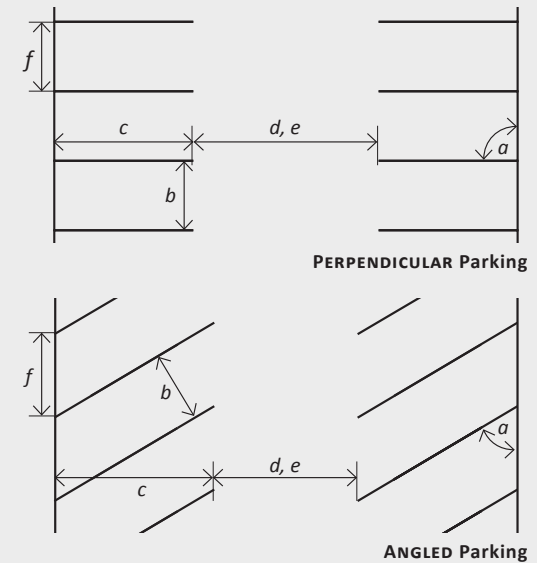


FIGURE 37: Parking Dimensions

Development Standards

Service

Walls, Fences, and Gates

Parkway Trees

D. Service

1. Service areas, such as trash and recycling, which are located outside of a Building shall be located within the third Layer of a Lot and shall not face the Street.
2. Service areas shall be screened on all sides with an opaque Fence or Wall of the same materials as the Primary Building, except:
 - i. Single-family, two-family, and Townhouse Residential Uses are not required to provide trash Screening.
3. Service areas, such as trash and recycling, and loading areas located inside of a Building shall have closable doors which are recessed from the face of the Building. Such doors shall not be located on Yorktown Ring Road within 200' of the intersection of Grace Street and Yorktown Ring Road.

E. Walls, Fences, and Gates.

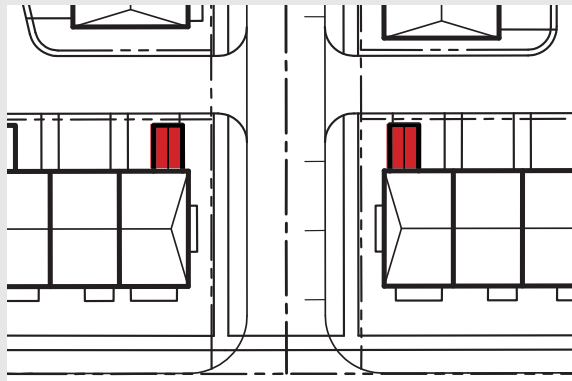
1. Above grade opaque Walls shall be prohibited within a Private Front Yard.
2. Except as required to comply with Restaurant liquor laws, Walls, Fences, and gates shall be prohibited in private front Yards where operating Retail or Office Uses are present.
3. Where permitted, Fences and gates within a Private Front Yard shall not exceed a height of 36".
4. Privacy Fences and Garden Walls shall not occur forward of the rear of a Primary Building.
5. Screening of Alleys in single-family, two-family, and Townhouse developments (see **FIGURE 38**).

- i. When an Alley goes through to the Street, the rear shall be screened with either:
 - a. A Garage, if applicable;
 - b. A full-height sunroom addition; or
 - c. A Wing Wall of six (6) feet height minimum of the same materials as the Primary Facades of the Building.
 - ii. When an Alley does not go through to the Street, it shall be screened with either:
 - a. A minimum six (6) feet tall Screen Wall is required on the Street side. It shall be the same materials as the Primary Facades and be designed as an extension of the house;
 - b. A full-height sunroom addition; or
 - c. A combination of the two.
6. Chain link, barbed wire, razor wire, chicken wire, stockade fencing, and split rail fencing shall be prohibited.
 7. The fencing type shall be standardized within a single development.

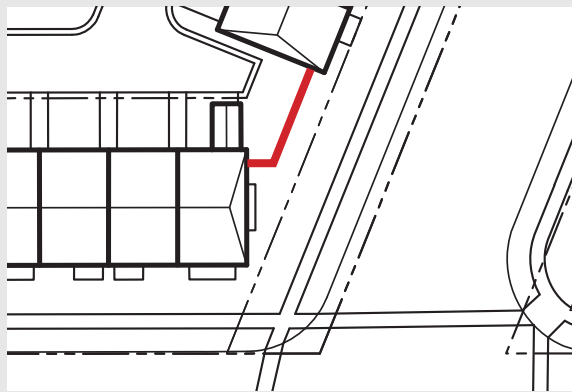
F. Parkway Trees

Right-of-way Landscaping shall be provided per § 155.705 of the Village Code, except for:

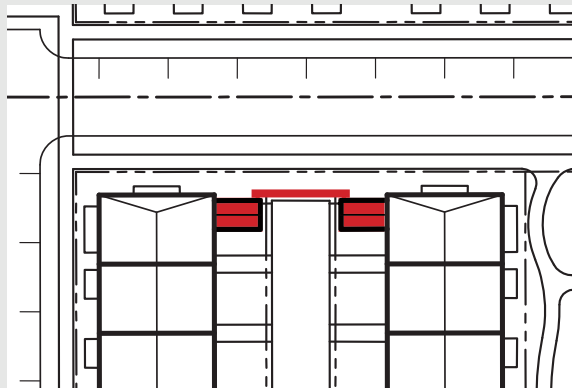
1. Parkway trees shall be regularly spaced, not clustered, at a spacing of thirty-five (35) to forty (40) feet and shall be paired on opposite sides of the Street, except in localized areas where utility conflicts exist.



Sunroom Screening Alley Entrance



Fence Screening Non-Connecting Alley



Fence and Sunroom Screening Non-Connecting Alley

FIGURE 38: Alley Screening

Signs

Applicability

Materials

Review and Approval

Configurations

Section IX. – Signs

A. Applicability

1. Signs shall adhere to regulations found in Chapter 153 of the Village Code of Ordinances, Articles I, II, III, IV, and V with the following exceptions and additions.
2. The following regulations shall apply to signs and to all permitted Uses located in the Yorktown Commons Planned Development.

B. Materials

1. Signs shall be of materials consistent with the architectural style.
2. Fluorescent or glowing paint, billboards, moving or intermittently illuminated signs or advertising devices, flashing signs, and signs that make or create noise shall be prohibited within the Yorktown Commons PD.
3. Signs shall be designed so that all wiring, transformers, ballasts, starters, raceways, and other necessary equipment shall be concealed.

C. Review and Approval

1. Sign design and configuration shall be subject to review by the Yorktown Commons Architectural Review Board prior to application for permit. The Yorktown Commons Architectural Review Board may approve signs that do not meet these requirements if it is determined that they meet the intent of the Design Guidelines.
2. The Community Development Director may approve a sign that is not consistent with the regulations of this section when the sign is designed as a site-specific piece of civic art if it is determined that such sign is consistent with the intent of the PD Design Guidelines.

D. Configurations

1. Attached Signs – Three types of attached signs are permitted:
 - i. Band Sign: The band sign consists of a band of lettering across the entire width of the Building. If lit, band signs shall be front-lit. Band signs shall be a maximum of 36" tall, and the bottom of the band sign shall not be installed less than 10 ft. above the sidewalk. See **FIGURE 39**.

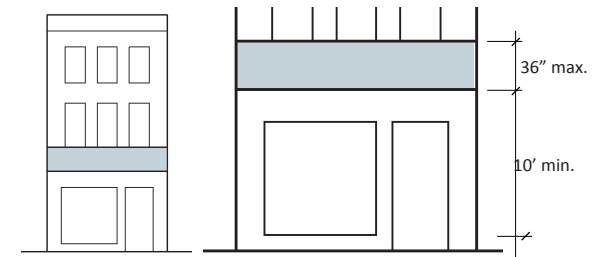


FIGURE 39: Diagram of Band Sign Dimension



FIGURE 39: Illustrative Example of Band Sign

- ii. Board Sign: The board sign consists of painted or vinyl graphics on a signboard attached flush with the Building Wall. See **FIGURE 40.**

- iii. Window Sign: The window sign is located behind the glass or is comprised of gold leaf or vinyl applied directly to the glass. Window signs shall not be mounted on opaque signboards. The height of any window sign is limited to one-third the height of the glass in the sash where the sign is installed, excluding muntins. The width of any window sign is limited to 90 percent of the width of the glass in the sash where the sign is installed. Signs may not be affixed with tape or other temporary means to the exterior nor to the interior of the glass surfaces. Neon signs shall be prohibited. Decals shall not be affixed to glass. See **FIGURE 41.**

The width of any window sign is limited to 90 percent of the width of the glass in the sash where the sign is installed. Signs may not be affixed with tape or other temporary means to the exterior nor to the interior of the glass surfaces. Neon signs shall be prohibited. Decals shall not be affixed to glass. See **FIGURE 41.**

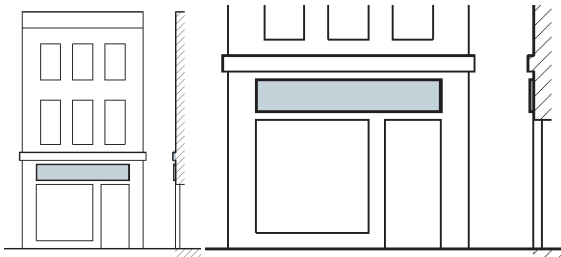


FIGURE 40: Diagram of Board Sign Dimension

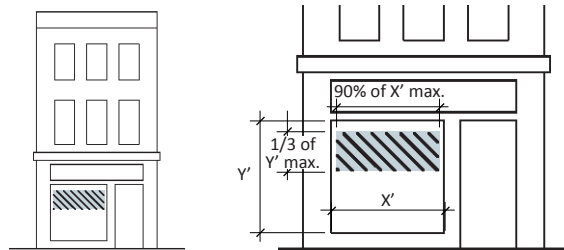


FIGURE 41: Diagram of Window Sign Dimension



FIGURE 40: Illustrative Example of Board Sign
(Credit: flickr K-FREE)



FIGURE 41: Illustrative Example of Window Sign

Signs

Configurations

2. Projecting Signs – Two types of projecting signs are permitted:

- i. Blade Signs: Blade signs hung from an architectural element shall be centered on that element. Blade signs projecting from the Wall may project a maximum of 4'. The top of the blade sign shall be between 9' and 12' above the sidewalk. The bottom of the sign shall be no less than 8' above the sidewalk. The blade sign shall be 32" tall maximum. Blade signs shall be no more than 4' wide. No blade sign shall exceed 6 square feet. Brackets or other suspension device shall match the architectural style of the Building and shall not be computed as part of the allowable size of the sign. See **FIGURE 42**.

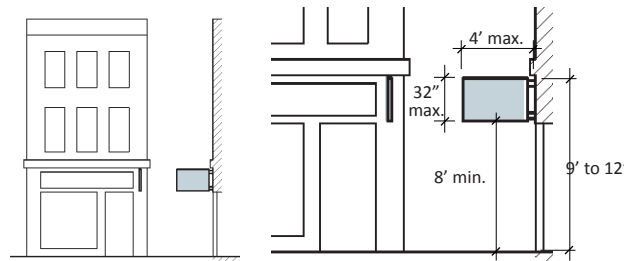


FIGURE 42: Diagram of Blade Sign Dimension



FIGURE 42: Illustrative Example of Blade Sign

- ii. Vertical Signs: Vertical corner signs are permitted at the corners of Blocks. They may project perpendicular from one side of the Building or at a 45 degree angle to the corner. Vertical corner signs may be lit with projecting lights. Vertical corner signs shall be mounted a minimum of 12' in height from the sidewalk, measured to the bottom of the sign. The height of the sign shall not exceed the first-story Wall height. Vertical corner signs shall be mounted 12" maximum away from the exterior face of the Building and shall be a maximum of 3 ft. wide. See **FIGURE 43**.

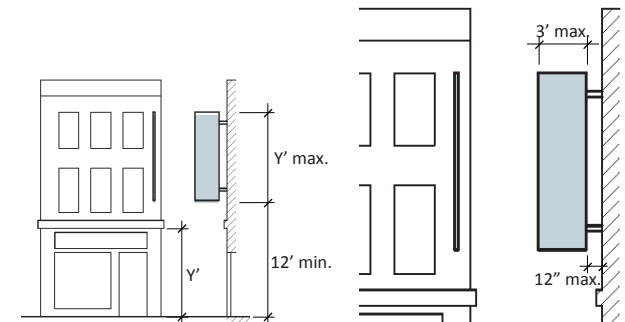


FIGURE 43: Diagram of Vertical Sign Dimension

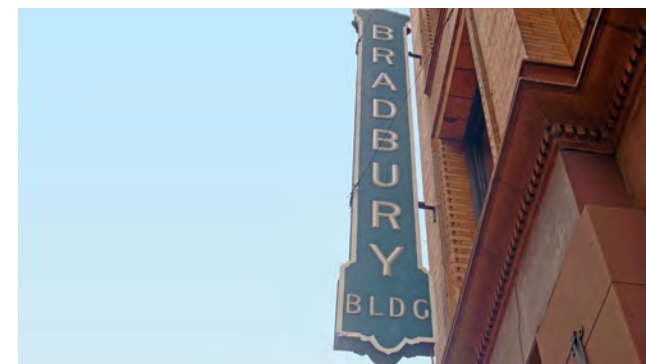


FIGURE 43: Illustrative Example of Vertical Sign
(Credit: flickr Kansas Sebastian)

3. Ground Signs – Sculptural and A-frame sign boards placed on the sidewalk shall be permitted if they are temporary and removed during non-operating hours and also comply with Section 153.234 of the Lombard Sign Ordinance. See **FIGURE 44**.
4. Awning Signs – Signage may be painted either on the fringe of an Awning or in the center of the body of the Awning. Awning signs shall be painted directly on canvas. Back lit Awnings are prohibited. Signs that occupy the fringe of the Awning may fill the entire height and width of the fringe up to a maximum fringe height of 10”.
5. Security Signs – One sign providing notice of a security system is permitted at each Frontage and shall be affixed to a Building.
6. Real Estate “For Sale” and “For Rent” Signs – One real estate sign advertising a property for sale or rent may be displayed at each Frontage. Signs must comply with Section 153.234 of the Lombard Sign Ordinance.
7. Freestanding, off-site and detached signs are prohibited unless noted otherwise.
8. Signs may Encroach into setbacks and across R.O.W. lines, but not across private Lot lines.
9. Temporary Signs shall be permitted for openings or special events for a maximum time period of 72 hours.
10. Project Identification signs shall be permitted if approved by the Yorktown Center Architecture Review Board.



FIGURE 44: Illustrative Example of A-Frame Ground Sign
(Credit: flickr kellan)

Architectural Standards: All Buildings

Orientation

Entries

Windows

Walls

Mechanical

Equipment

Site Utilities

Materials

Townhouse String

Configuration

Section X. – Architecture Standards: All Buildings

To promote the creation of walkable neighborhoods, and in recognition of the architectural history of the Village of Lombard, Building designs should provide sufficient articulation to create human-scaled architecture and be compatible with the historic character. Architectural design shall be subject to review by the Yorktown Commons Architectural Review Board and the following architecture standards shall apply to all Buildings within the Yorktown Commons PD.

A. Orientation

1. Primary Building Facades shall be oriented to the Street. A Building located on a corner Lot shall have two Primary Building Facades.

B. Entries

1. The Primary Entry shall be located along the Primary Building Facade and shall be designed with architectural features and details that clearly communicate the entry.
2. Service entrances shall be located at the rear of the Building.

C. Windows Along Street Frontages

1. Primary Building Facades shall have windows.
2. Windows may be clear glass or may have a low-emissivity coating. No mirrored or tinted glass shall be permitted.
3. Windows shall have a vertical proportion meaning that they are taller than they are wide (see **FIGURE 45**).
4. For Residential Buildings, windows in a Facade shall be regularly spaced and the same size, except for accent windows.

D. Walls

1. Designs and materials shall be consistent on all Primary Facades.
2. Where real or simulated brick, stone, stucco, or similar materials are used on the Primary Facade(s), the material shall continue along the secondary facades a minimum of 16" in depth measured from the face of the Primary Facade.

E. Mechanical Equipment

1. Utility meters shall be located so as not to be visible from the Primary Frontage. Such equipment shall be located to the interior of the Block behind Buildings.

F. Site Utilities

1. Site utility Structures, such as transformers, shall be located to the interior of Blocks (behind Buildings) or along the side of the Buildings.

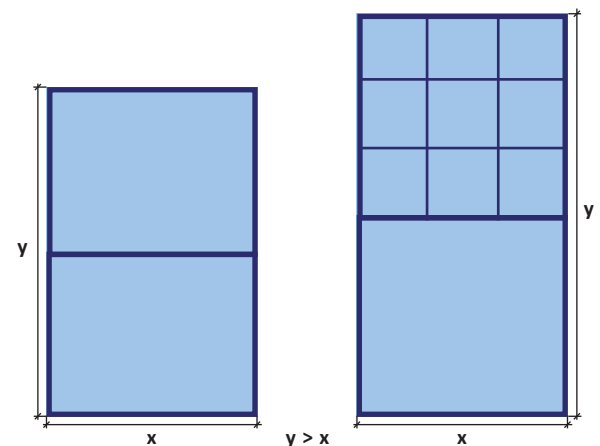


FIGURE 45: Vertical Proportions

G. Materials

1. Vinyl siding shall not be permitted on any Facade for all Buildings.
2. Membrane roof material shall only be permitted for flat roofs.

H. Townhouse String Configuration

1. String shall be designed as a composed Building and shall not be designed as individual independent Facades (see **FIGURE 46**).
2. Breaks and recesses between units in the string shall be designed to occur as a part of the string composition and shall not alternate back and forth between every Dwelling unit (see **FIGURE 47**).
3. Changes in material shall occur to reinforce the string composition and shall not occur with each Dwelling unit.
4. Changes in color or material from one Townhouse unit to another shall only occur when there is either a change in Facade plane or vertical break in the Facade plane.

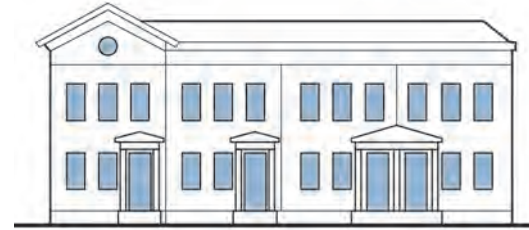
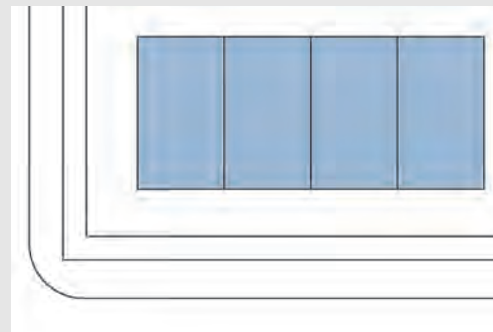
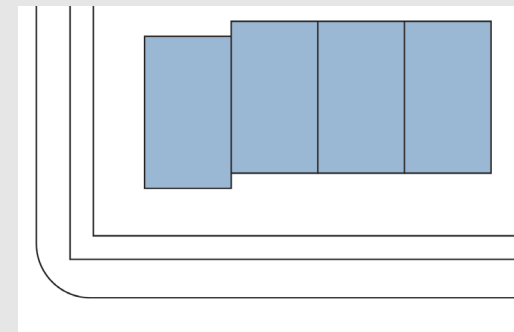


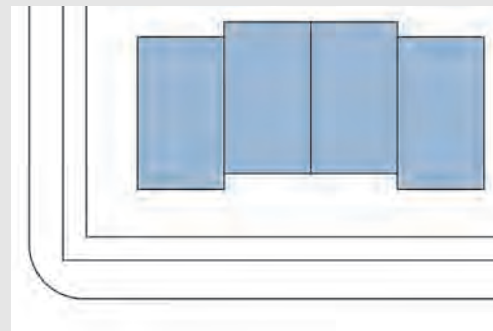
FIGURE 46: Townhouse String Designed as a Building



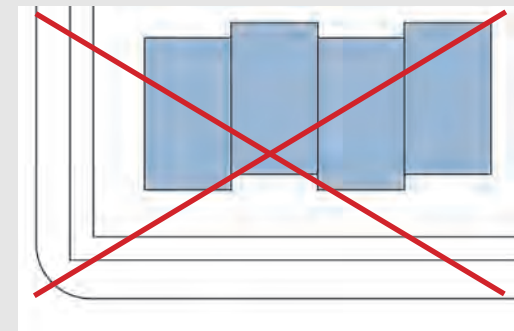
ACCEPTABLE Townhouse Configuration



ACCEPTABLE Townhouse Configuration



ACCEPTABLE Townhouse Configuration



UNACCEPTABLE Townhouse Configuration

FIGURE 47: Horizontal Breaks in Townhouse Strings

Architectural Standards: Commercial, Mixed-Use, and Large Multi-family

Configuration

Commercial Ground-

Floor Windows

Mechanical

Equipment

Materials

Entries

Section XI. – Additional Architecture Standards: Commercial, Mixed-Use, and Large Multi-Family Buildings

In order to reinforce the idea of a pedestrian scale and to have the varied look of a number of Buildings built over time, the following requirements apply to all commercial, Mixed-Use, and large Multi-Family Buildings:

A. Configuration

1. The minimum Building height shall be eighteen (18) feet.
2. Blank Walls shall not face Streets. Liner Buildings with Primary Building Facades shall be used to screen blank Walls from the Street.
3. Facades longer than 250' shall be designed to look like more than one Building. For Buildings that are 250' or longer, no section of Building longer than 250' shall look like one Building. Each section of Building shall be different in color and/or material than the other sections and shall have different base heights, cornice heights, and window sizes.

B. Commercial Ground-Floor Windows

1. The minimum percentage of Facade surface along Streets that shall be glazed (window surface area) is forty (40) percent of the first floor Facade. The area of the first floor Facade is calculated as the length of Facade multiplied by the average height, as measured from the ground to the elevation of the second floor (see **FIGURE 48**).
2. The maximum height of sill above sidewalk shall be twenty (20) inches.

3. Solid metal security gates, solid roll-down windows, link, grill or other similar security devices shall only be permitted when installed from the inside of a Building within the window or frame.
4. The maximum percentage of glass on the first floor that may be blocked with interior fixture, paint, paper signs, paper, or similar material shall be 20% and may not exceed a height of four (4) feet above the exterior ground line.

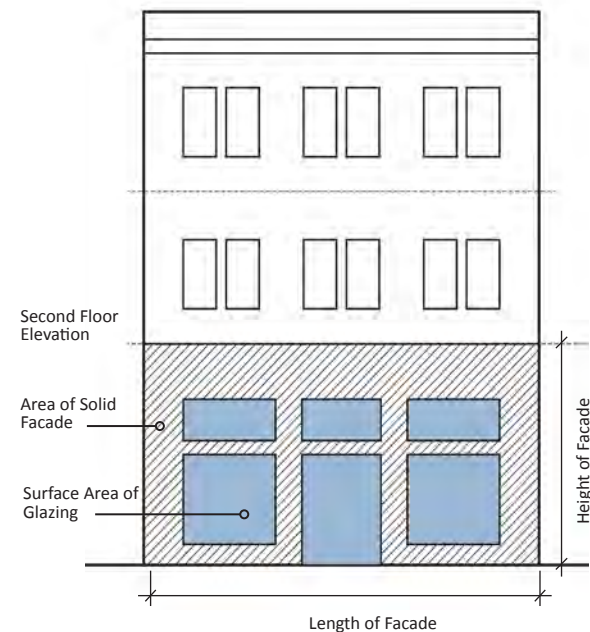


FIGURE 48: Percentage of Window Surface Area

C. Mechanical Equipment

1. Window air conditioning units shall not be permitted on Primary Facades.

D. Materials

1. A maximum of two primary materials shall be used on a Facade or a Facade section as it is described Section XI(A)(3) of this document. A primary material is one that occupies one-third or more of the Facade (excluding windows) for 3-Story Buildings or one-fourth or more of the Primary Facade (excluding windows) for 4-Story and taller Buildings. A maximum of one primary material shall be used on a Primary Facade for 2-Story Buildings unless a Craftsman style is used where the proportions of materials shall be consistent with the historical style.
2. Exterior light fixtures shall match the architectural style of the Building.

E. Entries (FIGURES 49 AND 50)

1. Mixed-Use Buildings shall have an entrance to access upper Story Uses located along the Primary Facade. Additional entrances to access upper Story Uses may be located elsewhere on the Building.
2. Entries along sloping Streets shall provide safe, flat entryways.
3. Residential style doors shall be prohibited.

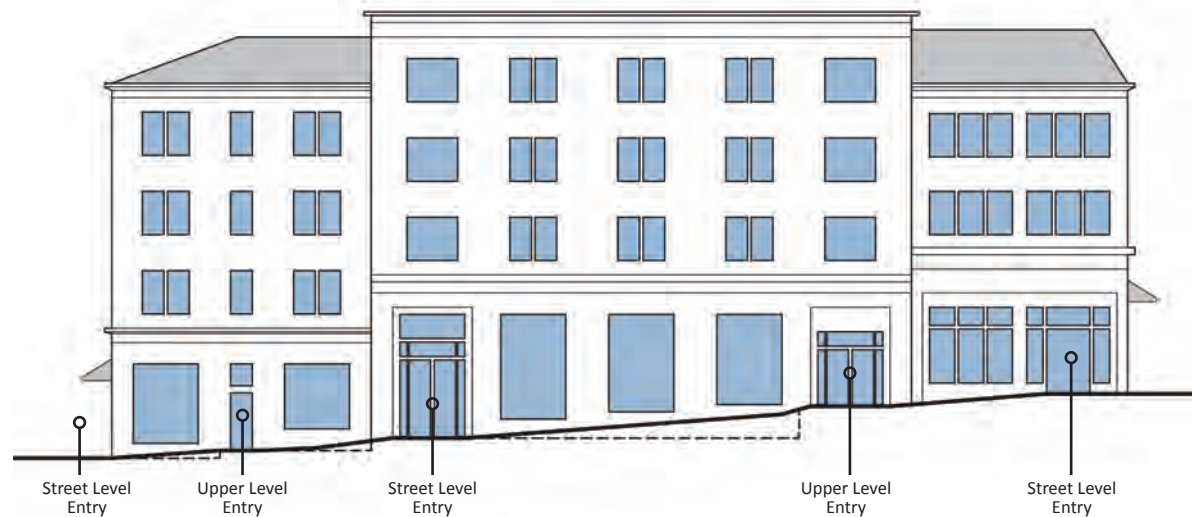


FIGURE 49: Sloping Street



FIGURE 50: Level Street

Definitions

Section XII. – Definitions

Except where specifically defined in the Design Guidelines or the Village Code of Ordinances, all words used carry their customary meanings. For terms not defined in this section and that have been defined in other sections or chapters of the Code of Ordinances, the definitions in those other sections or chapters apply. In the event that a definition in this section conflicts with a definition in another part of the Code of Ordinances, the definitions in these Design Guidelines shall control.

Words used in the present tense include the future. Words used in the singular number include the plural, and words used in the plural include the singular, unless the context clearly indicates the contrary. The word “shall” is always mandatory. The word “may” is permissive. The word “should” indicates a recommendation but not a legal requirement. Building or structure includes any part thereof. The word “lot” includes the word “plot” or “parcel.” The word “person” includes an individual person, a firm, a corporation, a partnership, and any other agency of voluntary action.” The words “include,” “includes,” and “including” shall be interpreted as though followed by the phrase “without limitation” or “but not limited to.” The phrase “used for” includes “arranged for,” “designed for,” “intended for,” “maintained for,” and “occupied for.” The official designated to make an interpretation of all terms used here is the Community Development Director.

A

Alley Entrance: The location along a street where an alley is accessed.

Architectural Review Board: A board established pursuant to a private Declaration of Covenants to be recorded against the Property and pursuant to which the owner of Yorktown Chopping Center, or such other party as the owner of Yorktown Shopping Center may nominate, may review and approve architectural elevations and/or signage for development in the Yorktown Commons PD.

B

Block Face: A single side of a block, including all primary facades on that block.

Build-to-Line: The line shown on the Regulating Plan along which the principal vertical plane of all primary facades must be erected (except as otherwise provided in this Part). The build-to-line location is measured from—and is generally parallel to—the parcel line, lot line, or right-of-way. To accommodate irregular small jogs in parcel lines, the build-to-line may be regularized as long as it is consistent with the intent of the regulations contained in these Guidelines.

C

Commercial: A generic term for the use of land or buildings for commerce or work, other than for civic uses. Examples of commercial uses include personal service businesses, retail businesses, offices for businesses and non-profit organizations, medical offices, professional offices, private gyms, privately owned entertainment facilities, and repair shops.

Corner Tower: A tower that is at the corner of a building, located at the corner of a block, Mews, or pedestrian paseo.

Courtyard: A space enclosed on three or four sides by buildings.

Covered Stoop: A raised landing located at a building entry with a roof covering the landing.

Cul-de-sac: A dead-end street with only one entry point and a turnaround.

E

Encroachment: To break the plane of a vertical or horizontal regulatory limit with a structural or architectural element, so that it extends into a required setback area or private frontage, or above a height limit. An element which breaks the plane is known as an encroachment.

F

Facade: The exterior wall of a building.

Form-Based Code: A type of land use regulation that significantly regulates the form of the built environment and public realm rather than the focusing on the separation of land uses as commonly found in conventional “Euclidean” zoning.

Frontage Occupancy: The minimum length of primary facade that shall be built along a Build-to-Line.

G

Garden Wall: A wall no greater than 48” in height that defines the frontage line and/or the perimeter of a property. A garden wall has ornamental qualities and may be solid or comprised of piers and a knee wall with pickets between piers.

L

Land Use: The purpose for which a land or a structure is designed, arranged, or intended to be occupied or used, or for which it is occupied, maintained, rented, or leased.

Layer: A range of depth within a lot used to regulate the location of permitted uses and structures.

Liner Building: A building that conceals a separately constructed garage and that is designed for occupancy by residential, commercial, or mixed-uses.

Definitions

M

Mews: A linear public or private open space lined on both sides by buildings, primary facades, and front doors.

Mixed-Use: A building or parcel with multiple uses.

Mixed-Use Building: A building designed for and containing more than one use.

Multi-Family Building: A condominium or rental building designed for occupancy by three or more families living independently of each other in separate dwelling units, other than Townhouses and Stacked Townhouses.

O

Office: A place of business where professional or clerical duties are performed in either for-profit or not-for-profit entities.

P

Parcel Line: (Syn: Lot Line)

Paseo: A public walk open to the sky located between two buildings that is designed for comfortable and safe pedestrian passage from one location to another and for pedestrian amenities such as seating, lighting, and landscaping.

Primary Building: The building or buildings on a lot that contain the principal use or uses.

Primary Entrance: The main point of access of pedestrians into a building, facing a street.

Primary Facade: The facade of a building that faces the street. When a building is located on a corner lot, both facades that face streets and/or a Mews are primary facades. When a building faces a Mews, the facade facing the Mews is the primary facade.

Primary Frontage: The side of a lot facing a street designated on the Regulating Plan as a primary street.

Primary Street: A street designated on the regulating plan as a primary street. Typically, a primary street has more vehicular circulation, a greater mix of uses, and more pedestrian accommodation than a secondary street.

Privacy Fence: A fence with a height greater than 48”.

Private Frontage: The space that occurs between the public right-of-way and the primary facade.

Public Frontage: The space that occurs between the street curb and the frontage line (right-of-way line).

Public Space: Land so designated on a Regulating Plan that is generally not developed with buildings and is dedicated and available to the general public for active or passive recreation and for civic uses. Public spaces may include civic uses and civic buildings within them.

R

Regulating Plan: A regulatory instrument in the form of a scalable, dimensionally correct, plan that is a key element of most Form-Based Codes.

Retail: Sale of goods and/or provision of personal services directly to consumers. Where a retail use is required in these Design Guidelines, a restaurant shall also be considered as a retail use for these purposes.

Retail Frontage: A frontage specifically designed to support pedestrian oriented retail activity.

S

Secondary Street: A street designated on the regulating plan as a secondary street. Typically, a secondary street is a street with less pedestrian circulation than a primary street and has a lesser degree of mixed uses than primary streets.

Secondary Frontage: The side of a lot facing a street designated as a secondary street.

Stacked Townhouse Building: A fee-simple, condominium, or rental dwelling unit configured such that one two-story unit is generally located over another two-story unit and both units have separate entrances facing a street or Mews. Each pair of stacked units is designed to look like a single townhouse and each pair of stacked units is attached to another pair of stacked units on one or two sides by a common party wall so that there are a minimum of four dwelling units configured together.

Streetscape: All of the elements that are located between the street curb and the primary facade when the sidewalk extends from the street curb to the primary facade, and all of the elements that are located between the street curb and right-of-way when the sidewalk does not extend to the primary facade.

T

Thoroughfare: The area contained in a right-of-way of a public or private street.

Tower, Architectural: A stand-alone structure that is significantly taller than it is wide, or a portion of a building that is significantly taller than it is wide and typically has more detail than the surrounding building(s). When a tower is a portion of a building, the tower eave or cornice is taller than the remainder of the building eave or cornice height and one or more of the tower facades is located forward of the remaining building facade.

Townhouse Building: A single-family fee-simple, condominium, or rental dwelling unit that is attached on one or two sides of the building by a common party wall where there are a minimum of three units and each unit has a separate entrance facing a street or Mews.

Definitions

V

Vista Termination: A building, structure, or portion of a building or structure, specifically designed to visually attract a viewer's attention at the end of a visual axis, i.e. to terminate a view. Vista terminations may include towers, corner towers, symmetrical facades centered on a visual axis, an architecturally embellished entry, or similar distinctive architectural devices.

W

Screen Wall: A masonry upright structure serving to enclose, divide, or protect an area.

Wing Wall: A solid wall extending from the back of a building to the alley rear property line setback line having a height equal to the first story of the building.



EXHIBIT C – “PRELIMINARY STORMWATER ANALYSIS”

**PRELIMINARY
STORMWATER
ANALYSIS –
YORKTOWN
COMMONS**



PROJECT SITE:

**YORKTOWN COMMONS AT YORKTOWN CENTER
VILLAGE OF LOMBARD, DUPAGE COUNTY, ILLINOIS**

PREPARED FOR:

**ROSANOVA & WHITAKER, LTD.
30 W. JEFFERSON AVENUE, SUITE 200
NAPERVILLE, IL 60540**

PREPARED BY:

**V3 COMPANIES, LTD.
7325 JANES AVENUE
WOODRIDGE, ILLINOIS 60517
630.724.9200**

OCTOBER 19, 2015



Preliminary Stormwater Analysis

Yorktown Commons

October 19, 2015

I. Introduction

This report was prepared to document the storm water management criteria and impacts for each parcel of the Yorktown Mall Redevelopment, while also providing a comprehensive analysis of the entire redevelopment area. The study includes: analysis of the existing site conditions; review of information that was gathered from the Village; Village and County storm water detention requirements; Village and County's BMP requirements, and; discussion of offsite drainage conveyance through the undeveloped parcel.

II. Existing Conditions

The Yorktown Commons Redevelopment is located within the existing Yorktown Center Development and is currently comprised of an existing development on Parcel 1, an undeveloped lot on Parcel 2, and an existing development on Parcels 3 & 4. See attached Yorktown Commons Exhibit.

The Parcel 1 is currently undeveloped. There is a ditch that conveys stormwater from Grace Street to a storm sewer that runs under the Target parking lot. The ditch, as currently configured, bisects the property. Offsite storm water from the 27.5 acres of residential development to the west is released into the ditch from a 30" storm sewer. The ditch outlets from the east side of the property into a 36" storm sewer and is then routed to the wetlands north of the Target building.

The Parcels 2 & 3 include the old Bamboo Room building and parking lot as well as the parking lots north and northwest of JCPenney. As shown on the atlas maps, existing underground infrastructure is in place throughout this parcel and detention is currently provided within the pond between the Westin and the Theatre.

The Parcel 4 is currently the site of the Carson's Furniture Store as well as several additional inline retail and restaurant tenants. The site is currently 95% impervious and, as shown on the attached utility atlas map, has a full array of existing underground infrastructure. Storm water from this site currently drains to the detention basin located between the Westin and the Theatre properties.

III. Information Provided by Village

The Village of Lombard provided the following information which has been incorporated into this analysis.

- Utility Atlases
- Storm Water Reports for the Target and Westin Developments
- Engineering Plans and Record Drawings for the Target Development

- Letter from Village to AMC Theaters documenting that the high water level of the detention basin between the Westin and the AMC Theater was raised as part of the Westin development. This created a condition where a small area of the AMC parking lot now ponds during the peak 100-year storms. This letter leads us to agree with the Village's conclusion that there is no additional detention capacity in this pond for us to utilize.

IV. Storm Water Detention Requirements

The Lombard Flood Control Ordinance and the DuPage County Countywide Stormwater & Flood Plain Ordinance are the governing ordinances for detention requirements pertaining to this development. Per Section 151.55 of the Lombard Flood Control Ordinance "All new development that increases the total impervious area, "TIA", of the development property by more than 5,000 square feet after July 1, 2012 shall require surface water runoff of the limits of said lot to be limited by detention or retention to the maximum of 0.10 cfs per acre of land..."

The approach to storm water detention discussed with the Village at a meeting on July 10th is to treat each parcel separately. This will be possible for Parcels 1 & 4 as they have already been subdivided as individual lots. Parcels 2 & 3 will require a new Subdivision of the two existing parcels that will be impacted by this development. As such, the Development Area for Parcels 2 & 3, for the purposes of the storm water detention requirements, will be the area of the two existing lots potentially impacted by the new subdivision. It is our understanding that the parcels defined throughout this report (Parcels 1 thru 4) will be developed separately (by separate developers) in the future.

As mentioned above and outlined in the attached letter, as part of the Westin development the capacity of the detention pond between the Westin and the AMC Theater was increased by raising the high water level (HWL) of the pond. While increasing the HWL provided the added storage volume needed for the Westin, it created a condition where areas of the AMC parking lot and a significant portion the drive aisle behind the theatre are at elevations below the 100-year storm. During 100-year storm events these areas see anywhere from 2 to 9 inches of ponding. These details lead us to agree with the Village's conclusion that there is no additional detention capacity within this pond for us to utilize should our proposed improvements trigger the detention requirements outlined below.

Parcel 1

As the 6 acre Parcel 1 is currently undeveloped, any proposed project is expected to trigger both the BMP and Detention requirements outlined above. Based on the Village's and County's detention requirements we have calculated that the proposed development will require approximately 2.75 ac-ft of detention. This calculation assumes a curve number (CN) of 92, which is consistent with an apartment or townhome development. An at-grade detention basin with a depth of 5' would require approximately 0.60 acres designated for detention. If this can be accommodated by the site plan, the basin could be designed and planted as a native vegetated wetland bottom basin. This should address all detention and BMP requirements.

The alternative to at-grade detention would be to provide storage in an underground system. A variety of systems are available to accomplish underground detention storage, including: incorporating storage into a chamber that is part of the building; providing storage in underground pipes; and providing storage in a vault system. Providing the full 2.75 ac-ft of detention underground using a StormTrap vault system would cost between \$720,000 and \$900,000. This cost depends on a variety of factors including the depth of the system and does not include the cost for hauling off the earthwork spoils generated by the underground detention. The underground system would have a footprint of 25,000 sf to 30,000 sf. An underground detention system does not meet the BMP requirements.

The detention facility for this parcel will have a restricted release of approximately 0.60 cfs. The restricted release, plus the unrestricted flow from upstream areas will be conveyed to the existing 36" storm sewer that is located at the east end of the existing ditch.

Parcel 2 & 3

As discussed at the July 10th meeting with the Village, it is our understanding that Parcels 2&3 is the only portion of the property to be subdivided as part of the planned development. Since this portion of the property is to be subdivided we understand that the impervious area of these lots prior to February 15, 1992 (date of Stormwater Ordinance adoption) must be compared to the current impervious area. The two lots to be subdivided are actually owned by separate entities (JCP & YTC) and are shown on the attached exhibits labelled "1988 & 2015 Yorktown Center Aerial Exhibit". When comparing 1988 aerial photography to the 2015 aerial photography it is apparent that some development has occurred on the two lots in question. However, there has been no appreciable increase in impervious area since February 15, 1992. For this reason we have defined a 'Development Area' so that we can measure the net change in the impervious area as a result of the proposed development.

On the attached "Pervious/Impervious Area – Parcels 2&3 Development Area Exhibit" we have drawn a conservative "Development Site" boundary for the purposes of determining an impervious area baseline. The exhibit shows that the Development Area is 17.26 acres (752,036 sf) with 687,087 sf of impervious area and 64,949 sf of pervious area. The impervious area that the proposed site plan must stay below to avoid triggering the detention requirements is 692,086 sf. If that number is exceeded, approximately 7.7 ac-ft of detention will be required. This volume would require approximately 1.7 to 2.1 acres of land (or equivalent underground area) to be provided for detention.

While Parcels 2&3 have a greater percentage of pervious area (9%) compared to Parcel 4, we believe a site plan can be developed to fit within the above parameters so that detention for this parcel will not be required. If the primary development area begins to approach the threshold, the large parking fields to the northwest and northeast of JC Penney offer opportunities to add some strategic landscape islands to avoid triggering detention requirements.

Parcels 4

The 9.64 acre Parcel 4 is currently developed and, per the attached "Pervious/Impervious Area – Parcel 4 Exhibit", has 400,528 sf of impervious area and 19,478 sf of pervious area. The existing site is 95% impervious, a number that will likely be reduced by any proposed site plan. If the proposed plan stays below 405,528 sf, storm water detention will not be required for this parcel. If the impervious area of this parcel exceeds that number, approximately 4.8 ac-ft of detention would be required. This volume would require approximately 1.0 to 1.3 acres of land (or equivalent underground area) to be provided for detention.

Based on current Village Code requirements for setbacks and landscaping, we feel that it is unlikely that the final site plan would increase the impervious area of this parcel by 5,000 sf, triggering the detention requirements.

V. BMP Requirements

The Lombard Flood Control Ordinance and the DuPage County Countywide Stormwater & Flood Plain Ordinance are the governing ordinances for BMP requirements. Per Section 15-63 of the DuPage County Countywide Stormwater & Flood Plain Ordinance, Post Construction Best Management Practices (PC BMPs) are required for all projects where the Net New Impervious Area is equal to or exceeds 2,500 square feet.

The approach to storm water BMPs discussed with the Village at the meeting on July 10th is to treat each parcel separately. This will be possible for Parcels 1 & 4 as they have already been subdivided as individual lots. Parcels 2 & 3 will require a new Subdivision of the existing two parcels that will be impacted. As such the Development Area for Parcels 2 & 3, for the purposes of the storm water BMP requirements, will be the area of the two existing lots impacted by the new subdivision.

Parcel 1

As the +/- 6 acre Parcel 1 is currently undeveloped, any proposed project will trigger the BMP requirements outlined above. If a proposed at-grade detention facility is designed as a native vegetated wetland bottom basin, all BMP requirements will be satisfied. If a wetland bottom detention basin is not used, BMPs such as vegetated filter strips, bioswales, and/or pervious pavers would need to be incorporated into the site plan to meet the BMP requirements. These at grade BMPs would have a detention volume component that would allow us to reduce the detention volume required to be stored underground. An additional option would be the use of a mechanical BMP system. For a site of this size a mechanical BMP system would likely cost between \$15,000 and \$20,000 and would require ongoing maintenance.

Parcel 2 & 3

As discussed at the July 10th meeting with the Village, it is our understanding that Parcels 2&3 is the only portion of the property to be subdivided as part of the planned development. Since this portion of the property is to be subdivided we understand that the impervious area of these lots prior to February 15, 1992 (date of Stormwater Ordinance adoption) must be

compared to the current impervious area. The two lots to be subdivided are actually owned by separate entities (JCP & YTC) and are shown on the attached exhibits labelled "1988 & 2015 Yorktown Center Aerial Exhibit". When comparing 1988 aerial photography to the 2015 aerial photography it is apparent that some development has occurred on the two lots in question. However, there has been no appreciable increase in impervious area since February 15, 1992. For this reason we have defined a 'Development Area' so that we can measure the net change in the impervious area as a result of the proposed development.

On the attached "Pervious/Impervious Area – Parcels 2 & 3 Development Area Exhibit" we have drawn a conservative "Development Area" boundary for the purposes of determining an impervious area baseline. Per the exhibit the Development Area is 17.26 acres with 687,087 sf of impervious area and 64,949 sf of pervious area. The impervious area that the proposed site plan must stay below to avoid triggering the BMP requirements is 689,586 sf. If this impervious area is exceeded BMP's such as vegetated filter strips, bioswales, and/or pervious pavers will be required. At-grade BMP's such as these would provide some storm water detention capacity which would reduce the volume required to be stored underground. An additional option would be the use of a mechanical BMP system. For a site of this size two mechanical BMP units would likely be required at a likely cost between \$30,000 and \$40,000 and would require ongoing maintenance.

While Parcels 2&3 have a much greater percentage of pervious area compared to the northwest parcel, we believe a site plan that will fit within the above parameters to avoid triggering the BMP requirements is very achievable. If the primary development area begins to approach the threshold, the large parking fields to the northwest and northeast of JCPenney offer opportunities to add some strategic landscape islands to avoid triggering the BMP requirements.

Parcels 4

The 9.64 acre Parcel 4 is currently developed and per the attached "Pervious/Impervious Area –Parcel 1 Exhibit" has 400,528 sf of impervious area and 19,478 sf of pervious area. The existing site is 95% impervious, a number that would most likely be reduced by any proposed site plan. The impervious area that the proposed site plan must stay below to avoid triggering the BMP requirements is 403,027 sf. If this impervious area is exceeded BMP's such as vegetated filter strips, bioswales, and/or pervious pavers will be required. At-grade BMP's such as these would provide some storm water detention capacity which would reduce the volume required to be stored underground. An additional option would be the use of a mechanical BMP system. A mechanical BMP could be installed in a manhole/structure downstream of the detention facility to comply with the BMP requirements. For a site of this size a mechanical BMP system would likely cost between \$17,500 and \$25,000 and would require ongoing maintenance.

Based on current Village Code requirements for setbacks and landscaping, we feel it is highly unlikely that a proposed site plan would increase the impervious area of this parcel triggering the BMP requirements.

VI. Offsite Drainage Conveyance

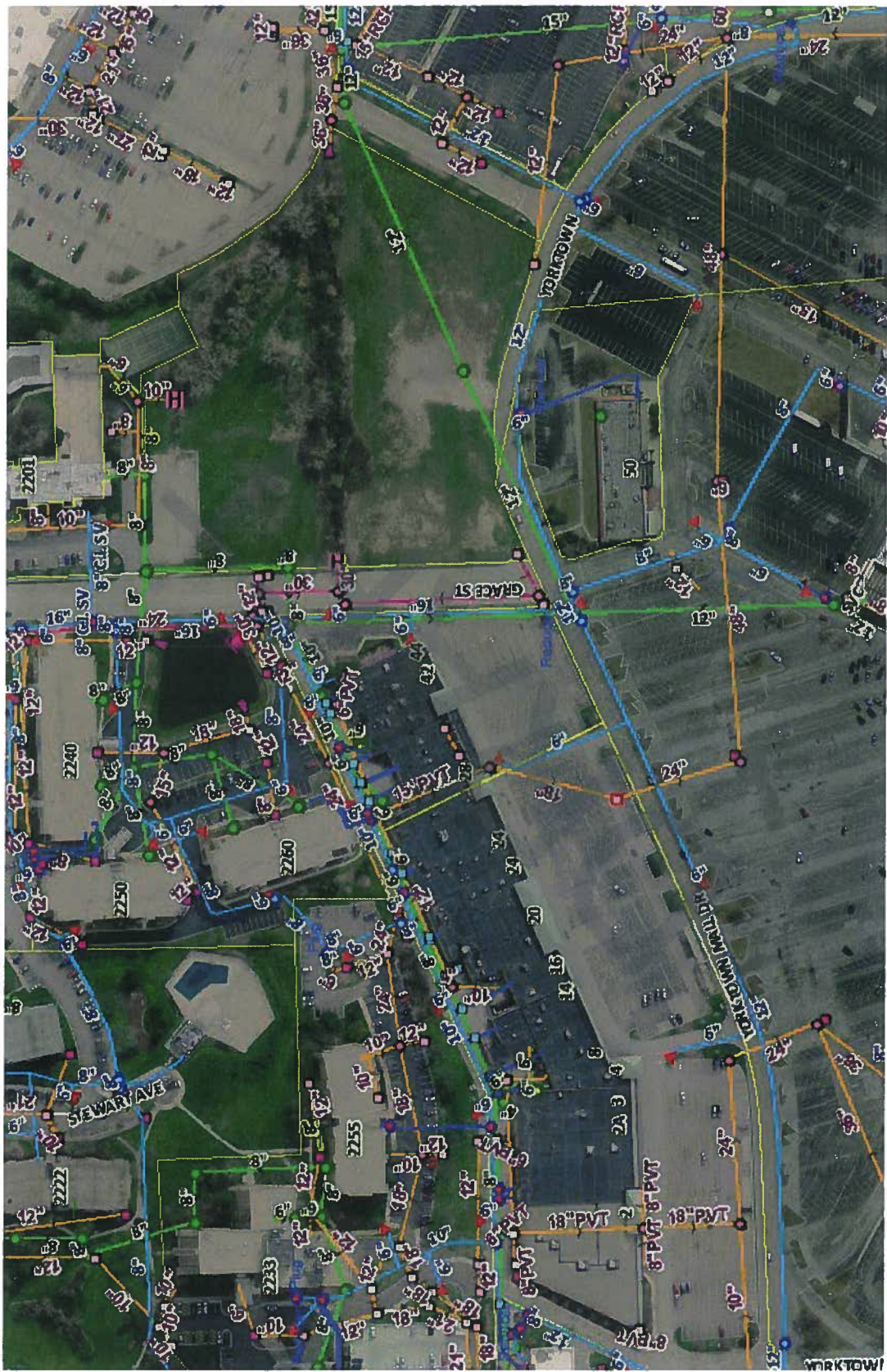
Illinois Drainage Law states that private landowners have certain rights to improve the drainage on their land as follows:

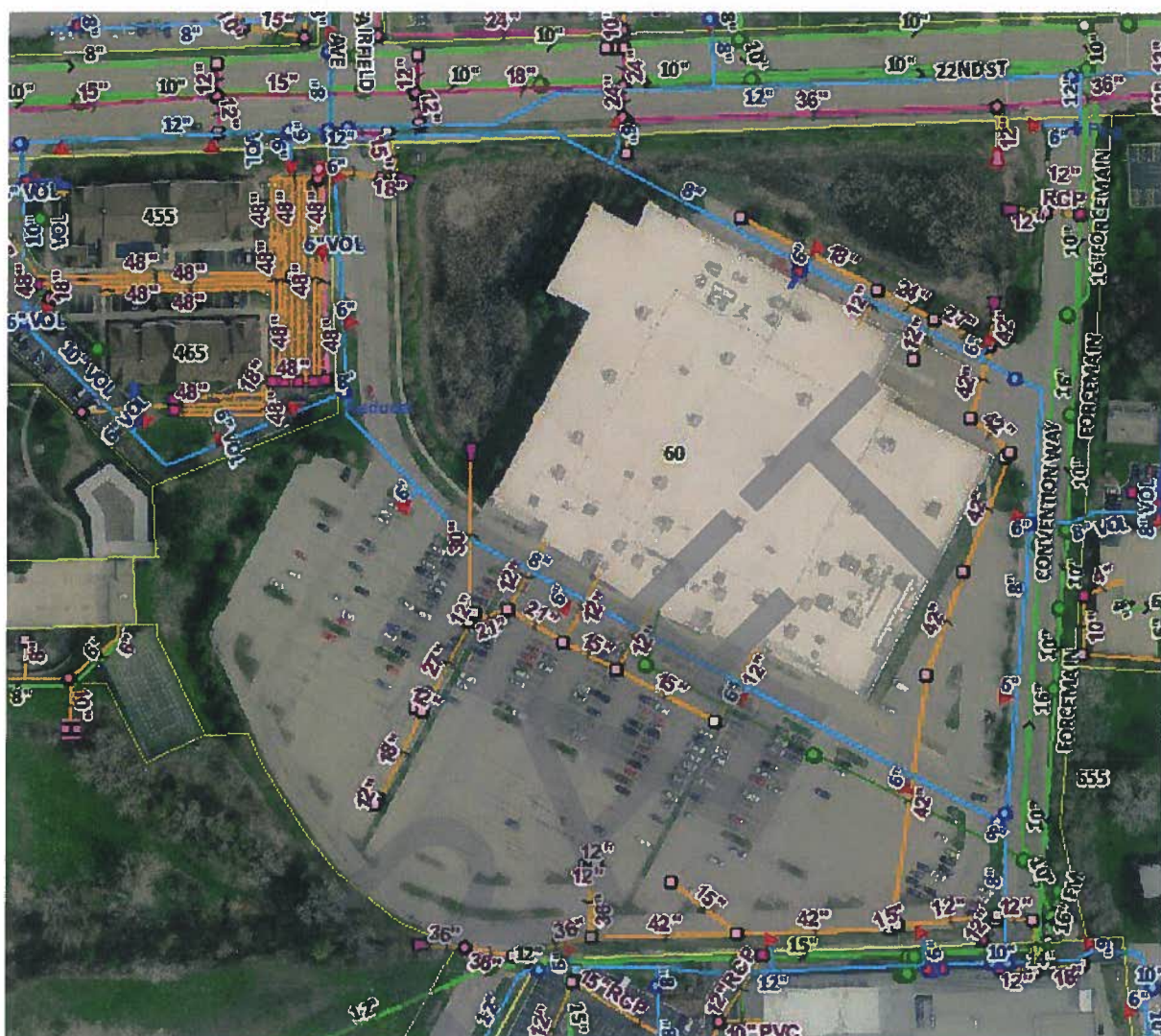
1. They can widen, deepen, and clean natural depressions that carry their surface water.
2. They can straighten out channels on their own property and accelerate the movements of surface water so long as they do not change the natural point of entry on lower land or unreasonable increase the flow onto servient tenements.
3. They can drain ponds or standing water in the direction of their overflow.
4. Expedite the flow of surface waters through natural lines of drainage by either open or closed drains into a water-course or stream.

The drainage law referenced above applies to all parcels on the Yorktown Center site. Generally speaking, piped storm water that flows to development parcels will continue to be piped. In this area storm sewers are generally designed to convey the 10-year storm event. Greater storm events must also be safely conveyed through the property, but those waters can flow overland across pavement and landscape areas.

The undeveloped Parcel 1 is impacted the most due to the ditches that convey significant volumes of water through the middle of the site. The information provided by the Village shows that there are four residential developments on the west side of Grace Street that are tributary to the 30" pipe that outlets to the upstream end of the ditch that bisects Parcel 1. The total area of these developments is 27 acres. The Village research shows that two of the properties provided detention with a restricted release, while the other two properties appear to have an unrestricted release to the ditch. Based on our calculations the piped offsite flow from the west for the 10-year storm event is 35 cfs. As part of the proposed plan this flow will be piped through or around Parcel 1, likely in a 36" pipe. In addition the 2.75 acre Yorktown Condos development immediately to the north of the property has a piped release to the site through a 10" pipe. We would propose to pipe this flow along the northeast property line to the existing outlet. See the attached Offsite Drainage Area Exhibit.

To gain an understanding of the drainage route for the overland overflow from the properties to the west the DuPage County 2' Contour Map was reviewed. It was determined that while nearly 27 acres west of Grace Street are tributary to Parcel 1 via the storm sewer system, only 14.8 acres are tributary via overland overflow. As part of the proposed plan the site shall be designed to safely convey this offsite flow overland through or around the property while ensuring that it ultimately drains to the same location as currently designed. Based on our preliminary calculations we believe that this offsite overflow can be safely conveyed through either a parking lot or a vegetated swale. It is important that the site plan accounts for the location where the overflow enters and exits the site to provide a parking lot configuration or swale location that allows for this conveyance through the site. See the Offsite Overland Overflow Exhibit.







VILLAGE OF LOMBARD

255 E. Wilson Ave.
Lombard, Illinois 60148
630/620-5700 FAX: 630/620-8222
TDD: 630/620-5812
www.villageoflombard.org

Village President
William J. Mueller

Ms. Diane Schulte
Senior Vice President of Real Estate
AMC Theaters
920 Main Street
Kansas City, MO 64105

RE: Yorktown AMC Land Lease

Trustees

Tyler L. Williams, Dist. 1
Richard J. Tross, Dist. 2
Karen S. Koenig, Dist. 3
Steven D. Sebby, Dist. 4
Kenneth M. Florey, Dist. 5
Rick Soderstrom, Dist. 6

Village Manager
William T. Lichter

Dear Ms. Schulte,

I am writing to you on behalf of the Village of Lombard. The Village has created the Lombard Public Facility Corporation for the purpose of building a hotel and conference center at the north end of the detention pond adjacent to AMC Yorktown. The Village's 500-room hotel will be operated by Westin Hotel and it will have a 17,000 square foot Gibson's Restaurant. The hotel/conference center will have 50,000 square feet of meeting space and 15,000 square feet of pre-function space.

The purpose of my letter is to express the Village's desire to raise the existing detention pond weir as a means to create additional detention capacity in the detention pond. By raising the weir, the Village would be able to satisfy the detention requirements for our development. However, raising the weir would impact six areas at the perimeter of your parking lot. These areas would have ponding occurring in them during severe storm events. I am enclosing a map identifying these areas.

At the peak 100-year (1% annual probability) storm event's elevation of 730.7 feet, eight inches of ponding would occur in the southeast corner of the lot. Six inches of ponding would occur at the main drive aisle just north of the right-in, right-out entrance. There would be nine inches of ponding at the south end and seven inches at the north end of the drive aisle behind the theater, and up to five inches to the north of the theater. Twelve parking spaces would be impacted. The ponding in these areas would recede within seven hours after the rainfall ceases. I am also enclosing a hydrograph representing this data.

I would greatly appreciate it if you would contact me at your first opportunity so that we might discuss this matter. Please be advised that I am willing to travel to your offices to review this matter and provide you and others with a presentation of our project. Your theater will benefit greatly from the Village's hotel/conference center by bringing additional visitors to the area who will be seeking entertainment venues. Also, it is very likely that there will exist the desire to rent some of your theaters for meeting room space for larger conference events.

I look forward to hearing from you so that we may expedite an agreement to facilitate this issue. Please contact me at 630-620-5756.

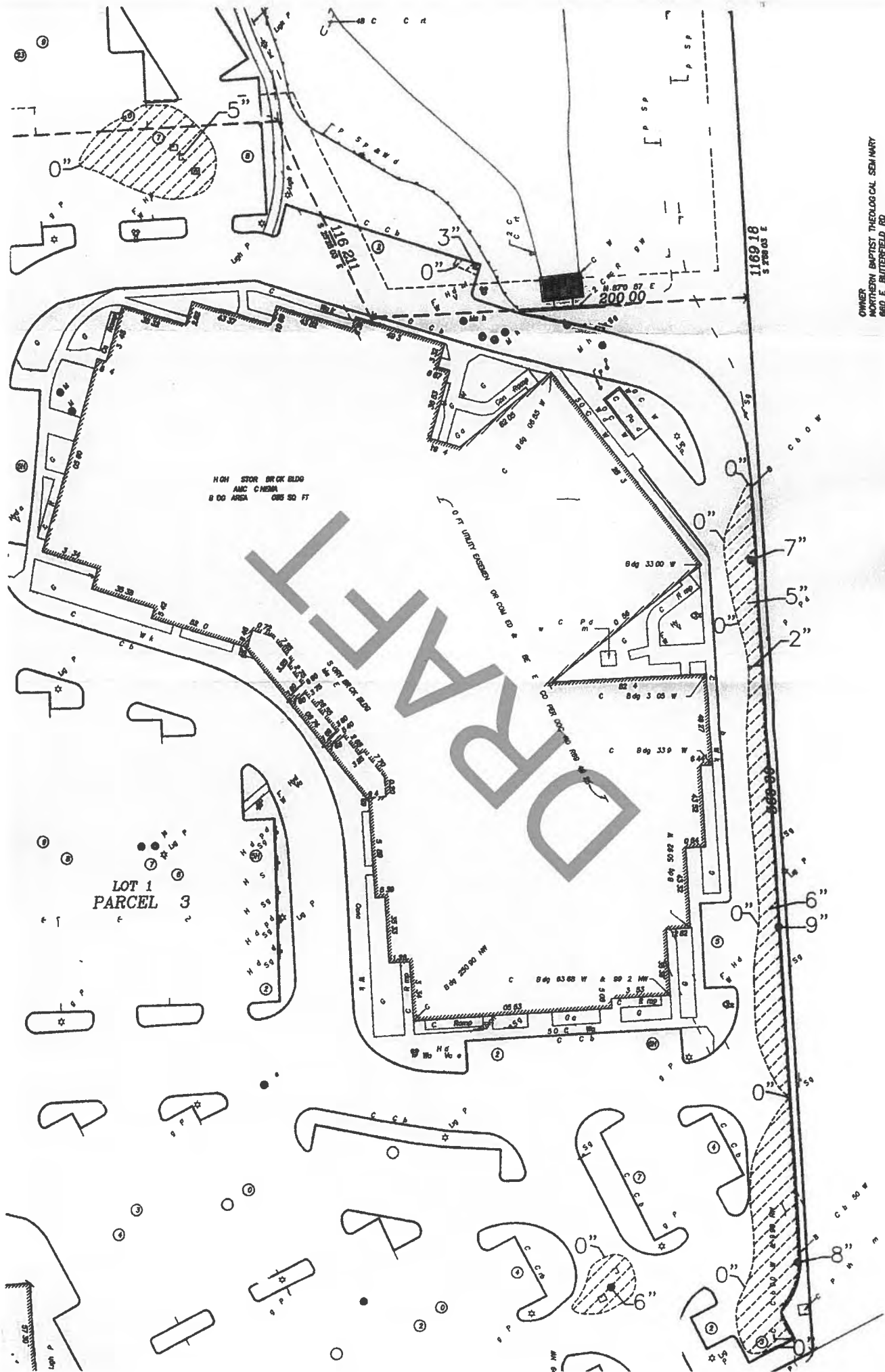
Sincerely,

David A. Hulseberg

David A. Hulseberg, AICP
Director of Community Development

"Our shared *Vision* for Lombard is a community of excellence exemplified by its government working together with residents and business to create a distinctive sense of spirit and an outstanding quality of life."

"The *Mission* of the Village of Lombard is to provide superior and responsive governmental services to the people of Lombard."



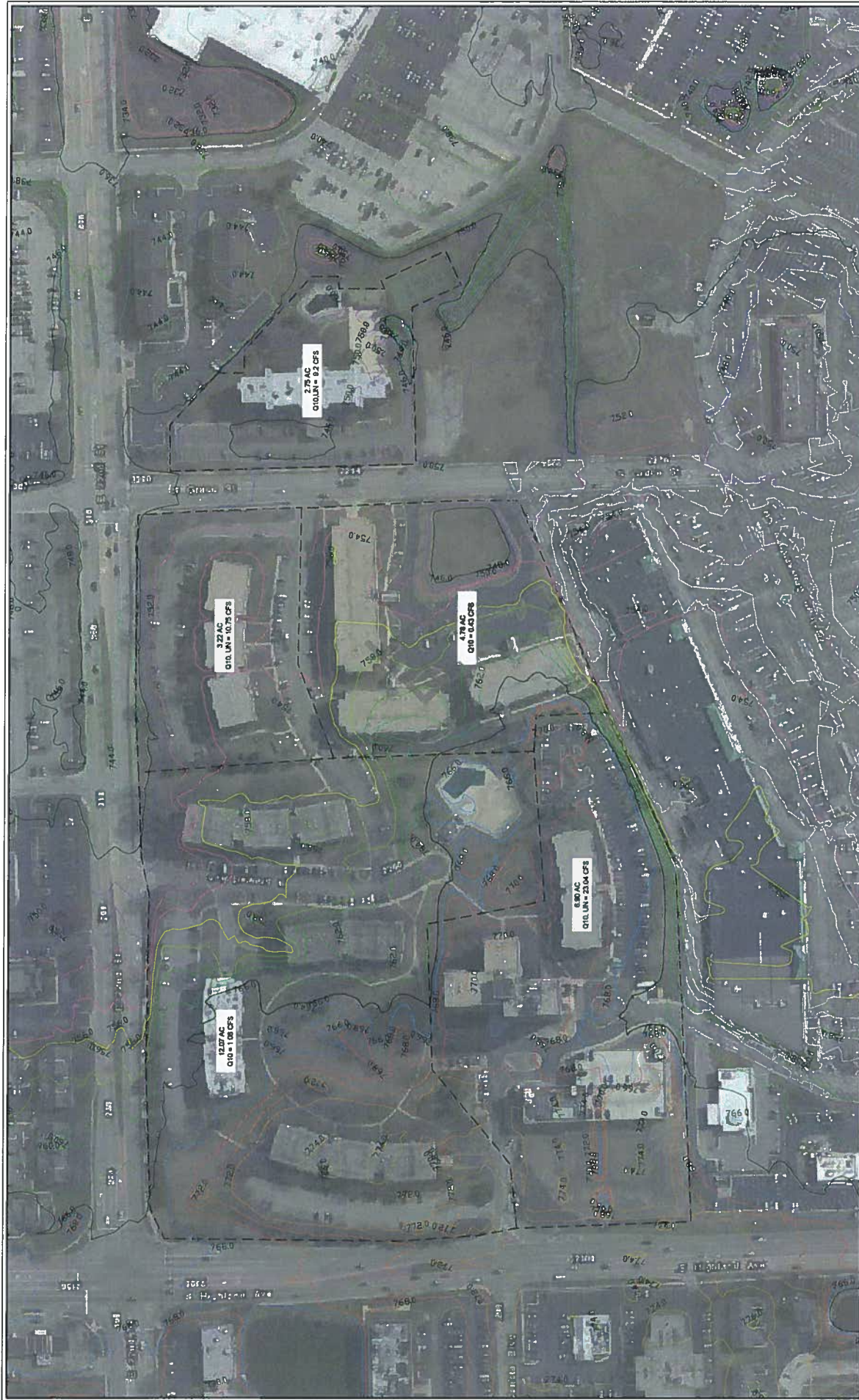
OWNER
NORTHERN BAPTIST THEOLOGICAL SEMINARY
680 E BUTTERFIELD RD



	<p>YORKTOWN MIXED USE DEVELOPMENT</p>	<p>PERVIOUS / IMPERVIOUS AREA - PARCEL 4</p>	<p>ILLINOIS</p>
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V360 Companies
 7325 W. 111th Avenue
 Woodridge, IL 60817
 630.724.9200 phone
 630.724.9202 fax
 www.v360.com

V360, V360S, V360S+ are trademarks of V360 Companies, Inc.



REVISIONS

NO.	DATE	DESCRIPTION

PROJECT INFO

PROJECT NO.: 15147

DATE: 07-28-11

SCALE: 1" = 60'

CLIENT INFO

CLIENT: V&S

PROJECT: YORKTOWN COMMONS DEVELOPMENT

DESIGNER INFO

DESIGNER: V&S

PROJECT: YORKTOWN COMMONS DEVELOPMENT

YORKTOWN COMMONS DEVELOPMENT

ILLINOIS LOMBARD

OFFSITE DRAINAGE AREA EXHIBIT

EX1

V&S Companies
7328 James Avenue
Westridge, IL 60017
Phone: 815.724.8202
Fax: 815.724.8203
www.vsc.com

NOTES: 1. ALL DRAINAGE AREAS SHOWN ARE BASED ON 100-YEAR FLOOD DATA.



LEGEND

- PERVIOUS AREA
64,949 SF (1.49 AC)
- IMPERVIOUS AREA
687,087 SF (15.77 AC)
- DEVELOPMENT AREA : 752,036 SF (17.26 AC)

BASED ON TOPOGRAPHIC SURVEY FROM GREMLY & BIEDERMANN, DATED NOVEMBER 16, 2013



PERVIOUS / IMPERVIOUS AREA -
PARCELS 2 & 3 DEVELOPMENT AREA

ILLINOIS

YORKTOWN COMMONS
LOMBARD

V3 Companies
7326 Jones Avenue
Naperville, IL 60563
630.724.9200 phone
630.724.9202 fax
www.v3co.com

V3

VISION. VENTURE. VICTORY... "The Vision is Transform with Excellence"



 V2 Corporate 10000 W. 10th Ave., Suite 100 Denver, CO 80231 303.733.8888 www.v2corp.com		REVISIONS REV. DATE: _____ REV. BY: _____ REV. DATE: _____ REV. BY: _____ REV. DATE: _____ REV. BY: _____		YORKTOWN CENTER REDEVELOPMENT BLANDS LOMBARD		1988 YORKTOWN CENTER AERIAL EXHIBIT		EX	
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Vista Company
1000 North 10th Street
Suite 100
Chicago, IL 60610
800.774.0000
www.vista.com

REVISIONS

NO.	DATE	DESCRIPTION
1	08/01/2018	ISSUED FOR PERMIT

YORKTOWN CENTER REDEVELOPMENT

ELANDS

LOMBARD

2015 YORKTOWN CENTER AERIAL EXHIBIT

EX

EXHIBIT D – “WETLAND DELINEATION AND ASSESSMENT REPORT”

**WETLAND
DELINEATION AND
ASSESSMENT REPORT**



PROJECT SITE:

**YORKTOWN MIXED USE DEVELOPMENT
LOMBARD, DuPAGE COUNTY, ILLINOIS**

PREPARED FOR:

CONTINUUM PARTNERS, LLC
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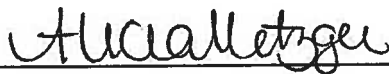
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AUGUST 12, 2015

We hereby certify that this Wetland Delineation and Assessment Report has been prepared by V3 Companies for use by Continuum Partners, LLC, their affiliates, lenders, and assignees.

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EXECUTIVE SUMMARY

The approximately 5.88-acre subject property was investigated by V3 Companies (V3) on July 15, 2015 to determine the presence, extent and quality of any wetlands or other areas under U.S. Army Corps of Engineers (USACE) and/or DuPage County jurisdiction.

Delineation Summary.

No wetland areas, Waters of DuPage, or Waters of the U.S. were delineated on the subject property. One area (Area 1) was identified in the southern and eastern portion of the subject property and consists of two constructed drainage ditches excavated in upland soils that did not satisfy the criteria to qualify as wetland, Waters of the U.S., or Waters of DuPage. The identified incised ditches appear to have been excavated sometime between 1962 and 1974, as shown on historical aerial imagery (**Appendix VI**). A previous wetland delineation report conducted in 1995 for the adjacent Target property and concurrence from the DuPage County Department of Economic Development and Planning and USACE for that delineation show that these areas did not qualify as wetland, Waters of DuPage, or Waters of the U.S. and were considered non-regulatory ditches (**Appendix VI**). A summary of the data points and identified areas is provided in Table 1.

Table 1. Data Point Summary Table.

Area	Data Point	Hydrophytic Vegetation?	Hydric Soils?	Wetland Hydrology?	Wetland (Y/N)
1	X01	N	N	Y	N
	X04	Y	N	Y	
	X06	N	N	Y	
	X07	Y	N	Y	
2	X02	Y	N	N	N
	X05	N	N	N	
3	X03	Y	N	N	N

In V3's professional opinion, Area 1 consists of two constructed drainage ditches excavated in upland soils and does not qualify as wetland, Waters of DuPage, or Waters of the U.S. No other wetlands, Waters of DuPage, or Waters of the U.S. were identified within 100-feet of the subject property per the DuPage County Ordinance. The non-regulatory excavated drainage ditches were field verified by Ms. Jen Boyer of the DuPage County Stormwater Management (SWM) on July 28, 2015 and Ms. Stasi Brown of the USACE on August 4, 2015.

Regulatory Summary.

Pursuant to Section 404 of the Clean Water Act, the U. S. Army Corps of Engineers (USACE) has jurisdiction over the placement of fill or dredged material in all jurisdictional waters of the United States, including wetlands. The property does not contain any wetlands or waters of the US. Therefore, a Section 404 permit from the USACE is not required for the development of the property

Pursuant to the 2013 *DuPage County Countywide Stormwater and Flood Plain Ordinance* (Ordinance), any development that affects a special management area (i.e., floodplain, wetland, wetland buffer, or waterway buffer) requires a stormwater management review and certification by DuPage County. The property does not contain any special management areas and does not require DuPage County certification. Information concerning applicable regulatory requirements is provided in **Appendix III**.

INTRODUCTION AND BACKGROUND

The approximately 5.88-acre subject property was investigated by V3 Companies (V3) on July 15, 2015 to determine the presence, extent and quality of any wetlands or other areas under U.S. Army Corps of Engineers (USACE) and/or DuPage County jurisdiction. Any identified wetland boundaries are marked in the field using pink wire flags labeled "Wetland Delineation" and numbered consecutively from beginning to end. This report summarizes the results of the field investigation and provides technical documentation for all investigated areas. The non-regulatory excavated drainage ditches were field verified by Ms. Jen Boyer of the DuPage County Stormwater Management (SWM) on July 28, 2015 and Ms. Stasi Brown of the USACE on August 4, 2015.

The subject property is located north of Yorktown Convenience Center, south of E. 22nd Street east of Grace Street, and west of Yorktown Shopping Center, in Lombard DuPage County, Illinois (Section 29, T39N, R11E; 41.843460 °N, -88.005060°W; Wheaton quadrangle, Figure 1).

No wetland areas are mapped on the subject property on the National Wetlands Inventory (NWI) map (Figure 2).

No wetland areas are mapped on the subject property on the DuPage County Advanced Identification of Wetlands (ADID) map (Figure 3).

The USGS Hydrologic Atlas (Figure 4) does not show the presence of any streams or creeks on the subject property.

The 12-Digit Hydrologic Unit Code (HUC) Map (Figure 5) shows that the subject property lies within the Lower Salt Creek sub watershed (Hydrologic Unit 071200040404), which is associated with the larger Des Plaines River watershed.

The FEMA Flood Insurance Rate Map (FIRM) (Figure 6) does not identify any areas on the subject property that are within a flood zone.

The DuPage County Regulatory Flood Map (RFM) (Figure 7) does not identify any areas on the subject property that are within a flood zone.

Three soil series were mapped within the subject property by the Natural Resources Conservation Service (USDA, NRCS 2014; Figure 8). These soils include Ashkum silty clay loam (232A), Orthents, loamy, undulating (802B), Orthents, clayey, undulating (805B). Ashkum silty clay loam is listed as a hydric soil in DuPage County, Illinois.

Figure 9, a DigitalGlobe aerial photograph (2014) shows the location of all sampling points and the locations of the identified areas as collected via a handheld GPS unit.

WETLAND DELINEATION METHODS

Wetland delineations are conducted following the methods given in the *Regional Supplement to the Corps of Engineers Wetlands Delineation Manual: Midwest Region*. Under the delineation procedures in this manual, an area must exhibit characteristic hydrophytic vegetation, hydric soils, and wetland hydrology to be considered a wetland. If field investigation determines that any of the three parameters are not satisfied, the area usually does not qualify as wetland. Moreover, drainage ditches excavated in dry land are generally not considered jurisdictional waters of the United States by the Corps of Engineers (preamble to 33 CFR Parts 320 through 330, *Federal Register* Vol. 56, No. 219, 41217).

As part of a delineation report, data forms and technical information are required by the U.S. Army Corps of Engineers to document the three parameters for any area determined to be wetland. Data forms for wetlands identified at the subject property are provided in **Appendix I**. The vegetation data calculated on the data forms reflects the changes made to the National Wetland Plant List as of June 1, 2012. Representative photographs of delineated wetlands are provided in **Appendix II**. A brief description of the field methods used and a description of the three wetland parameters are provided in **Appendix IV**.

Plant species lists are compiled for each area identified, focusing on the plant communities within each identified wetland area. This accumulated floristic data is analyzed using the Floristic Quality Assessment (FQA) methodology, which is an assessment technique that was developed for a rapid quality evaluation of vegetation in a defined area. Technical names in the FQA and this report follow the nomenclature of *The National Wetland Plant List: 2014 Update of Wetland Ratings* (Lichvar *et. al.*, 2014). A detailed explanation of the Floristic Quality Assessment method is provided in **Appendix IV**.

As part of the wetland delineation assessment, Illinois Department of Natural Resources (IDNR) and US Fish and Wildlife Service (USFWS) threatened and endangered species evaluations were conducted (**Appendix V**).

The IDNR confirmed that the Illinois Natural Heritage Database contains no record of State-listed threatened or endangered species, Illinois Natural Area Inventory sites, dedicated Illinois Nature Preserves, or registered Land and Water Reserves in the vicinity of the project location. A copy of the termination letter from the IDNR is included in **Appendix V**.

The USFWS Section 7 consultation did not find species or critical habitat present on the subject property. A copy of the USFWS Section 7 consultation is included in **Appendix V**.

RESULTS OF THE FIELD INVESTIGATION

NON- JURISDICTIONAL AREAS

Area 1 – Constructed Drainage Ditches

Data Points X01, X04, X06, and X07

Area 1 was identified in the southern and eastern portion of the subject property and consists of two constructed drainage ditches excavated in upland soils that did not satisfy the criteria to qualify as wetland, Waters of the U.S., or Waters of DuPage. The ditches appear to have been excavated sometime between 1962 and 1974, as shown on historical aerial imagery (**Appendix VI**). A previous wetland delineation report conducted in 1995 for the adjacent Target property and concurrence from the DuPage County Department of Economic Development and Planning and USACE for that delineation show that these areas did not qualify as wetland, Waters of the U.S., or Waters of DuPage and were considered non-regulatory ditches (**Appendix VI**).

Summary:

- Constructed Drainage Ditches
- Jurisdiction: N/A
- Quality: N/A
- Vegetated Buffer Required: N/A

Vegetation:

- There was no vegetation present at Data Point X01, so the soils criterion is not satisfied.
- The dominant plant species at Data Point X04 was eastern cottonwood (*Populus deltoides*). 100% of the dominant species are hydrophytic, so the vegetation criterion is satisfied.
- There was no vegetation present at Data Point X06, so the soils criterion is not satisfied.
- The dominant plant species at Data Point X07 are eastern cottonwood (*Populus deltoides*) and common buckthorn (*Rhamnus cathartica*). 100% of the dominant species are hydrophytic, so the vegetation criterion is satisfied.

The floristic quality data and plant species inventory for Area 1 are provided below.

Conservatism-Based Metrics		Additional Metrics	
Mean C (native species)	2.00	Species Richness (all)	3
Mean C (all species)	1.33	Species Richness (native)	2
Mean C (native trees)	2.00	% Non-native	33%
Mean C (native shrubs)	0.00	Wet Indicator (all)	0.00
Mean C (native herbaceous)	n/a	Wet Indicator (native)	0.00
FQAI (native species)	2.83	% hydrophyte (Midwest)	100%
FQAI (all species)	2.31	% native perennial	67%
Adjusted FQAI	16.33	% native annual	0%
% C value 0	33%	% annual	0%
% C Value 1-3	67%	% perennial	100%
% C value 4-6	0%		
% C value 7-10	0%		

Species Acronym	Species Name (NWPL/Mohlenbrock)	Synonym	Common Name	C Value	Midwest WET Indicator	WET Indicator (numeric)	Habit	Duration	Nativity
popdel	Populus deltoides	Populus deltoides	Eastern Cottonwood	2	FAC	0	Tree	Perennial	Native
rhacat	Rhamnus cathartica	RHAMNUS CATHARTICA	European Buckthorn	0	FAC	0	Shrub	Perennial	Adventive
rhurad	Toxicodendron radicans	Rhus radicans	Eastern Poison-Ivy	2	FAC	0	Vine	Perennial	Native

Soils:

- The soil profile at Data Point X01 consisted of gravel, so the soils criterion is not satisfied.
- The soil profile at Data Point X04 consisted of 0-12 inches of very dark grayish brown (10YR 3/2) silty clay loam. Hydric soil indicators were not observed, so the soils criterion is not satisfied.
- The soil profile at Data Point X06 consisted of gravel, so the soils criterion is not satisfied.
- The soil profile at Data Point X07 consisted of 0-10 inches of mixed fill consisting of very dark grayish brown (10YR 3/2) silty clay loam. Below that, to a depth of 18 inches below the surface, the soil profile consisted of mixed fill containing 50% very dark grayish brown (10YR 3/2) and 50% brown (10YR 5/4) silty clay loam. Hydric soil indicators were not observed, so the soils criterion is not satisfied.

Hydrology:

- The area at Data Point X01 was inundated to a depth of 3 inches, so the hydrology criterion is satisfied.
- The area at Data Point X04 was inundated to a depth of 1 inch, so the hydrology criterion is satisfied.
- The area at Data Point X06 was inundated to a depth of 1 inch, so the hydrology criterion is satisfied.
- The presence of two secondary wetland hydrology indicators, B10, Drainage Patterns, and D2, Geomorphic Position, at Data Point X07 satisfies the hydrology criterion.

Conclusion: Data Points X01 and X06 do not satisfy the vegetation and soils criterion and Data Points X04 and X07 do not satisfy the soils criterion; therefore Area 1 does not qualify as wetland. In V3's professional opinion, Area 1 does not qualify as a regulatory waterway because it was excavated in upland soils for stormwater conveyance purposes and was previously concurred upon by EDP and USACE to be not regulatory.

ADDITIONAL AREAS INVESTIGATED

Area 2 – Upland

Data Points X02 and X05

Area 2 consists of the upland areas adjacent to Area 1.

Vegetation:

- The dominant plant species at Data Point X02 is common buckthorn (*Rhamnus cathartica*). 100% of the dominant species are hydrophytic, so the vegetation criterion is satisfied.
- The dominant plant species at Data Point X05 are chokecherry (*Prunus virginiana*) and common buckthorn (*Rhamnus cathartica*). Only 50% of the dominant species are hydrophytic, so the vegetation criterion is not satisfied.

Soils:

- The soil profile at Data Point X02 consisted of 0-10 inches of brown (10YR 4/3) gravelly silty clay loam. Hydric soil indicators were not observed, so the soils criterion is not satisfied.
- The soil profile at Data Point X05 consisted of 0-15 inches of very dark grayish brown (10YR 3/2) silty clay loam. Hydric soil indicators were not observed, so the soils criterion is not satisfied.

Hydrology:

- Neither primary nor secondary wetland hydrology indicators were observed at Data Point X02, so the hydrology criterion is not satisfied.
- Neither primary nor secondary wetland hydrology indicators were observed at Data Point X05, so the hydrology criterion is not satisfied.

Conclusion: Data Point X02 does not satisfy the soils and hydrology criteria and Data Point X05 did not satisfy the three criteria; therefore Area 2 does not qualify as wetland.

Area 3 – Upland

Data Point X03

Area 3 consists of an upland area located north of Area 1.

Vegetation: The dominant plant species at Data Point X03 are redtop (*Agrostis gigantea*), common plantain (*Plantago major*), white clover (*Trifolium repens*), and sidewalk knotweed (*Polygonum aviculare*). 75% of the dominant species are hydrophytic, so the vegetation criterion is satisfied.

Soils: The soil profile at Data Point X03 consisted of 0-6 inches of very dark grayish brown (10YR 3/2) silty clay loam underlain by 6 inches, to a depth of 12 inches below the surface, of brown (2.5Y 4/4) silty clay loam. Hydric soil indicators were not observed, so the soils criterion is not satisfied.

Hydrology: Neither primary nor secondary wetland hydrology indicators were observed, so the hydrology criterion is not satisfied.

Conclusion: Area 3 fails to satisfy the soils and hydrology criterion and does not qualify as wetland.

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EXHIBIT E – “CIVIL DESIGN GUIDELINES”

**CIVIL DESIGN
GUIDELINES –
YORKTOWN
COMMONS**



PROJECT SITE:

**YORKTOWN COMMONS AT YORKTOWN CENTER
VILLAGE OF LOMBARD, DUPAGE COUNTY, ILLINOIS**

PREPARED FOR:

**ROSANOVA & WHITAKER, LTD.
30 W. JEFFERSON AVENUE, SUITE 200
NAPERVILLE, IL 60540**

PREPARED BY:

**V3 COMPANIES, LTD.
7325 JANES AVENUE
WOODRIDGE, ILLINOIS 60517
630.724.9200**

OCTOBER 19, 2015

Yorktown Commons
Civil Design Guidelines
October 19, 2015

Project Overview

The intent of this project is to provide four mixed-use development-ready parcels (see Yorktown Commons Location Map) with approved Civil Design Guidelines as outlined in this report. The project will include the demolition of the existing buildings on Parcels 2 and 4; the demolition of existing parking lots and other site improvements on all four parcels, and; improvements to the common areas adjacent to the four parcels.

Common area improvements will include landscaping and lighting along Grace Street, the Yorktown Center Ring Road and into the north end of the Yorktown Center property. Pedestrian and bicycle improvements will provide connectivity between the four parcels, the mall, and the existing properties that surround the redevelopment area.

This report will serve as the Civil Design Guidelines for future residential and mixed-use developments that will ultimately be proposed on each of the parcels.

Parcel 1

Demolition

- The parking lot located at the northwest corner of the property will be removed. This will require some coordination with the Yorktown Condominiums to the north as the curb cut for this parking lot is on their property.
- There is an existing 10" storm sewer that outlets onto Parcel 1 near the north property line. This pipe will be removed and this flow will be required to be re-routed around or through the site.
- A segment of the 30" storm sewer outlet that is located near the existing ditch will be removed.
- The 12" sanitary sewer that cuts through the site from southwest to northeast will be removed and re-routed around the perimeter of the property as shown on the attached Demolition Plan – Parcel 1.
- The property in general, especially along the ditch, will require clearing and grubbing in advance of earthwork activities.
- The existing curb and gutter around the property is intended to be maintained and repaired only as required.
- There are four light poles along Grace Street to be removed.

Layout & Paving

- Roadway and paving design and construction shall comply with Chapter 500 of the Village of Lombard Specifications Manual, the "Illinois Department of Transportation (IDOT) "Standard Specifications for Road and Bridge Construction", and the "Supplemental Specifications and Recurring Special Provisions", latest editions and updates (IDOT).
- Recycled asphalt shall be allowed so long as it is in conformance with IDOT Specifications.
- Site access is to be from Grace Street, the Yorktown Center Ring Road, and the entrance drive to Target. Access points are to be approved by the Village and Yorktown Center.

- Sidewalk will be installed along the Yorktown Center Ring Road and the driveway along the southeast property line. Sidewalks will comply with the Illinois Accessibility Code.
- Asphaltic pavement sections for driveways and parking lots shall consist of 4" of HMA over 8" of aggregate base.
- Concrete pavement sections for driveways shall consist of 7^{1/2}" of Portland Cement Concrete Pavement over 4" of aggregate base.
- Parking requirements are to be in accordance with the Planned Development Design Guidelines for Yorktown Commons.
- Site geometrics and density are to be in accordance with the Planned Development Design Guidelines for Yorktown Commons.

Sanitary Sewer

- Sanitary Sewer shall be in accordance with the "Illinois Recommended Standards for Sewage Works" as published by the IEPA, the "Standard Specifications for Water and Sewer Main Construction in Illinois" and the latest edition of the Village of Lombard Municipal Code.
- The 12" sanitary sewer that currently cuts through this parcel will be re-routed as shown on the attached Improvement Plan – Parcel 1. Future sanitary services or extensions for any proposed buildings on the property should ultimately tie into this new 12" sewer.

Water Main

- Water Main shall be in accordance with the "Standard Specifications for Water and Sewer Main Construction in Illinois" as well as the latest edition of the Village of Lombard Municipal Code.
- Fire protection shall be in accordance with the National Fire Prevention Code as well as the latest edition of the Village of Lombard Municipal Code.
- Existing 12" water mains run along the south side of the ring road and the east side of the roadway connection between this parcel and the Westin property. An existing 16" water main runs along the west side of Grace Street.
- A new water main loop shall be provided for this property. The new loop shall connect to the 12" water main along the west side of Grace Street, run through the property and connect to the 12" water main within the parkway in front of the Westin property as depicted on the attached Improvement Plan – Parcel 1.

Stormwater

- The stormwater management approach has been detailed in the attached Preliminary Stormwater Analysis – Yorktown Commons Redevelopment.
- Storm sewer shall be designed to convey the flow resulting from the 10-year storm event.
- 100-year storm events shall be conveyed through the property in accordance with the DuPage Countywide Stormwater Ordinance.
- Based on the Village's and County's requirements a proposed development on this parcel will require approximately 2.75 ac-ft of detention assuming a curve number (CN) of 92, which is consistent with an apartment or townhome development.
- An at-grade detention basin with a depth of 5' would require approximately 0.60 acres designated for detention.
- Alternatively, an underground detention system could be utilized. Such a system would likely have a footprint of 25,000 sf to 30,000 sf.
- The site/detention facility will have an allowable release rate of approximately 0.60 cfs, which will be conveyed to the existing 36" storm sewer that currently serves as the outlet for the existing ditch.

- A development creating 2,500 sf or greater of net new impervious area will require Best Management Practices (BMP's) in accordance with the DuPage Countywide Stormwater Ordinance. An appropriately designed at-grade native vegetated wetland bottom basin would satisfy all BMP requirements. If underground detention is provided, other BMPs such as vegetated filter strips, bioswales, and/or pervious pavers would need to be incorporated into the site plan to meet the BMP requirements. An additional option would be the use of a mechanical BMP system.
- Based on calculations the piped offsite flow from the west for the 10-year storm event is 35 cfs. As part of the proposed plan this flow will be piped through or around Parcel 2, likely in a 36" pipe. In addition the 2.75 acre Yorktown Condos development immediately to the north of the property has a piped release to the site through a 10" pipe. This flow should be routed along the northeast property line to the existing outlet.
- It was determined that while nearly 27 acres west of Grace Street are tributary to Parcel 2 via the storm sewer system, only 14.8 acres are tributary via overland overflow. This tributary area results in a 100-year flow of approximately 84 cfs. As part of the proposed plan the site shall be designed to safely convey this offsite flow overland through or around the property.

Lighting

- Lighting shall meet Village of Lombard and Yorktown Center requirements.
- Lighting electric design shall comply with the National Electric Code.

Private Utilities

- AT&T provides cable, telephone, and data to the project area. Ryan Kennelly (630-573-5681) should be contacted to initiate coordination with AT&T.
- COMCAST is a local broadband provider for the area. Contact Denise Corbin (847-789-0638) with installation/coordination questions.
- ComEd is the local electric provider. Call the New Business hotline to initiate project coordination.
- NICOR is the local natural gas provider. Contact Daniel McEvilly (New Business Coordinator 815-272-9251) to initiate project coordination.

Parcel 2

Demolition

- The existing approximately 10,500 sf building will be demolished.
- Curb and gutter removal will be required as needed to accommodate the proposed site plan.
- The storm sewer, sanitary sewer, and water main service connections to the building will be removed and properly capped. See attached Demolition Plan – Parcel 2.
- Tree removals will be required.
- The parking lots will be removed as needed and properly disposed of.
- 3 light poles and foundations will require removal.

Layout & Paving

- Roadway and paving design and construction shall comply with Chapter 500 of the Village of Lombard Specifications Manual, the "Illinois Department of Transportation (IDOT) "Standard Specifications for Road and Bridge Construction", and the "Supplemental Specifications and Recurring Special Provisions", latest editions and updates (IDOT).
- Recycled asphalt shall be allowed so long as it is in conformance with IDOT Specifications.

- Access to any proposed parking lot on this property shall be provided off of the existing drive along the south property line of the parcel and/or the Yorktown Center Ring Road or from the Yorktown Center parking lot. Access points are to be approved by the Village of Lombard and Yorktown Center.
- Sidewalk will be installed along the Yorktown Ring Road and the Grace Street extension south of the Ring Road. Sidewalks will comply with the Illinois Accessibility Code.
- Asphaltic pavements sections for driveways and parking lots shall consist of 4" of HMA over 8" of aggregate base.
- Concrete pavement sections for driveways shall consist of 7^{1/2}" of Portland Cement Concrete Pavement over 4" of aggregate base.
- Parking requirements are to be in accordance with the Planned Development Design Guidelines for Yorktown Commons.
- Site geometrics and density are to be in accordance with the Planned Development Design Guidelines for Yorktown Commons.

Water

- Water Main shall be in accordance with the "Standard Specifications for Water and Sewer Main Construction in Illinois" as well as the latest edition of the Village of Lombard Municipal Code.
- Fire protection shall be in accordance with the National Fire Prevention Code as well as the latest edition of the Village of Lombard Municipal Code.
- There is a 12" water main along the south side of the Ring Road (north of the property) and an 8" water main just west of the property. Water and/or fire service taps for any proposed development shall be made off of the 12" main to the north.
- The existing fire hydrant at the southeast corner of the property is to remain as will the hydrant just west of the property. Additional fire hydrants are not likely to be required for this property.

Sanitary

- Sanitary Sewer shall be in accordance with the "Illinois Recommended Standards for Sewage Works" as published by the IEPA, the "Standard Specifications for Water and Sewer Main Construction in Illinois" and the latest edition of the Village of Lombard Municipal Code.
- The 12" sanitary sewer main to the north provides capacity for this property. The sanitary service serving the existing building on this property should be inspected for possible re-use, otherwise a new connection to the 20' deep 12" main will be required.

Stormwater

- The stormwater management approach has been detailed in the attached Preliminary Stormwater Analysis – Yorktown Commons Redevelopment.
- Storm sewer shall be designed to convey the flow resulting from the 10-year storm event.
- The proposed development will result in a reduced runoff rate from this property. Therefore it is understood that the existing 48" storm sewer to which this property is tributary (south of the property) has adequate capacity to convey the 10-year storm event from this site.
- A development increasing the total impervious area by greater than 5,000 sf will require stormwater detention in accordance with Chapter 151 of the Village of Lombard Code of Ordinances and with the DuPage Countywide Stormwater Ordinance.
- A development creating 2,500 sf or greater of net new impervious area will require Best Management Practices (BMP's) in accordance with the DuPage Countywide Stormwater Ordinance. BMPs such as vegetated filter strips, bioswales, and/or pervious pavers

would need to be incorporated into the site plan to meet the BMP requirements. An additional option would be the use of a mechanical BMP system.

Lighting

- Lighting shall meet Village of Lombard and Yorktown Center requirements.
- Lighting electric design shall comply with the National Electric Code.

Private Utilities

- AT&T provides cable, telephone, and data to the project area. Ryan Kennelly (630-573-5681) should be contacted to initiate coordination with AT&T.
- COMCAST is a local broadband provider for the area. Contact Denise Corbin (847-789-0638) with installation/coordination questions.
- ComEd is the local electric provider. Call the New Business hotline to initiate project coordination.
- NICOR is the local natural gas provider. Contact Daniel McEvilly (New Business Coordinator 815-272-9251) to initiate project coordination.

Parcel 3

Demolition

- Curb and gutter removal will be required as needed to accommodate the proposed site plan.
- The parking lots will be removed as needed and properly disposed of.
- Tree removals will be required.
- Approximately 175 lf of 12" sanitary sewer at a depth of approximately 25 feet will be required to be removed and routed around the proposed building. See attached Demolition Plan – Parcel 3.

Layout & Paving

- Roadway and paving design and construction shall comply with Chapter 500 of the Village of Lombard Specifications Manual, the "Illinois Department of Transportation (IDOT) "Standard Specifications for Road and Bridge Construction", and the "Supplemental Specifications and Recurring Special Provisions", latest editions and updates (IDOT).
- Recycled asphalt shall be allowed so long as it is in conformance with IDOT Specifications.
- Access to any proposed parking lot on this property shall be provided from the existing Yorktown Center parking lot drive aisles. Access points are to be approved by the Village of Lombard and Yorktown Center.
- Sidewalk will be installed along the Yorktown Ring Road and the Grace Street extension south of the Ring Road. Sidewalks will comply with the Illinois Accessibility Code.
- Asphaltic pavements sections for driveways and parking lots shall consist of 4" of HMA over 8" of aggregate base.
- Concrete pavement sections for driveways shall consist of 7^{1/2}" of Portland Cement Concrete Pavement over 4" of aggregate base.
- Parking requirements are to be in accordance with the Planned Development Design Guidelines for Yorktown Commons.
- Site geometrics and density are to be in accordance with the Planned Development Design Guidelines for Yorktown Commons.

Water

- Water Main shall be in accordance with the "Standard Specifications for Water and Sewer Main Construction in Illinois" as well as the latest edition of the Village of Lombard Municipal Code.
- Fire protection shall be in accordance with the National Fire Prevention Code as well as the latest edition of the Village of Lombard Municipal Code.
- There is a 12" water main along the south side of the Ring Road (north of the property) and an 8" water main just east of the property. Water and/or fire service taps for any proposed development shall be made off of the 12" main to the north.
- The existing fire hydrant at the northeast corner of the property is to remain. Additional fire hydrants are not likely to be required for this property.

Sanitary

- Sanitary Sewer shall be in accordance with the "Illinois Recommended Standards for Sewage Works" as published by the IEPA, the "Standard Specifications for Water and Sewer Main Construction in Illinois" and the latest edition of the Village of Lombard Municipal Code.
- The re-routing of the existing 12" sanitary sewer to the perimeter of the property will require approximately 210 lf of 12" sanitary sewer at depths between 20'-25' to be installed. 3 additional manholes will be required. See attached Improvement Plan – Parcel 3.

Storm Water

- The stormwater management approach has been detailed in the attached Preliminary Stormwater Analysis – Yorktown Commons Redevelopment.
- Storm sewer shall be designed to convey the flow resulting from the 10-year storm event.
- The proposed development will result in a reduced runoff rate from this property. Therefore it is understood that the existing 48" storm sewer to which this property is tributary (south of the property) has adequate capacity to convey the 10-year storm event from this site.
- A development increasing the total impervious area by greater than 5,000 sf will require stormwater detention in accordance with Chapter 151 of the Village of Lombard Code of Ordinances and with the DuPage Countywide Stormwater Ordinance.
- A development creating 2,500 sf or greater of net new impervious area will require Best Management Practices (BMP's) in accordance with the DuPage Countywide Stormwater Ordinance. BMPs such as vegetated filter strips, bioswales, and/or pervious pavers would need to be incorporated into the site plan to meet the BMP requirements. An additional option would be the use of a mechanical BMP system.

Lighting

- Lighting shall meet Village of Lombard and Yorktown Center requirements.
- Lighting electric design shall comply with the National Electric Code.

Private Utilities

- AT&T provides cable, telephone, and data to the project area. Ryan Kennelly (630-573-5681) should be contacted to initiate coordination with AT&T.
- COMCAST is a local broadband provider for the area. Contact Denise Corbin (847-789-0638) with installation/coordination questions.
- ComEd is the local electric provider. Call the New Business hotline to initiate project coordination.

- NICOR is the local natural gas provider. Contact Daniel McEvilly (New Business Coordinator 815-272-9251) to initiate project coordination.

Parcel 4

Demolition

- The existing building will be demolished from the demising wall of the Carson's Furniture Gallery to the east (approximately 76,300 sf) as shown on the attached Demolition Plan – Parcel 4.
- Curb and gutter removal will be required as needed to accommodate the proposed site plan.
- Tree removals will be required.
- The parking lots and sidewalks will be removed as needed and properly disposed of.
- 4 light poles and foundations will require removal.
- The existing monument sign at the southeast corner of the property will be removed.
- 3 grease traps at the rear of the existing building will be removed.
- Approximately 1,100 feet of sanitary sewer main and services and 5 manholes will likely require removal from the rear of the building.
- Approximately 1,900 lf of water main and water services will be removed.
- The 10" watermain near the north property line has two water main connections and multiple service connections that run to the neighboring property to the north. These connections will be required to be reconnected to the proposed watermain.
- There are 2 fire hydrants to be removed.
- Approximately 1,000 lf of storm sewer and 5 manholes will be removed.

Layout & Paving

- Roadway and paving design and construction shall comply with Chapter 500 of the Village of Lombard Specifications Manual, the "Illinois Department of Transportation (IDOT) "Standard Specifications for Road and Bridge Construction", and the "Supplemental Specifications and Recurring Special Provisions", latest editions and updates (IDOT).
- Recycled asphalt shall be allowed so long as it is in conformance with IDOT Specifications.
- Access is to be allowed from Grace Street and the Yorktown Center Ring Road. Access points are to be approved by the Village of Lombard and Yorktown Center.
- Sidewalk will be installed along the Yorktown Ring Road and Grace Street. Sidewalks will comply with the Illinois Accessibility Code.
- Asphaltic pavement sections for driveways and parking lots shall consist of 4" of HMA over 8" of aggregate base. Asphaltic pavement sections for a proposed residential roadway shall consist of 4" of HMA over 12" of aggregate base.
- Concrete pavement sections for driveways shall consist of 7^{1/2"} of Portland Cement Concrete Pavement over 4" of aggregate base.
- Parking requirements are to be in accordance with the Planned Development Design Guidelines for Yorktown Commons.
- Site geometrics and density are to be in accordance with the Planned Development Design Guidelines for Yorktown Commons.

Water

- Water Main shall be in accordance with the "Standard Specifications for Water and Sewer Main Construction in Illinois" as well as the latest edition of the Village of Lombard Municipal Code.

- Fire protection shall be in accordance with the National Fire Prevention Code as well as the latest edition of the Village of Lombard Municipal Code.
- There is a 16" watermain along the east property line and an existing 12" watermain on the south side of the Ring Road to remain in place.
- The 10" watermain near the north property line to be removed will be replaced and reconnected to the existing 16" water main along the east property line. There are two water main connections and multiple service connections, off this existing 10" main, that run to the neighboring property to the north. These connections will be required to be reconnected to the proposed watermain.

Sanitary

- Sanitary Sewer shall be in accordance with the "Illinois Recommended Standards for Sewage Works" as published by the IEPA, the "Standard Specifications for Water and Sewer Main Construction in Illinois" and the latest edition of the Village of Lombard Municipal Code.
- The existing 12" sanitary sewer running south along the east property line is intended to remain.
- The 8" and 10" sanitary mains being removed along the north side of the property will be required to be re-installed as the Carson's building to remain and the property to the west are tributary to this sewer system. See attached Improvement Plan – Parcel 4.

Storm Water

- The stormwater management approach has been detailed in the attached Preliminary Stormwater Analysis – Yorktown Commons Redevelopment.
- Storm sewer shall be designed to convey the flow resulting from the 10-year storm event.
- The proposed development will result in a reduced runoff rate from this property. Therefore it is understood that the existing 24" storm sewer to which this property is tributary (near the south property line) has adequate capacity to convey the 10-year storm event from this site.
- Should a proposed development increase the total impervious area by greater than 5,000 sf approximately 4.8 ac-ft of detention will be required.
- A development creating 2,500 sf or greater of net new impervious area will require Best Management Practices (BMP's) in accordance with the DuPage Countywide Stormwater Ordinance. BMPs such as vegetated filter strips, bioswales, and/or pervious pavers would need to be incorporated into the site plan to meet the BMP requirements. An additional option would be the use of a mechanical BMP system.


Lighting

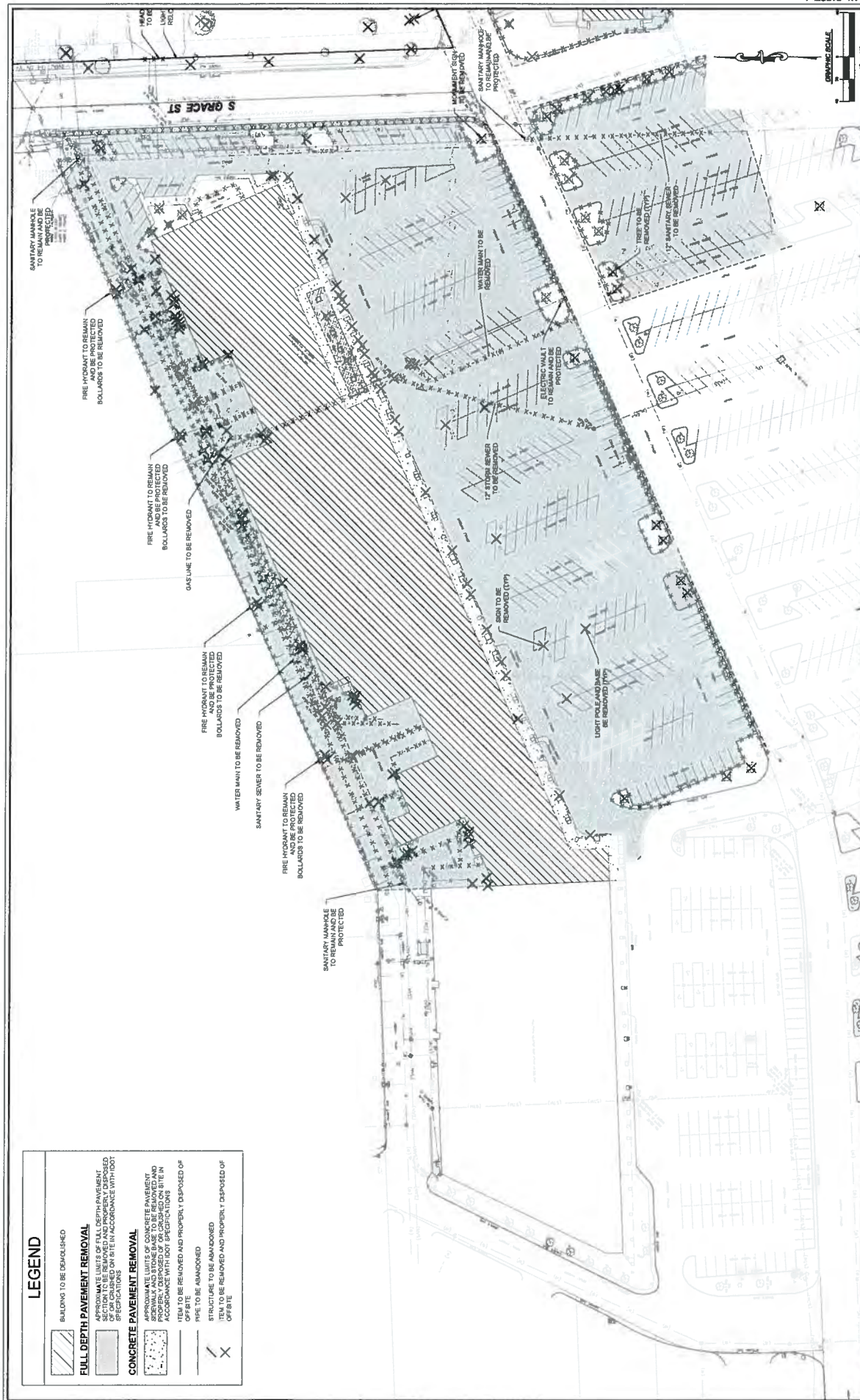
- Lighting shall meet Village of Lombard and Yorktown Center requirements.
- Lighting electric design shall comply with the National Electric Code.

Private Utilities

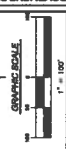
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 VVA VISION VISION... It's What We Transform With Experience. 7320 Franklin Westfield, IL 60591 830.324.0200 Phone 830.324.0201 Fax www.vva.com		REVISIONS <table border="1"> <thead> <tr> <th>NO.</th> <th>DATE</th> <th>DESCRIPTION</th> <th>BY</th> <th>DATE</th> <th>DESCRIPTION</th> </tr> </thead> <tbody> <tr><td> </td><td> </td><td> </td><td> </td><td> </td><td> </td></tr> <tr><td> </td><td> </td><td> </td><td> </td><td> </td><td> </td></tr> <tr><td> </td><td> </td><td> </td><td> </td><td> </td><td> </td></tr> <tr><td> </td><td> </td><td> </td><td> </td><td> </td><td> </td></tr> <tr><td> </td><td> </td><td> </td><td> </td><td> </td><td> </td></tr> </tbody> </table>		NO.	DATE	DESCRIPTION	BY	DATE	DESCRIPTION																															<table border="1"> <tr> <td>PROJECT NO.</td> <td>19147</td> <td>REVISED BY</td> <td>RMH</td> </tr> <tr> <td>CLIENT</td> <td>SPRINKZ INC.</td> <td>DESIGNED BY</td> <td>VCS</td> </tr> <tr> <td>CONTRACT DATE</td> <td>10-09-2015</td> <td>CHECKED BY</td> <td>DLG</td> </tr> <tr> <td>SCALE</td> <td>1" = 100'</td> <td>PROJECT MANAGER</td> <td>DLG</td> </tr> </table>		PROJECT NO.	19147	REVISED BY	RMH	CLIENT	SPRINKZ INC.	DESIGNED BY	VCS	CONTRACT DATE	10-09-2015	CHECKED BY	DLG	SCALE	1" = 100'	PROJECT MANAGER	DLG	YORKTOWN COMMONS LONGBARD ILLINOIS		EXISTING CONDITION PLAN C2.0 DRAWING NO.	
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NOTES:
1 THEORETICAL SITE PLAN SHOWN FOR REFERENCE ONLY



YORKTOWN COMMONS

ILLINOIS

LOMBARD

OVERALL IMPROVEMENT PLAN

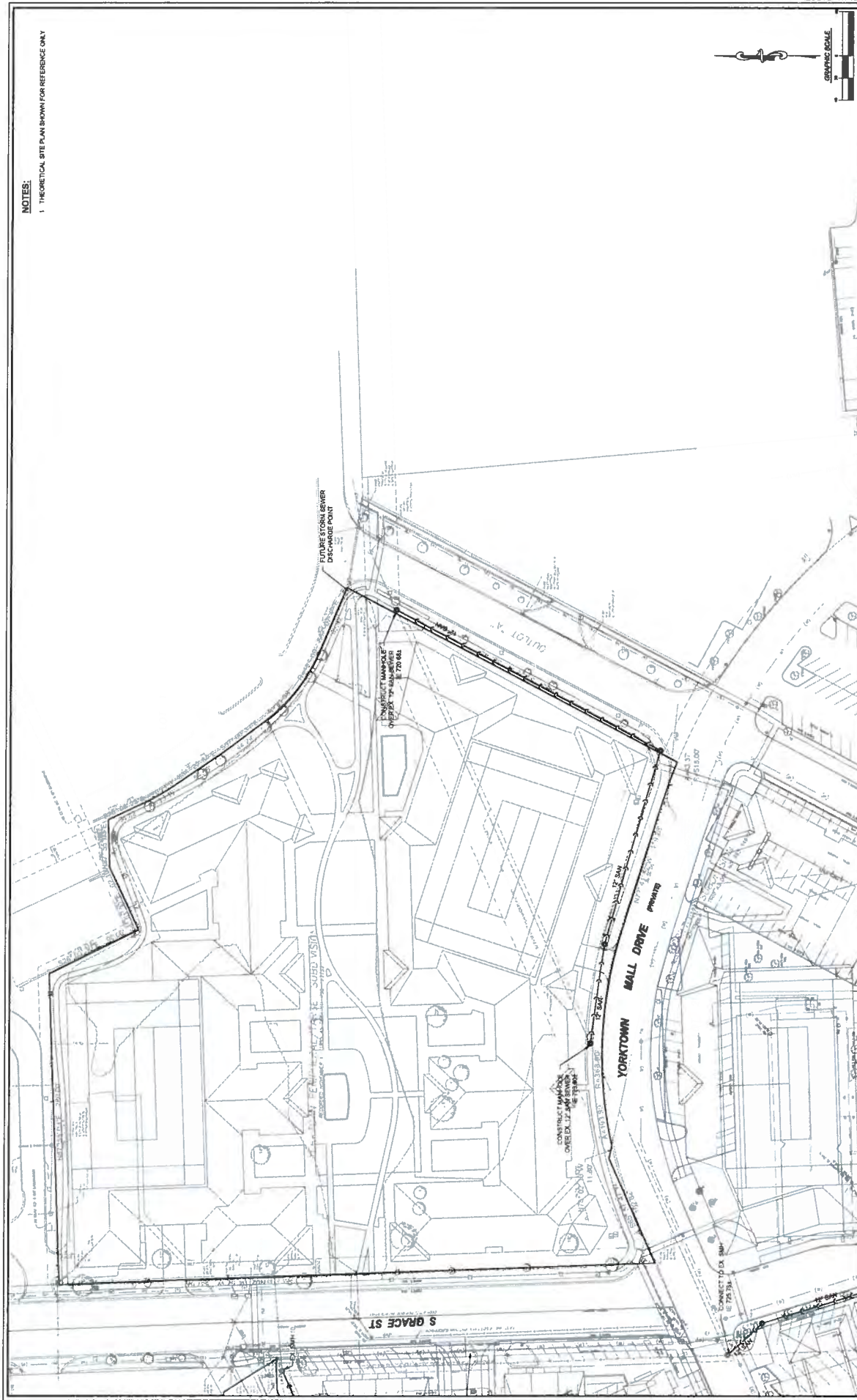
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REVISIONS		PROJECT NO.	DATE	REVISIONS	BY	CHECKED BY	DATE
NO.	DATE	DESCRIPTION	NO.	DATE	DESCRIPTION	NO.	DATE

V3 Companies
7325 Janes Avenue
Woodridge, IL 60517
Tel: 630.724.8200
Fax: 630.724.8202
www.v3co.com

15147 - OVERALL IMPROVEMENT PLAN

DATE: 01-08-2018
SCALE: 1" = 100'



NOTES:
1. THEORETICAL SITE PLAN SHOWN FOR REFERENCE ONLY

REVISIONS

NO.	DATE	DESCRIPTION

PROJECT INFORMATION

PROJECT NO: 15147
 DATE: 10-09-2016
 PROJECT MANAGER: D.C.G.

DESIGNER INFORMATION

DESIGNED BY: VRS
 CHECKED BY: D.C.G.
 SCALE: 1"=40'

CLIENT INFORMATION

7338 Anna Avenue
 Westfield, IL 60091
 630.724.9200 phone
 630.724.9200 fax
 www.v3c.com

LOCATION

YORKTOWN COMMONS
 LOMBARD
 ILLINOIS

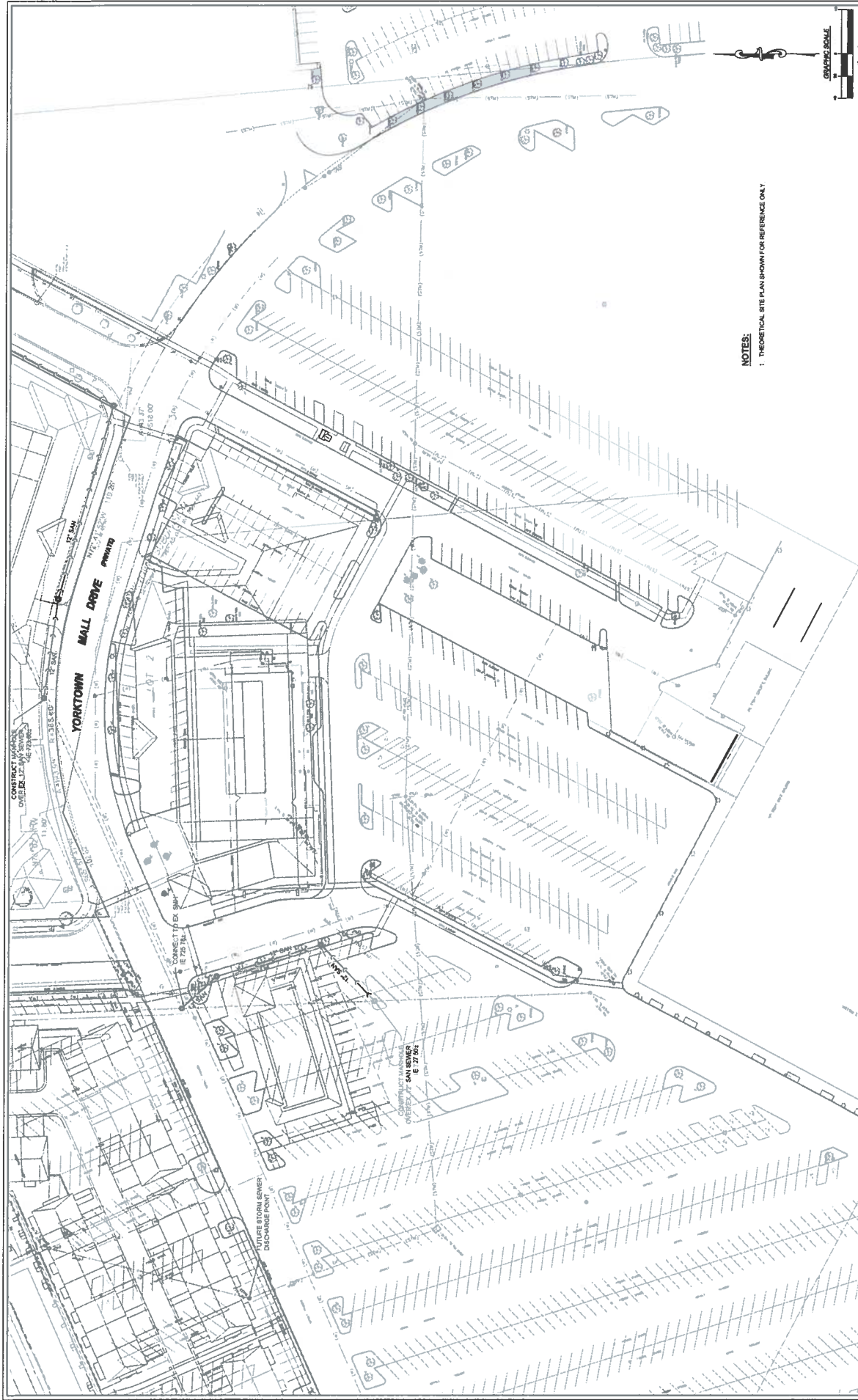
TITLE

IMPROVEMENT PLAN -
 PARCEL 1

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15147 - IMPROVEMENT PLAN - PARCEL 1





YORKTOWN COMMONS

ILLINOIS

LOMBARD

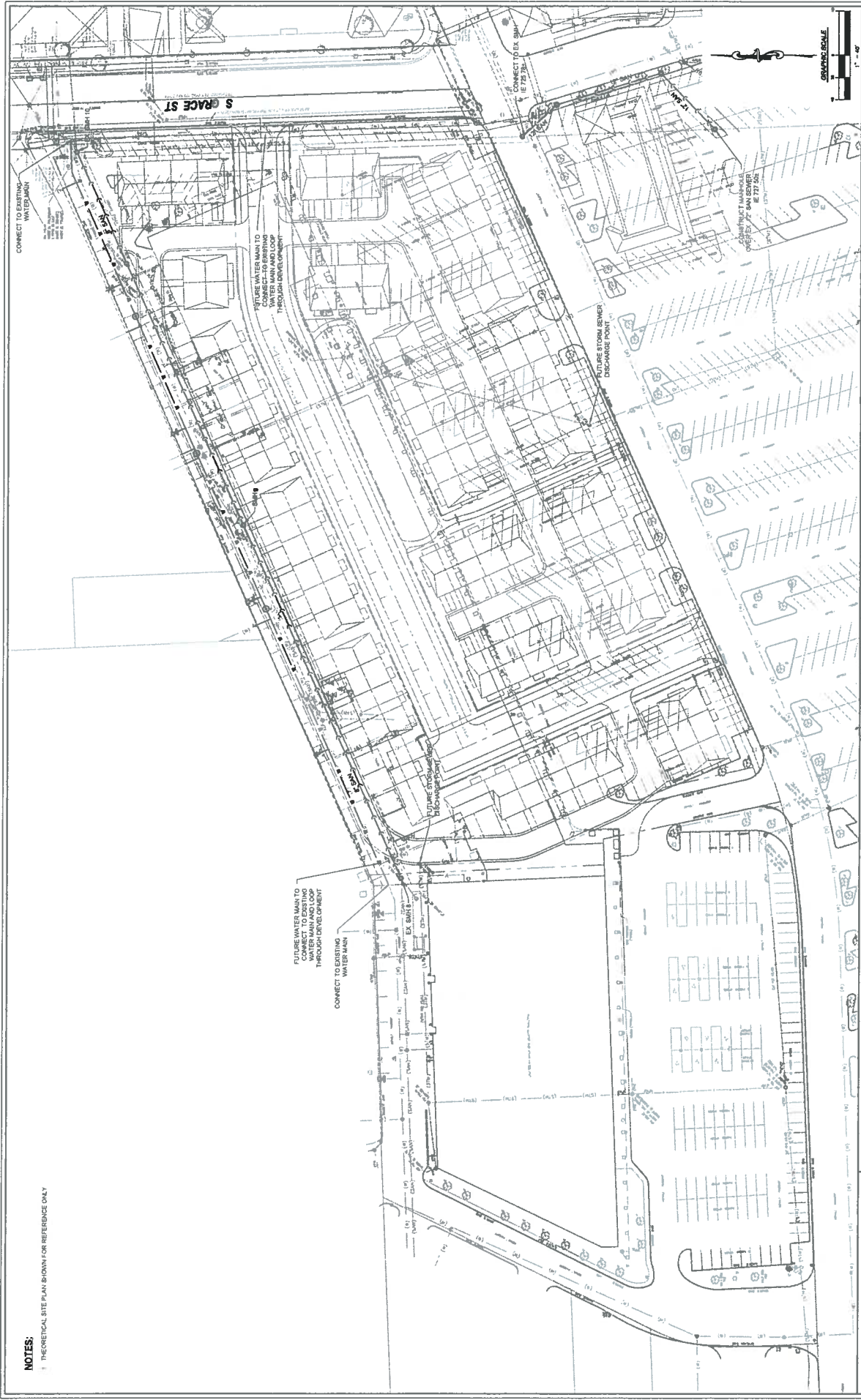
IMPROVEMENT PLAN -
PARCEL 2 AND 3

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REVISIONS		PROJECT INFO	
NO.	DATE	DESCRIPTION	DATE

VS Consulting
7338 Janna Avenue
Woodridge, IL 60517
630.724.9200 phone
630.724.9202 fax
www.vs3c.com

15147
PROJECT NO.
15147
PROJECT NAME
15147 DWS
10-09-2015
DATE
10-09-2015
SCALE
1" = 40'



NOTES:

1 THEORETICAL SITE PLAN SHOWN FOR REFERENCE ONLY



V3 Companies
7335 Jana Avenue
Woodridge, IL 60517
Tel: 815.724.8200 Phone
815.724.8202 fax
www.v3co.com

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NO.	DATE	DESCRIPTION	NO.	DATE	DESCRIPTION

PROJECT NO.	15147	DATE	10-09-2015
PREPARED BY	MP/15147/DWG	CHECKED BY	DLG
DESIGNED BY	DLG	PROJECT MANAGER	DLG

YORKTOWN COMMONS
ILLINOIS
LOMBARD

IMPROVEMENT PLAN - PARCEL 4
C3.3

EXHIBIT F – YORKTOWN COMMONS ZONING COMPARISON

PC 15-27: YORKTOWN COMMONS ZONING COMPARISON

Key	More permissive than the underlying zoning, but roughly equivalent to the existing PDs.	More permissive than the underlying zoning, but stricter than the existing PDs.	More permissive than both of the existing PDs.
	Underlying Zoning (B3 Community Shopping District)	Existing Yorktown Center PD	Proposed Yorktown Commons PD
Uses	The primary differences are that the existing Yorktown Shopping Center PD allows for any permitted or conditional use within a "B" classified zoning district and the B3 Community Shopping District has a broad array of permitted uses, but also a significant amount of conditional uses. The proposed Yorktown Commons PD is more limiting than the existing Yorktown Center PD in regards to commercial uses, but does allow for more flexibility in housing type and the amount of ground-floor residential permitted.		
Min. Lot Area	20,000 square feet	No specific minimum lot area defined.	Variation requested, although at this time none of the four proposed parcels are smaller than 20,000 square feet. Future resubdivisions that would reduce lot areas to less than 20,000 square feet are possible.
Min. Lot Width	100 feet	No specific minimum lot width defined.	Variation requested, although at this time none of the four proposed parcels are less than 100 feet wide. Future resubdivisions that would reduce lot widths to less than 100 feet wide are possible.
Min. Front Yard Setback	30 feet	In lieu of all other building setback requirements of the Zoning Ordinance, the PD requires a setback of 25 feet from the outer perimeter of the tract. There are no specific setbacks within the interior of the PD.	In lieu of traditional building setback requirements, the Design Guidelines utilize "build-to-lines" which establish a more pedestrian-oriented streetwall where buildings are closer to each other and their entrances are closer to sidewalks. The proposed build-to-lines range from 6 feet for townhomes to up to 30 feet from the property line for commercial or mixed-use buildings. Furthermore a "frontage occupancy" standard requires a certain percentage of each structure to be built along a build-to-line.
Min. Corner Side Yard Setback	30 feet		
Min. Int. Side Yard Setback	10 feet		
Min. Rear Yard Setback	30 feet		
Max. Building Height	2 stories or 30 feet, whichever is less.	Four office building shall be permitted to a max. height of 870 feet above sea level and all other buildings may be constructed to a max. height of 830 feet above sea level (This translates to four buildings of up to approximately 105-120 feet in height allowed and the remaining buildings are to be 65 feet to 80 feet or less in height).	6 stories or 100 feet, whichever is less. The maximum height of a story on the first floor is 30 feet for non-residential or mixed-use buildings and 25 feet for residential buildings. All other floors shall not exceed 14 feet in height. (The existing PDs allowed for four buildings taller than anything proposed, but the proposed PD allows for the remaining buildings to be slightly taller than in the existing PDs.)

	Underlying Zoning (B3 Community Shopping District)	Existing Yorktown Center PD	Proposed Yorktown Commons PD
Min. Open Space	10%	The PD allows for up to 30% of the site to be occupied for building purposes provided the remaining area shall remain as open spaces, which may be utilized for parking, loading berths, signs, traffic lanes and pedestrian walkways and landscaping. (This technically allows for the open space, as currently defined by the Zoning Ordinance, to be 0%)	12.5% (Includes some hardscape uses such as courtyards, squares, pedestrian paths. Usable open space provided on a roof or a podium may also be counted. The primary difference between the existing PDs and the proposal is that the existing PDs allowed for parking, traffic lanes, etc. to be counted as open space, while the proposed PD allows for uses with impervious surface as open spaces, and thus can also be 0% open space as currently defined by the Zoning Ordinance, but such spaces must be of a type that is a pedestrian amenity).
Restrictions on Business Uses	1. All business establishments shall be retail or service establishments dealing directly with consumers. All goods produced on the premises shall be sold at retail on the premises where produced; and 2. All business activities, servicing, processing and storage, except for off-street parking or loading, shall be conducted within completely enclosed buildings; except that outside display and sales, outside service areas, and drive-through/drive-in services may be allowed as conditional uses pursuant to subsection 155.103 (F) of this Chapter.	N/A	Variation requested. (The proposed PD allows for outdoor dining by right and further language is requested by staff to clarify that drive-throughs are conditional uses as opposed to permitted within the existing PDs. Outside storage will still not be permitted).
Transitional Building Setbacks	40 feet where a rear yard or interior side yard lot in the B3 district abuts a lot in the CR or a residence district	The PD waived all screening requirements provided in the Zoning Ordinance	Variation requested.
Transitional Landscape Yards	30 feet where a rear yard or interior side yard lot in the B3 district abuts a lot in the CR or a residence district		Variation requested.
Off-Street Parking	Freestanding Stores and Neighborhood and Community Centers (up to 200,000 square feet) must provide 4 spaces per 1,000 square feet of gross floor area; and Regional Shopping Centers (more than 200,000 square feet) must provide 5 spaces per 1,000 square feet)	In lieu of all other off-street parking regulations the PD requires a minimum of 7,500 parking spaces.	Variation requested to reduce the 7,500 required spaces for Yorktown Shopping Center PD and Yorktown Peripheral PD to 6,800 spaces.

EXHIBIT G –LOMBARD MULTI-FAMILY RESIDENTIAL DEVELOPMENT DENSITIES

Lombard Multi-Family Residential Developments							
Case No. or Year Built	Name	Address	Zoning	Height	# of Units	Size (Acres)	DU/Acre
Yorktown Apartments (1967 Approval)							
1998	Liberty Square Condos	2240-60 S. Grace St.	R5PD	4 stories	112	4.42	25.34
1972, SPA 14-03	Yorktown Apartments	2233, -39, and -55 S. Highland Ave.	R5PD	5-15 stories	374	7.13	52.45
		2233 S. Highland Ave.	R5PD	15 stories	236		
		2239 S. Highland Ave.	R5PD	7 stories	78		
		2255 S. Highland Ave.	R5PD	5 stories	60		
1972	Yorktown Apartments	2200-10 S. Grace St.	R5PD	6 stories	106	3.54	29.94
1969	Yorktown Green Condos	2201-22 S. Highland Ave.	R5PD	6 stories	264	12.21	21.62
				Totals	856	27.30	31.36
			Max. Allowed by Ord. No. 1323		1722	27.30	63.08
				Unit Surplus	866		
			Max. Yorktown Commons Units		970	15.00	64.67
Other Multi-Family Development							
1967	Unnamed	145 S. Main St	BSPD	3 stories	16	0.59	27.12
1968	Crescent Arms Apartments	442-44 W. Crescent Blvd.	R4	3 stories	24	1.15	20.87
1968	Westmore Apartments	1021-69 S. Westmore-Meyers Rd	R4	2 stories	230	11.48	20.03
1969	International Village	1300 S. Finley Rd.	R5PD	3 stories	671	40.24	16.67
1969	Lombard Tower Condos	33 N. Main St.	R6	11 stories	120	1.47	81.63
1972	Crescent Bend Condos	410 W. Crescent Blvd.	R4	3 stories	18	0.65	27.69
1973	Pointe West (Harmony) Apts.	1301-31 S. Finley Rd	R5PD	4 stories	336	12.89	26.07
1975	Cove Landing	2001-15 S. Finley Rd.	R5PD	8 stories	294	11.20	26.25
1975	Village Apartments	49 N. Park Ave	R6	7 stories	48	0.54	88.89
1976	Yorktown Condos	2201 S. Grace St.	R5PD	6 stories	60	2.70	22.22
1979	Park Manor Condos	43 N. Park Ave.	R6	3 stories	22	0.46	47.83
1987	Clover Creek Apartments	810-90 W. Foxworth Blvd.	R5PD	4 stories	504	30.46	16.55
1988	Covington Apartments	2101-87 S. Finley Rd.	R5PD	2 stories	257	13.43	19.14
1990	Park Avenue Condos	150 W. St. Charles Road	R6PD	8 stories	279	6.58	42.40
1990	Townhomes of Lombard Condos	42-48 N. Park Ave. & 43-49 N. Orchard Terr.	R6PD	2-7 stories	64	3.05	20.98
1971	Burnwood Apartments	2-10 Woodland Ct.	R5	3 stories	60	3.33	18.02
PC 96-26, SPA 04-09	St. Regis / Yorkbrook Condos	2000-30 St. Regis Dr.	R4PD	6 stories	210	10.50	20.00
1998	Lincoln Terrace Condos	212 W. St. Charles Rd.	R6	4 stories	48	1.41	34.04
2000	Lombard Station Condos	31 E. Grove St.	R6PD	7 stories	41	1.05	39.05
2001	Fountain Square Condos	845-75 E. 22nd St.	R4PD	5 stories	270	12.46	21.67

Case No. or Year Built	Name	Address	Zoning	Height	# of Units	Size (Acres)	DU/Acre
Other Multi-Family Development (cont.)							
2001	Parkview Point Condos	123 W. St. Charles Rd.	B5PD	4 stories	24	0.55	43.64
2002	Park West Condos	105 W. St. Charles Rd.	B5PD	4 stories	9	0.11	81.82
2003	City View Apartments	2720 S. Highland Ave.	B3PD	5 stories	403	5.00	80.60
PC 03-09	Lincoln Place Condos	1 S. Lincoln Ave.	B5PD	4 stories	39	1.03	37.86
PC 03-25	Main Street Place	229 S. Main St.	B5APD	3 stories	10	0.61	16.39
PC 04-10	Oakview Estates Condos	400-540 E. St. Charles Rd.	R4PD	5 stories	80	2.21	36.20
PC 05-43	Prairie Path Villas	300 S. Main St.	B5APD	3 stories	36	1.04	34.62
PC 06-02	Hidden Valley Condos	1400-12 and 1500-20 S. Fairfield Ave.	R4PD	3 stories	69	4.60	15.00
PC 15-04	Unnamed - Highlands of Lombard	2740 S. Highland Ave.	B3PD	5 stories	181	2.98	60.74

EXHIBIT H – “TRAFFIC STUDY”

October 2015

Yorktown Commons

Traffic Study



Prepared for:
**Rosanova &
Whitaker, Ltd.**

Eriksson Engineering Associates, Ltd.

145 Commerce Drive, Suite A

Grayslake, IL 60030

(847) 223-4804

601 W. Randolph St., Suite 500

Chicago, IL 60661

(312) 463-0551

1- INTRODUCTION

This report summarizes Eriksson Engineering Associates' (EEA) traffic analysis for a proposed mixed-use residential development, Yorktown Commons, in Lombard, Illinois. The proposed site consists of approximately 15 acres located around the intersection of Grace Street and the Yorktown Center Ring Road. Parcel 1 is located east of Grace Street and north of the Yorktown Ring Road on vacant land. Parcel 2 is the site of the former Bamboo Room restaurant and its adjacent parking. Parcel 3 will be created in the existing JC Penney parking lot. The Shops of Yorktown will be redeveloped as Parcel 4.

Yorktown Commons will be a multi-family development with the flexibility to develop a mixture of townhomes, condominiums, or apartments along with ancillary retail and restaurant space. For the traffic study, a conservative analysis was completed based on 970 apartments and 28,000 square feet of retail and restaurant space.

The purpose of this study was to identify the transportation system serving the proposed development, to determine the development's transportation characteristics, and to evaluate the impact of the development on area roadways.

EEA's evaluation of the transportation network included an analysis of the pedestrian, bicycle, public transportation, and vehicular network around the site. Data was collected of the existing volumes along with traffic control devices, sidewalks, bike routes, and traffic lanes. Projections of future traffic volumes were made for the proposed land-use plan and other near-by developments. Capacity analyses were conducted and recommendations developed for the transportation needs to accommodate the development.

Summary of Recommendations

Based on the traffic study results, the following conclusions were developed:

- The proposed development of Yorktown Commons and its associated traffic volumes can be accommodated by the area road network.
- The internal intersection of the Yorktown Ring Road at the North Entrance/McDonalds should be modified to correct existing deficiencies by providing better striping and signage to guide drivers.
- With the modifications approved for the Butterfield Entrance to Yorktown Center that are under construction and the proposed improvements for the Sam's Club project, the total projected traffic volumes will work well at the intersection of Fairfield Avenue/Yorktown Center and Butterfield road
- It is recommended that each parcel provide sidewalks along their frontage with Grace Street, the ring road, and the access routes to JC Penney, Target, or the Westin.
- It is recommended that the Grace Street be modified by converting the existing curb travel lanes into a bike lane (5 feet) and on-street parking (7 feet).

2- EXISTING TRANSPORTATION NETWORK

Site Location and Area Land-Use

Yorktown Center is a regional shopping center located in the southern section of Lombard, Illinois at the junction of Highland Avenue and Butterfield Road (northeast quadrant). It is north of the Reagan Memorial Tollway (I-88) accessible via interchanges at Highland Avenue, Midwest Road, and Illinois Route 83. The Veterans Memorial Tollway (I-355) is located one mile to the west with an interchange with Butterfield Road. Access to Yorktown Center is provided by one access point on Butterfield Road, three access drives on Highland Avenue, and three access routes to 22nd Street. **Figure 1** illustrates the location of Yorktown Center and the regional road network.

The site of the proposed master plan development is located at the intersection of Grace Street and the Yorktown Ring Road. It consists of four parcels. Parcel 1 is vacant land on the northeast corner of the intersection. Parcel 2 is to the southeast with the vacant Bamboo Room lounge and parking. Parcel 3 is located in the parking lot to the southwest. Parcel 4 contains the Shops of Yorktown retail center, without the Carson Pierre Scott Furniture store, and its parking in the northwest corner.

Land uses around the site consist of the residential apartments and condominiums, Target, and a hotel to the north. Yorktown Center's main retail building and parking is to the south. The Westin Hotel is to the east. The Carson Pierre Scott Furniture store is located to the west. **Figure 2** illustrates the project's location and adjacent roadways.

Roadway Characteristics

A description of the area roadways providing access to the site is provided below:

Butterfield Road (IL 56) is a six-lane Strategic Regional Arterial (SRA) in front of Yorktown Center with a signalized intersection at Fairfield Avenue/Yorktown Center. Butterfield Road provides three through lanes and a right-turn lane in each direction. Dual left-turn lanes are provided on westbound Butterfield Road for turns into the center and a single left-turn lane westbound for turns onto Fairfield Avenue. Butterfield Road has a speed limit of 45 mph and is under the jurisdiction of the Illinois Department of Transportation (IDOT).

Highland Avenue is a north-south arterial roadway that extends along the west side of Yorktown Mall from south of I-88 to Roosevelt Road. It has a grade separated interchange with Butterfield Road and signalized intersections at the three Yorktown access drives and with 22nd Street. There are two lanes in each direction with a center left-turn lane and a right-turn lane at Yorktown's northern entrance. It has a 35 mph speed limit and is under the jurisdiction of the Village of Lombard.

22nd Street is an east-west arterial roadway with two travel lanes in each direction and a center left-turn median. At its signalized intersection with Grace Street and Northlake Road, it has separate left-turn lanes and an eastbound right-turn lane. It has a 40 mph speed limit and is under the jurisdiction of the Village of Lombard.

Grace Street is a local four-lane undivided collector road that serves existing residential/hotel developments and provides a connection to Yorktown Center from 22nd Street. It is under the jurisdiction of the Village of Lombard.

Fairfield Avenue extends south of Butterfield Road into a business park between Butterfield Road and the tollway (I-88). At Butterfield Road, it has dual-left turn lanes and a shared thru-right-turn lane. Fairfield Avenue is under the jurisdiction of the Village of Lombard.

Yorktown Ring Road is a three-lane private road that directs internal traffic around the Yorktown Center perimeter. It has one lane in each direction and a center left-turn lane. It has all-way-stop-intersections at the Target/Westin access road and Grace Street. At the north entrance, the ring road has a three way stop intersection with the inbound traffic from Highland Avenue entering without stopping.

Majestic Drive is a two-lane residential road extending west of Highland Avenue opposite the north signalized access drive for Yorktown Center. It has a 30 mph speed limit and is under the jurisdiction of the Village of Lombard.

Northlake Road is a two-lane north-south road that extends north of 22nd Street serving existing commercial properties. It is aligned with Grace Street at its signalized intersection with 22nd Street. As it approaches 22nd Street it widens out to allow a lane for left-turns. It has a 25 mph speed limit and is under the jurisdiction of the Village of Lombard.

Figure 3 illustrates the existing roadway geometrics and traffic control.

Pedestrian and Bicycle Facilities

The development parcels were originally designed as part of an auto-centric retail development in the late 1960's with no pedestrian/bike amenities other than a sidewalk in front of the retail buildings. Sidewalks are located along the public street system, except for the east side of Highland Avenue. A side walk was provided along the Westin Hotel frontage with Target when it was constructed.

The Lilac Bikeway is a bike route designated by the Village of Lombard to connect activity centers within the village. Yorktown Center is the southern terminus of that route. It runs along 22nd Street to the west to south on Grace Street. 22nd Street is also designated as a bikeway near the site. **Figure 4** illustrates the existing pedestrian and bike facilities.



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Yorktown Center Regional Access Routes

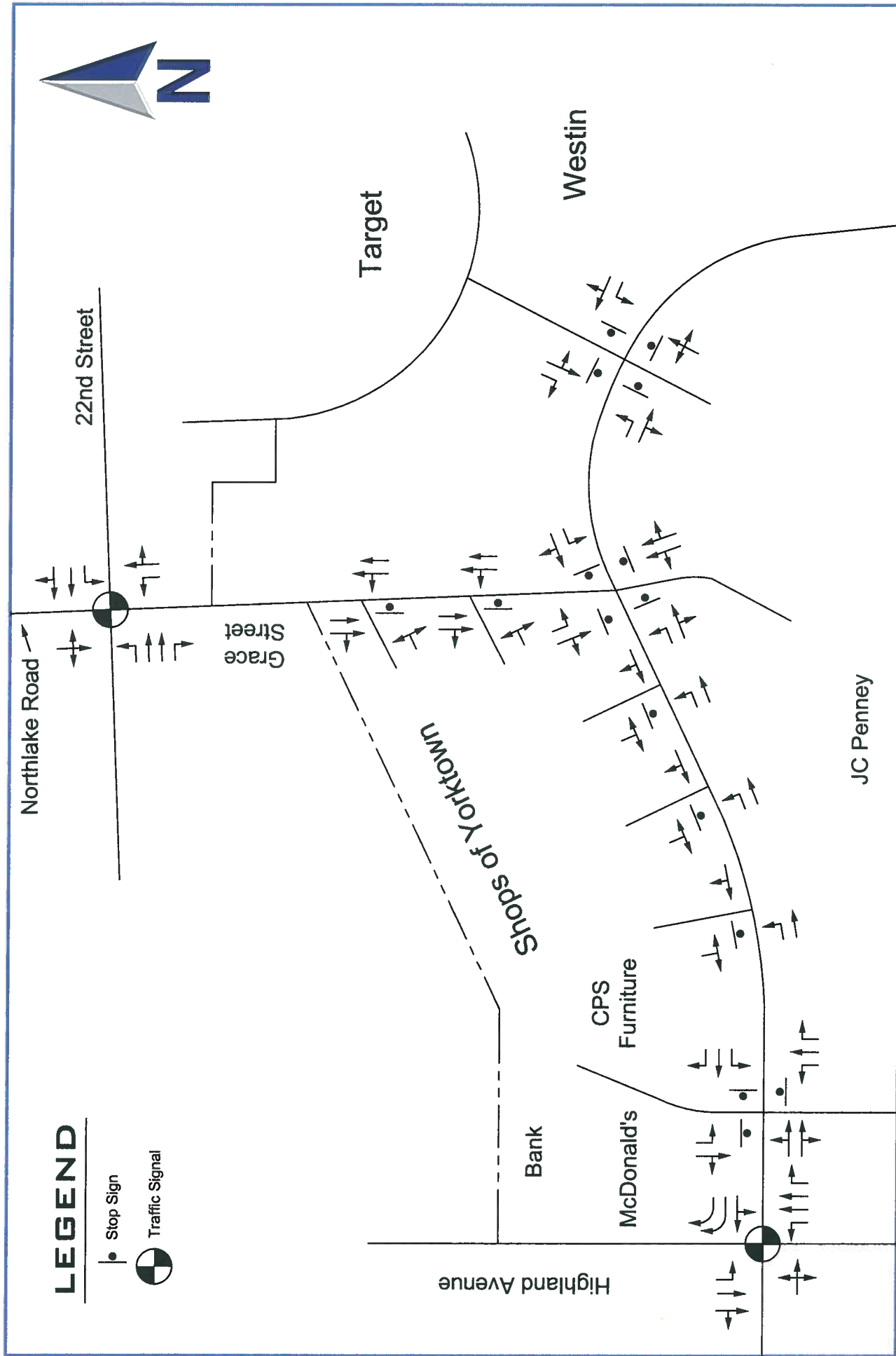
Figure 1



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Yorktown Commons Site Location and Roadways

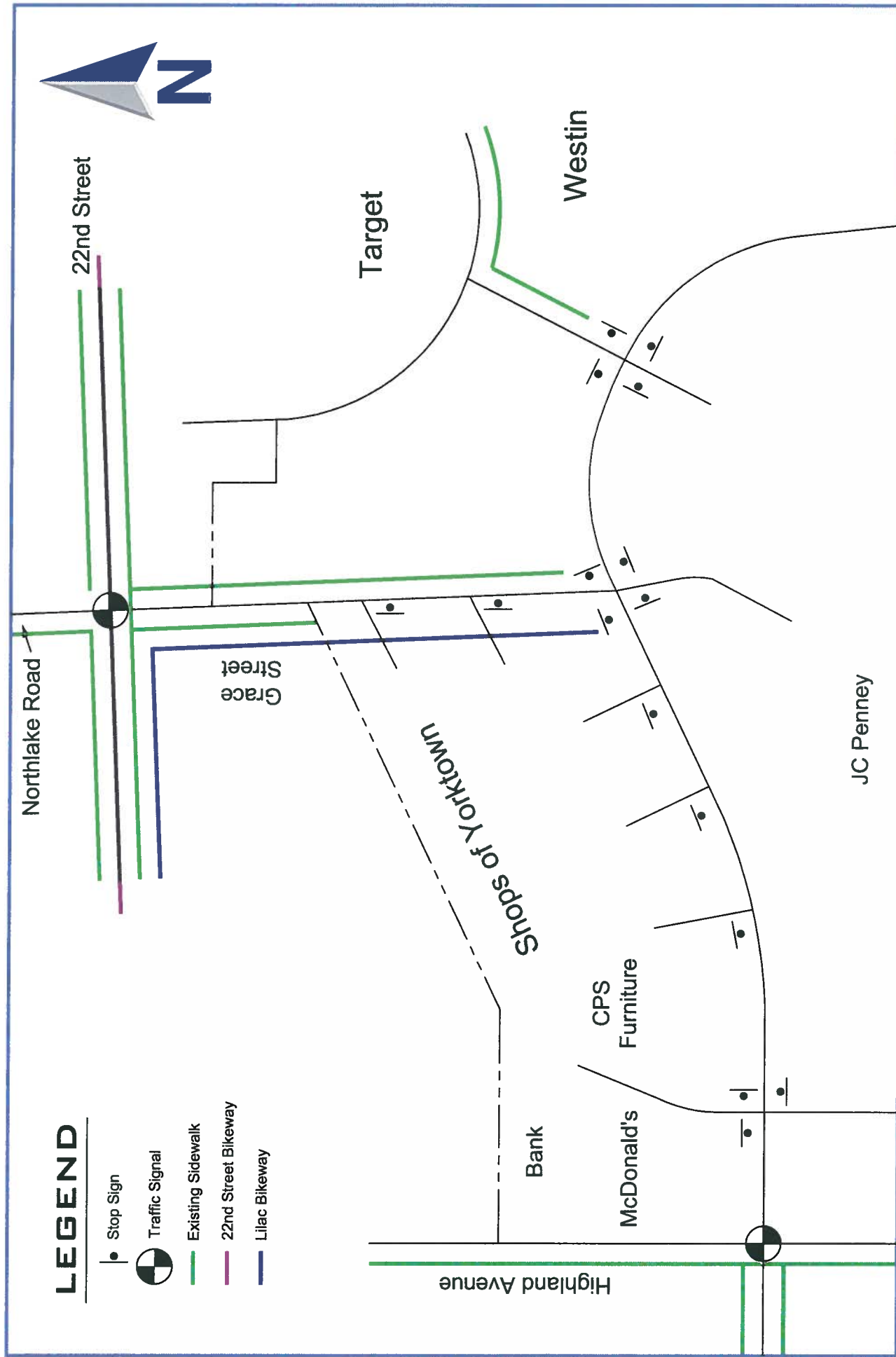
Figure 2



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Existing Roadway Geometrics and Traffic Control

Figure 3



Public Transportation

Public transportation at the site is provided by five PACE bus routes. Yorktown Center has a PACE transfer center located in the parking lot north of the JC Penney store just to the east of the development site. The five bus routes near the site are shown on **Figure 5** and are summarized below:

Route 313 - St. Charles Road

Provides daily service between the CTA Green Line Austin Station in Chicago and Branding/Finley in Downers Grove. Serves the CTA Green Line Austin Station, West Suburban Hospital, Oak Park River Forest High School, Westlake Hospital, Metra UP-West Line Melrose Park Station, Eastgate Center and Yorktown Center.

Route 322 - Cermak Road - 22nd Street

Provides daily service between the CTA Pink Line 54th/Cermak Station in Cicero and Yorktown Center in Lombard. Serves the CTA Pink Line 54th/Cermak Station, Morton West High School, North Riverside Park Mall, Broadview Village Square Shopping Center, Immaculate Heart of Mary High School, St. Joseph High School, Oakbrook Center and Yorktown Center.

Route 674 - Southwest Lombard

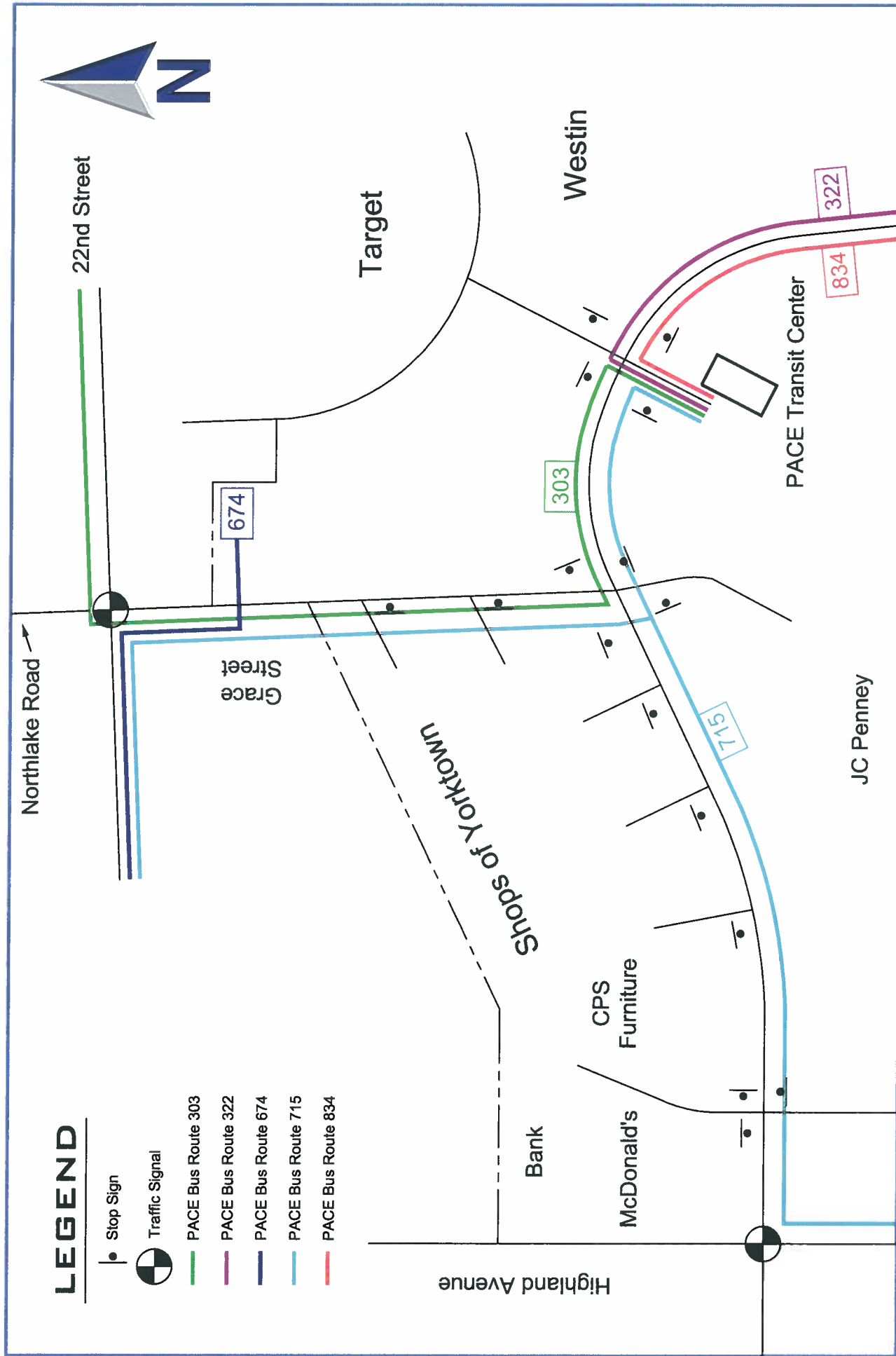
Provides weekday rush hour service between southwestern Lombard and the Metra UP - West Line Lombard Station. Serves the Yorktown Condominiums, The Covington, Cove Landing, Clover Creek, Point West, International Village, Royal Glen, the Glen Ellyn Apartment Homes and the Metra UP - West Line Lombard Station.

Route 715 - Central DuPage

Provides weekday service between the Addison Walmart and Brookhaven Plaza in Darien. Certain weekday rush hour trips serve Argonne National Laboratory. Serves the Addison Walmart, DeVry, Universal Technical Institute, Glen Oaks Hospital, the Metra UP - West Line Glen Ellyn Station, College of DuPage, Finley Square, Yorktown Center, Midwestern University, the Metra BNSF Line Westmont Station, Brookhaven Plaza and Argonne National Laboratory (certain weekday rush hour trips only).

Route 834 - Joliet-Downers Grove

Provides weekday and Saturday service from the Joliet central business district and Metra Station to Finley/Branding. Serves Lewis University, Good Samaritan Hospital, Romeoville, Lockport, Bolingbrook, Will County Courthouse, Yorktown Shopping Center and Downers Grove. Certain trips connect with Metra-BNSF service in Downers Grove.



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Public Transportation Routes

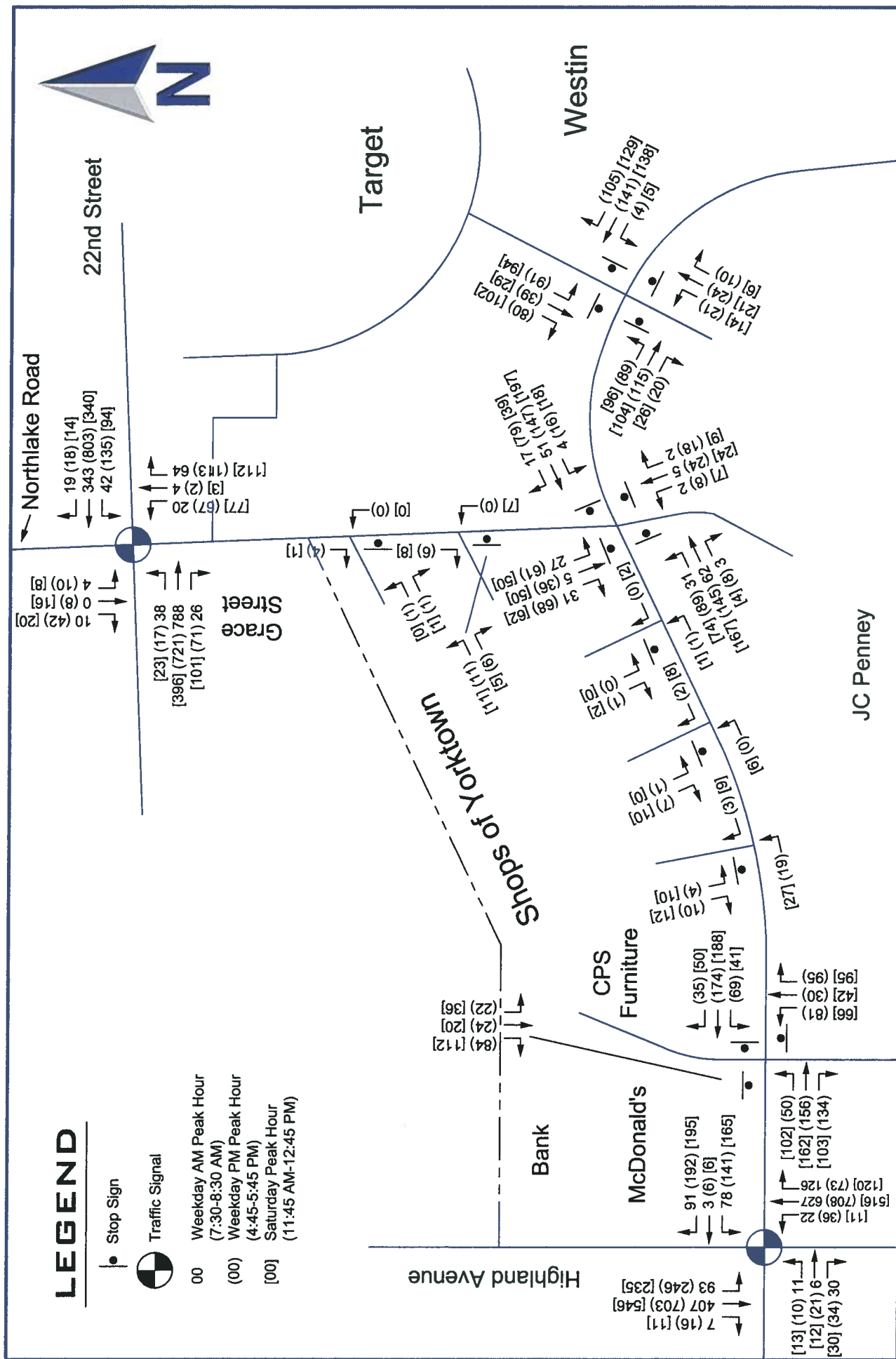
Figure 5

Existing Vehicular Volumes

Weekday morning (7:00 to 9:00 AM), evening (4:00 to 6:00 PM), and Saturday (11:00 AM to 1:00 PM) manual counts of vehicles were conducted at the following study area intersections:

- Highland Avenue and North Yorktown Center Driveway
- 22nd Street at Grace Street/Northlake Road
- Yorktown Center Ring Road at:
 - McDonalds Access
 - Shops of Yorktown Driveways
 - Grace Street/JC Penney Access Road
 - Target/Westin/JC Penney Access Roads
- Grace Street at Shops of Yorktown Driveways

Additional counts were obtained for Butterfield Road at Fairfield Avenue/Yorktown Center from the previous Yorktown and the Sam's Club traffic studies. These counts showed the peak-hours of traffic occurring from 7:30 to 8:30 AM and 4:45 to 5:45 PM on a weekday and 11:45AM to 12:45 PM on a Saturday. **Figures 6A and 6B** summarizes the existing traffic volumes with copies of the counts in the **Appendix**.





LEGEND

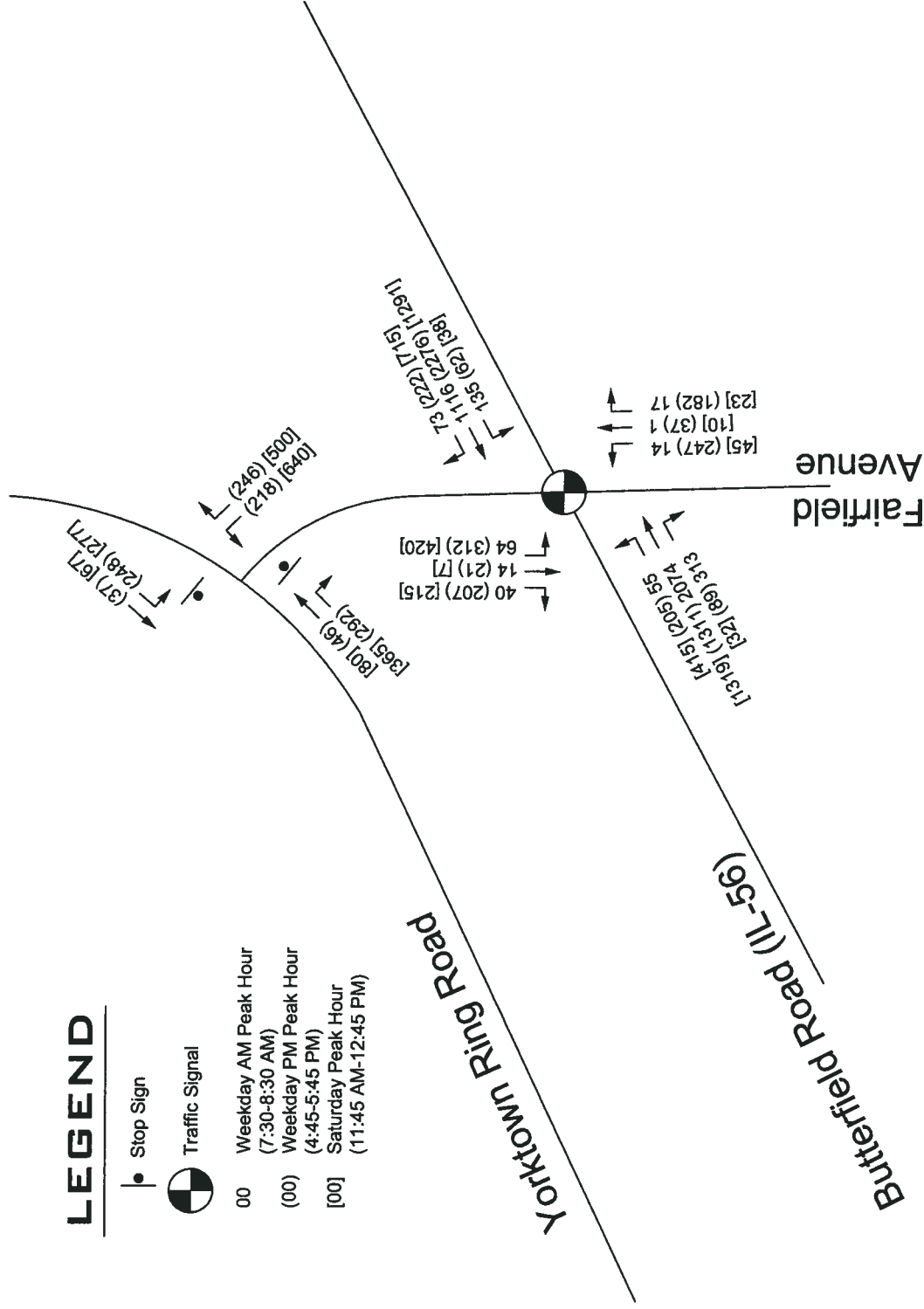


Stop Sign



Traffic Signal

- 00 Weekday AM Peak Hour
(7:30-8:30 AM)
- (00) Weekday PM Peak Hour
(4:45-5:45 PM)
- [00] Saturday Peak Hour
(11:45 AM-12:45 PM)



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Butterfield Entrance Existing Traffic Volumes

Figure 6B

3- DEVELOPMENT CHARACTERISTICS

Land-Use Plan

The Yorktown Center Masterplan calls for a mixed-use development with a combination of multi-family residential and commercial uses. Parcels 1, 2, and 4 will consist of residential units with a retail component. Parcel 3 would have a stand-alone restaurant building.

The residential development could be a mixture of several multi-family types including townhomes, row homes, condominiums, apartments, or senior housing. As each parcel is developed, detailed site plans with unit types will be presented. With this range of housing types and associated densities, the traffic volumes generated could vary significantly. To be conservative, a land-plan with multi-story apartments was used to generate the highest total volume of site traffic. For Parcel 3, a restaurant would generate the highest volume of traffic compared to a retail use.

Table 1 summarizes the land-use plan assumed for this traffic study.

Table 1
Yorktown Commons Master Plan

Parcel	Land-Use	Size
One	Retail	5,000 sq. ft.
	Apartments	400 units
Two	Retail	5,000 sq. ft.
	Apartments	170 units
Three	Restaurant	8,000 sq. ft.
Four	Retail	10,000 sq. ft.
	Apartments	400 units

Site Access

Access to each parcel will be developed as detailed engineering site plans are presented.

Trip Generation Rates

Traffic estimates were made for the site using data provided by the Institute of Transportation Engineer's Trip Generation 9th Ed. manual which contains trip generation surveys of the proposed land-uses. Trip Generation serves as the most widely accepted reference guide for establishing vehicle trip generation. The rate of vehicle trip generation was applied to the proposed development with the results are shown in **Table 2**. These trip generation rates conservatively assume the no use of public transportation by residents or employees and any interactive trips between uses on-site.

Table 2
Site Traffic Volumes Estimates

Parcel	Use	Size	ITE LUC	Morning Weekday			Evening Weekday			Saturday Peak		
				In	Out	Total	In	Out	Total	In	Out	Total
One	Retail	5,000 sq. ft.	820	3	2	5	9	10	19	14	10	24
	Apartments	400 units	220	40	160	200	154	83	237	92	92	184
Two	Retail	5,000 sq. ft.	820	3	2	5	9	10	19	14	10	24
	Apartments	170 units	220	17	70	87	72	39	111	44	44	88
Three	Restaurant	8,000 sq. ft.	931	4	2	6	40	20	60	51	35	86
Four	Retail	10,000 sq. ft.	820	6	4	10	18	19	37	28	20	48
	Apartments	400 units	220	40	160	200	154	83	237	92	92	184
Total Traffic				113	400	513	456	264	720	335	303	638

Comparison to Prior Uses

A trip generation comparison was made between the currently approve uses on the parcels and the proposed land-plan. The Shops of Yorktown contains 70,430 square feet of retail space and the Bamboo Room has 10,539 square feet of space. The Bamboo Room has been vacant for a number of years and the Shops of Yorktown are only partially occupied. **Table 3** summarizes the trip generation estimates based on the ITE trip generation data.

Table 3
Trip Volume Estimates for Existing Uses

Building	Use	Size	ITE LUC	Morning Weekday			Evening Weekday			Saturday Peak		
				In	Out	Total	In	Out	Total	In	Out	Total
Shops	Retail	70,430	820	42	26	68	125	136	261	177	163	340
Bamboo	Restaurant	10,539	931	6	3	9	53	26	79	68	47	115
Subtotal				48	29	77	178	162	340	245	210	455
Difference in Traffic from Prior Uses				+65	+371	+436	+278	+102	+380	+90	+93	+183

As expected, the proposed plan will generate more traffic in the morning peak-hour when most retail and restaurant uses are not very active at Yorktown Center. In the evening peak-hour, more inbound traffic is generated than under the previous uses. Saturday traffic only sees a marginal increase when the residential plan replaces the existing retail/restaurant uses.

Directional Distribution

The directional distribution of traffic approaching and departing the site was estimated based on the general travel patterns through the study area derived from the peak hour traffic volumes. The trip distribution for the site is shown on **Table 4** and **Figure 7**.

Table 4
Directional Distribution

Direction	Distribution
North on Highland Avenue	20%
South on Highland Avenue	20%
East on 22 nd Street	20%
West on 22 nd Street	20%
East on Butterfield Road	10%
West on Butterfield Road	10%
Total	100%

Site Traffic Assignment

Based on trip generation and directional distribution estimates, the site generated traffic was assigned to the proposed access drives and area roadways. **Figure 8** illustrates the traffic assignment for the Yorktown Center traffic volumes based on the distribution analysis. Since this is a master plan, specific site plans and access drives have not been determined for each development parcel. The traffic assignments made an assumption of the future driveway locations.

Other Development Traffic

Three other developments and their site traffic volumes were included in the projections of future traffic volumes in the study area. These developments are:

- Sam' Club at the southeast corner of Butterfield Road and Fairfield Avenue (135,016 sq. ft.)
- Yorktown Apartments expansion on Highland Avenue north of Yorktown Center (76 units)
- City View at the Highlands apartments north of The Dump store (183 units)

The Village of Lombard provided the Sam's Club Traffic Study by KLOA, Inc. and apartment traffic estimates were made by EEA. The site traffic from each project was then assigned to the road system and included in the total traffic volumes.

Total Projected Traffic Volumes

The future volume projections represent the Year 2021 traffic conditions, which projects traffic beyond the construction of the development. The total volumes included the following:

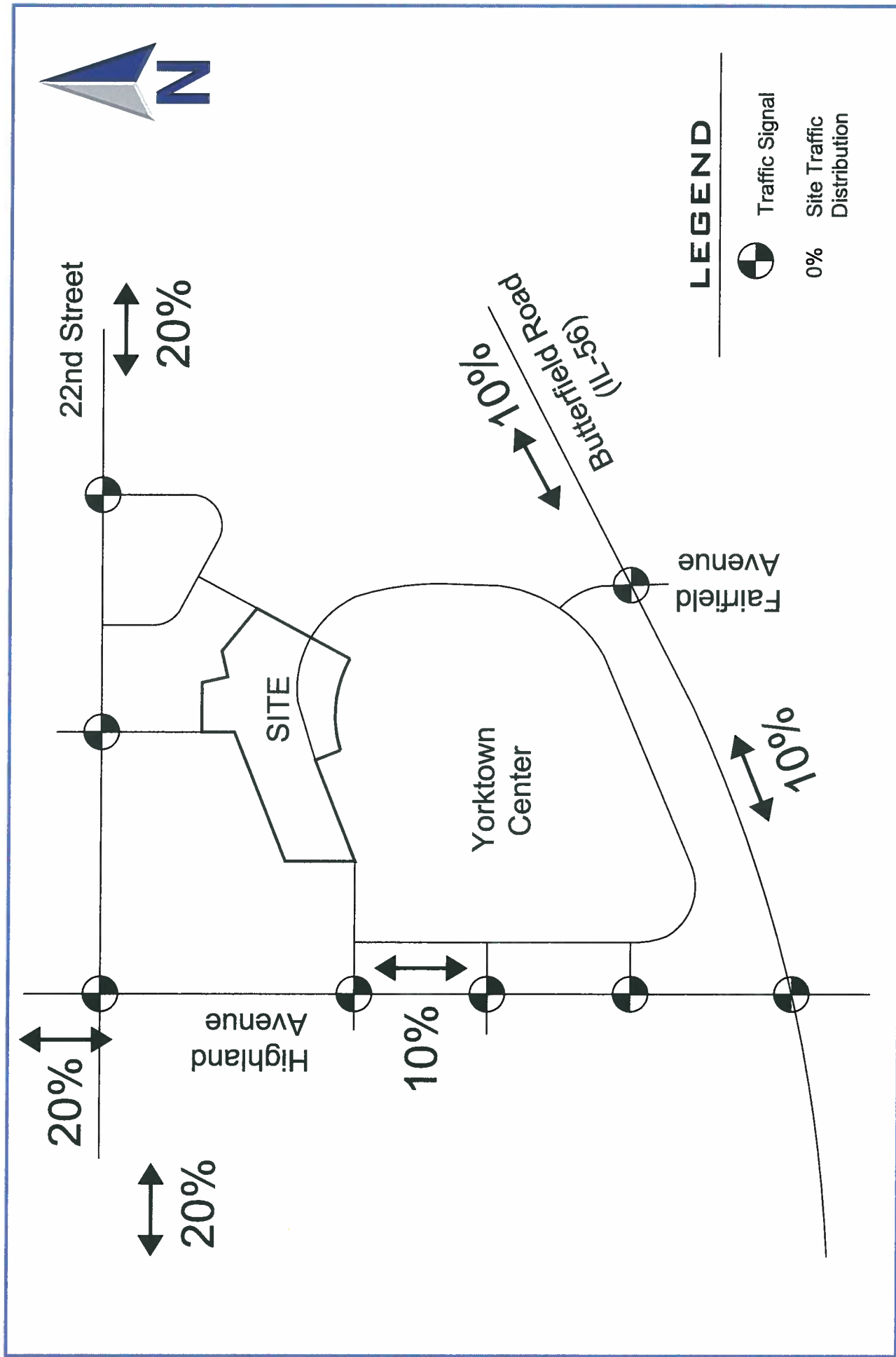
- Existing peak hour traffic volumes from the 2015 counts.
- Estimated site-generated traffic for the complete build out of the three near-by developments
- Regional growth in traffic volumes on the surrounding roadway system.
- The Yorktown Commons development site traffic volumes.

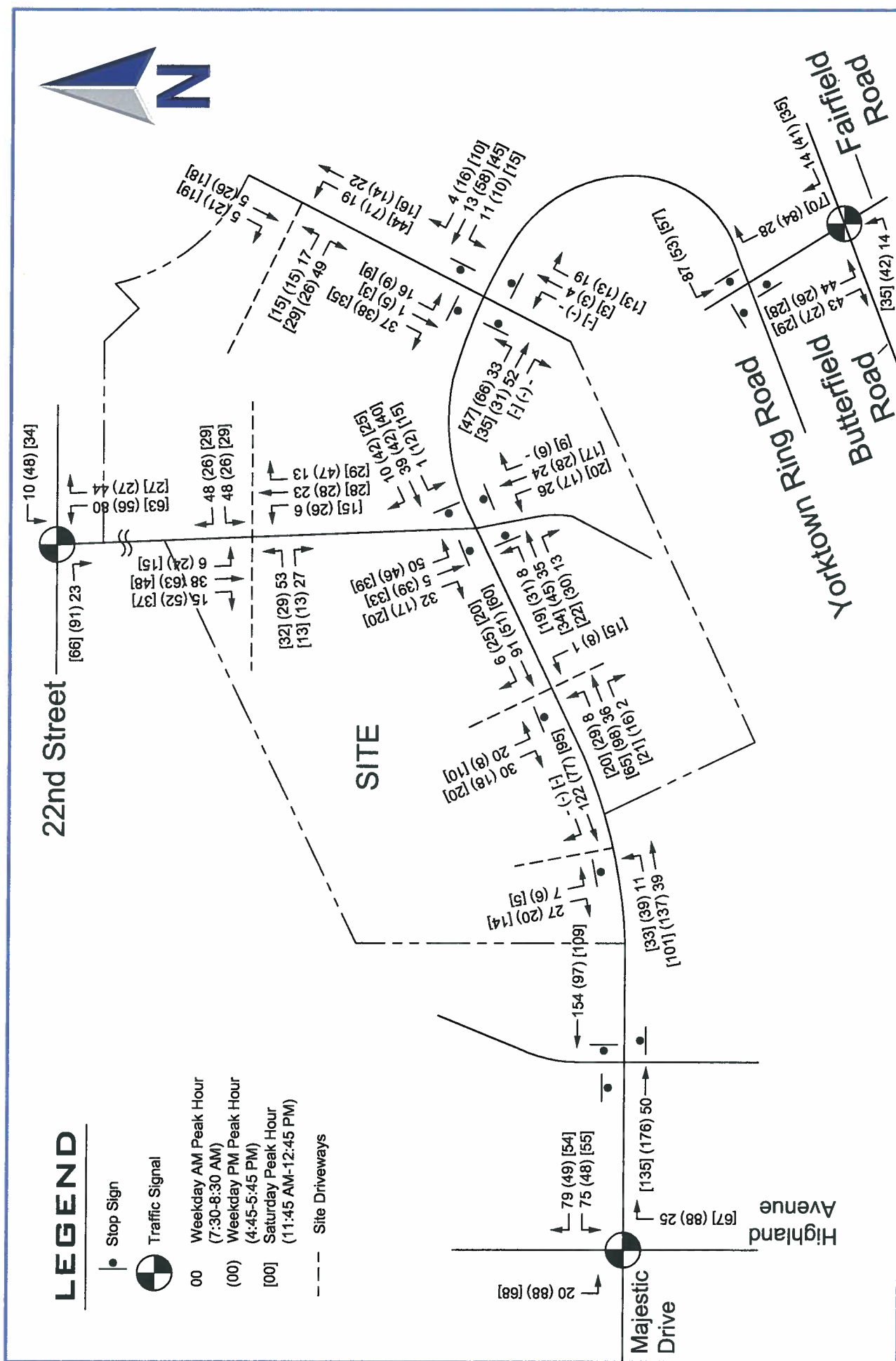
Regional growth represents an increase in existing traffic volumes due to ambient growth not attributable to any one particular new development. A regional growth rate of 0.6 percent per year was applied to the existing traffic volumes based on the 2040 Chicago Metropolitan Agency for Planning (CMAP) population and employment projections. The Year 2021 total traffic projections are shown in **Figure 9**.

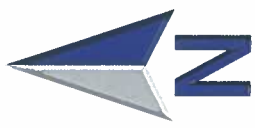


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Directional Distribution
Figure 7

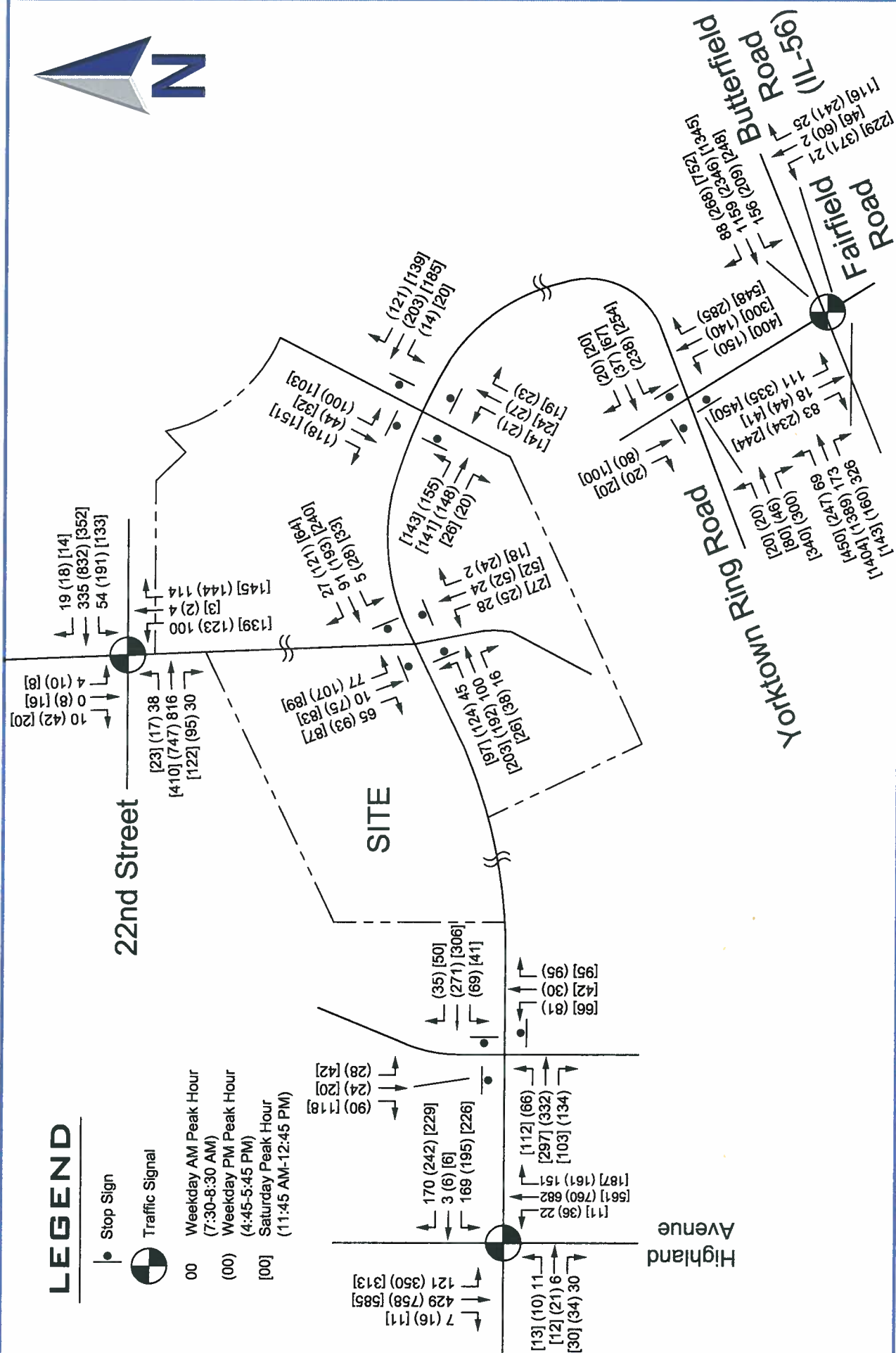






LEGEND

- Stop Sign
-  Traffic Signal
- 00 Weekday AM Peak Hour (7:30-8:30 AM)
- (00) Weekday PM Peak Hour (4:45-5:45 PM)
- [00] Saturday Peak Hour (11:45 AM-12:45 PM)



2021 Total Traffic Volumes

Figure 9

4- ANALYSES

In order to determine the impact of the proposed development on the study area intersections and the access drives, intersection capacity analyses were conducted with the proposed and nearby developments included. An intersection's ability to accommodate traffic flow is based on the average control delay experienced by vehicles passing through the intersection. The intersection and individual traffic movements are assigned a level of service (LOS), ranging from A to F based on the control delay created by a traffic signal or stop sign. Control delay consists of the initial deceleration delay, queue move-up time, stopped delay, and final acceleration delay. LOS A has the best traffic flow and least delay. LOS E represents saturated or at capacity conditions. LOS F experiences oversaturated conditions and extensive delays. The Highway Capacity Manual definitions for levels of service and the corresponding control delay for both signalized and unsignalized intersections are shown in **Table 5**.

Table 5
Level of Service Criteria for Intersections

Level of Service	Description	Control Delay (seconds/vehicle)	
		Signals	Stop Signs
A	Minimal delay and few stops	<10	<10
B	Low delay with more stops	>10-20	>10-15
C	Light congestion	>20-35	>15-25
D	Congestion is more noticeable with longer delays	>35-55	>25-35
E	High delays and number of stops	>55-80	>35-50
F	Unacceptable delays and over capacity	>80	>50

Source: Highway Capacity Manual 2010

Capacity analyses were conducted for each intersection area using the SYNCHRO computer program to determine the existing and future operations of the access system. These analyses were performed for the weekday and Saturday peak-hours. Copies of the capacity analysis summaries are included in the **Appendix**.

22nd Street at Grace Street/Northlake Road

This intersection is a signalized intersection that works well today and will continue to do so in the future with the projected traffic volumes (See **Table 6**) with minimal delays. No additional improvements are proposed.

Table 6
22nd Street at Grace Street/Northlake Road
Intersection Level of Service and Total Delay

Approach	AM Peak Hour		PM Peak Hour		Saturday Peak Hour	
	2015	2021	2015	2021	2015	2026
Eastbound	A-8.1	A-9.9	B-10.5	B-11.2	A-8.8	A-9.1
Westbound	A-6.7	A-8.1	A-8.5	A-8.7	A-8.4	A-8.5
Northbound	B-18.2	C-25.0	C-21.5	C-26.7	B-19.8	C-24.5
Southbound	C-30.2	C-30.1	C-26.9	C-26.9	C-31.3	C-31.3
Overall Intersection	A-8.6	B-11.6	B-11.0	B-12.3	B-11.2	B-12.8

Yorktown Ring Road at the Grace Street/JC Penney Access Road

This intersection is an All-Way-Stop-Controlled intersection that works well today and with the total traffic volumes (See **Table 7**). It is recommended that the two-lane northbound approach should be restriped to a separate left-turn lane and shared thru/right-turn lane. Crosswalks and sidewalks should be provided at this intersection as each quadrant is developed.

Table 7
Yorktown Ring Road at Grace Street/JC Penney Access
Intersection Level of Service and Total Delay

Approach	AM Peak Hour		PM Peak Hour		Saturday Peak Hour	
	2015	2021	2015	2021	2015	2026
Eastbound	A-8.1	A-9.1	B-10.4	B-14.9	B-10.3	B-13.4
Westbound	A-7.8	A-9.1	B-11.4	C-20.5	B-11.4	C-16.9
Northbound	A-7.8	A-8.8	A-9.2	B-11.8	A-9.2	B-11.2
Southbound	A-7.8	A-8.9	A-10.0	B-13.6	B-9.9	B-12.5
Overall Intersection	A-7.9	A-9.0	B-10.6	C-16.1	B-10.5	B-14.1

Yorktown Ring Road at the JC Penney/Target/Westin Access Roads

This intersection is an All-Way-Stop-Controlled intersection that works well today and will continue to do so in the future (See **Table 8**) with minimal delays. No roadway improvements are required as part of the master plan development. Crosswalks and sidewalks should be provided at this intersection.

Table 8
Yorktown Ring Road at JC Penney/Target/Westin Access
Intersection Level of Service and Total Delay

Approach	PM Peak Hour		Saturday Peak Hour	
	2015	2021	2015	2026
Eastbound	B-10.2	B-12.3	B-10.0	B-11.7
Westbound	B-12.0	C-17.3	B-11.9	C-15.7
Northbound	B-10.1	B-11.5	A-9.9	B-10.9
Southbound	B-10.4	B-12.0	B-10.2	B-11.5
Overall Intersection	B-10.9	B-13.9	B-10.7	B-13.0

Highland Avenue at North Yorktown Entrance and Majestic Drive

This intersection is a signalized intersection serving as the north entrance of Yorktown Center. Based on the capacity analyses (See **Table 9**) and total traffic volumes, no additional improvements are required.

Table 9
Highland Avenue at North Yorktown Entrance and Majestic Drive
Intersection Level of Service and Total Delay

Approach	AM Peak Hour		PM Peak Hour		Saturday Peak Hour	
	2015	2021	2015	2021	2015	2026
Eastbound	C-22.7	B-15.9	C-20.6	B-17.8	C-23.5	C-20.7
Westbound	C-30.5	C-29.6	C-24.7	C-23.0	C-25.3	C-25.3
Northbound	A-7.0	B-12.8	B-14.9	C-22.0	B-13.2	B-16.6
Southbound	A-5.8	B-10.4	B-11.7	B-18.4	B-11.2	B-14.6
Overall Intersection	A-9.8	B-15.3	B-15.2	B-20.5	B-15.0	B-17.8

Butterfield Road at Yorktown Center/Fairfield Avenue

This intersection is a signalized intersection serving Yorktown Center to the north and the proposed Sam's Club project to the south. As part of the Sam's Club project, the intersection will be improved by restriping the westbound left-turn lane to dual left-turns and adding a northbound right-turn lane. Previous studies of the Yorktown Center entrance have identified problems that are being corrected with the current construction at the internal intersection just north of this intersection. Overall, the intersection level of service is good under existing and projected traffic conditions as shown in **Table 10**.

Table 10
Highland Avenue at Yorktown Ring Road and Majestic Drive
Intersection Level of Service and Total Delay

Approach	AM Peak Hour		PM Peak Hour		Saturday Peak Hour	
	2015	2021	2015	2021	2015	2026
Eastbound	B-16.9	B-12.0	C-25.9	C-30.6	C-25.5	C-30.3
Westbound	B-15.8	B-16.4	C-30.1	D-38.3	B-18.5	C-27.5
Northbound	D-44.6	C-31.4	E-56.4	E-58.2	D-47.2	E-57.0
Southbound	D-42.1	D-38.8	D-54.0	D-55.0	D-48.8	E-61.6
Overall Intersection	B-17.5	B-17.7	C-33.7	D-40.0	C-26.0	D-35.2

Yorktown Butterfield Access at the Ring Road

This internal intersection is a three legged intersection connecting the Butterfield Road traffic to the Yorktown ring Road. The ring road traffic is under stop sign control. It has a number of traffic issues that have been documented in previous reports and will be corrected with the improvements under construction (See **Table 11**). No additional roadway improvements are required as part of the master plan development.

Table 11
Yorktown Butterfield Entrance at the internal Ring Road
Intersection Level of Service and Total Delay

Approach	PM Peak Hour		Saturday Peak Hour	
	2015 Existing Geometrics	2021 Proposed Improvements	2015 Existing Geometrics	2021 Proposed Improvements
Overall Intersection	B-19.7	B-12.6	F-59.4	D-34.4

Yorktown Ring Road at the North Yorktown/ McDonald's Access Road

This intersection is a four legged intersection with a three stop way that stops the northbound, westbound, and southbound legs. The eastbound leg has inbound free flow traffic from Highland Avenue. The southbound traffic has two lanes striped as a shared thru/right-turn lane and a separate left-turn lane. Westbound and northbound traffic has separate right-turn, thru, and left-turn lanes (three lanes). Eastbound traffic is striped as a shared thru/left-turn lane and a separate right-turn lane.

Observations during the evening and Saturday peak-hours at the intersection revealed the following:

- Overall, the intersection worked well from a capacity viewpoint with minimal congestion.
- Driver confusion was regularly observed regarding which vehicle has the right-of-way.
- Two thirds of the southbound traffic turns right toward Highland Avenue.
- The signage indicating that oncoming traffic does not stop (westbound) was partially obscured by vegetation.

- Inbound traffic turning left to McDonald's came to a stop even though they have the right-of-way. As a result, the inbound thru traffic uses the lane striped as a right-turn lane as a thru lane to bypass the left-turns.
- Approximately 50% of the northbound right-turn traffic used the northbound thru lane to turn right instead of the separate right-turn lane that is provided. Approaching the intersection, the existing striping/signage does not indicate a separate right-turn lane.
- Westbound thru traffic uses the left-turn lane to enter the left-turn lane at the Highland Avenue traffic signal.

Based on these observations and capacity analyses (see **Table 12**), the following improvements are recommended.

- Install larger signage on the three stop approaches indicating inbound traffic from Highland Avenue has the right-of-way.
- Stripe the inbound leg from Highland Avenue with a left-turn lane and a shared thru/right-turn lane.
- Stripe the southbound approach as a separate right-turn lane to accommodate the high-right-turn volume and a shared thru/left-turn lane.
- Stripe the northbound leg with additional advanced striping/signage to indicate that the northbound right-turn lane is available.

Table 12
Yorktown Ring Road at North Yorktown Entrance/McDonald's Access
Intersection Level of Service and Total Delay

Approach	PM Peak Hour		Saturday Peak Hour	
	2015	2021	2015	2026
Overall Intersection	A-4.1	A-5.9	A-5.0	A-11.3

Pedestrian Connections

It is recommended that each parcel provide sidewalks along their frontage with Grace Street, the ring road, and the access routes to JC Penney, Target, or the Westin.

Bike Routes on Grace Street

As noted previously, the Lilac Bikeway follows Grace Street to its southern terminus at Yorktown Center. Currently, it does not have a designated on-street or off-street bike lane. Grace Street is a four lane road with two travel lanes in each direction. The existing and projected traffic volumes do not warrant four lanes. One travel lane in each direction would be adequate.

As part of the Yorktown Commons project, it is proposed that the cross-section would be modified by converting the curb travel lane in each direction into a bike lane (5 feet) and on-street parking (7 feet). Near the intersections at Grace Street and at the Ring Road, on-street parking will not be permitted to maintain the existing left-turn lanes.

EXHIBIT I – TRAFFIC STUDY REVIEW (KLOA)

MEMORANDUM TO: William Heniff
Village of Lombard

FROM: Javier Millan
Senior Consultant

DATE: November 16, 2015

SUBJECT: Yorktown Commons Traffic Study Review
Lombard, Illinois

This memorandum summarizes the results of a review conducted by Kenig, Lindgren, O'Hara, Aboona, Inc. (KLOA, Inc.) of the traffic study for the proposed Yorktown Commons mixed use development prepared by Eriksson Engineering Associates, Ltd.

Background

The proposed Yorktown Commons development will consist of approximately 15 acres of land located around the intersection of Grace Street and the Yorktown Center ring road. As proposed, various types of residential land uses together with ancillary retail and restaurant space would be developed. The development would have access via all of the existing access drives serving the Yorktown Center.

Traffic Impact Study Review

The traffic study was conducted following all industry guidelines set forth by the Institute of Transportation Engineers (ITE) and it analyzed a conservative scenario as it relates to trip generation by assuming the development of 970 apartment units and 28,000 square feet of retail and restaurant space. Based on a review of the traffic study and the results of the capacity analyses we offer the following:

- The capacity analyses indicate that the studied intersections have sufficient reserve capacity to accommodate the projected traffic volumes and continue operating at acceptable levels of service.

- We concur with the recommended striping and signing modifications at the intersection of the Yorktown Center ring road and the north-south access drive immediately east of Highland Avenue. However, we would like to see a preliminary striping plan for our review.
- We concur with the recommendations to provide sidewalks along the frontage of each parcel on Grace Street and the Yorktown Center ring road

Conclusion

Based on our review, it is our opinion that the proposed Yorktown Commons development can be accommodated by the Yorktown Center's internal roadway and the external intersections. However, as plans for each parcel are developed, the Village should review them individually as it relates to accessibility, site circulation and compliance with Village Code.

**EXHIBIT J – N: ALTA / ACSM LAND TITLE AND TOPOGRAPHIC SURVEY – PARCELS 1
THROUGH 4**

ALTAIACSM LAND TITLE SURVEY
OF

YORKTOWN MIXED USE DEVELOPMENT
LOWBARD, ILLINOIS

YORKTOWN COMMONS - PARCEL 2



Table with 3 columns: Station, Description, and Remarks. The table lists various survey points and measurements along the property boundaries.

Table with 2 columns: Station and Description. This table provides specific details about the survey points and the nature of the measurements taken.



NOTES FROM SCHEDULE B
1. THE SURVEY WAS MADE BY THE SURVEYOR IN THE FIELD AND THE RESULTS WERE CHECKED BY THE SURVEYOR IN THE OFFICE.
2. THE SURVEY WAS MADE IN ACCORDANCE WITH THE REQUIREMENTS OF THE ILLINOIS SURVEYING ACT.
3. THE SURVEY WAS MADE IN ACCORDANCE WITH THE REQUIREMENTS OF THE ILLINOIS SURVEYING ACT.

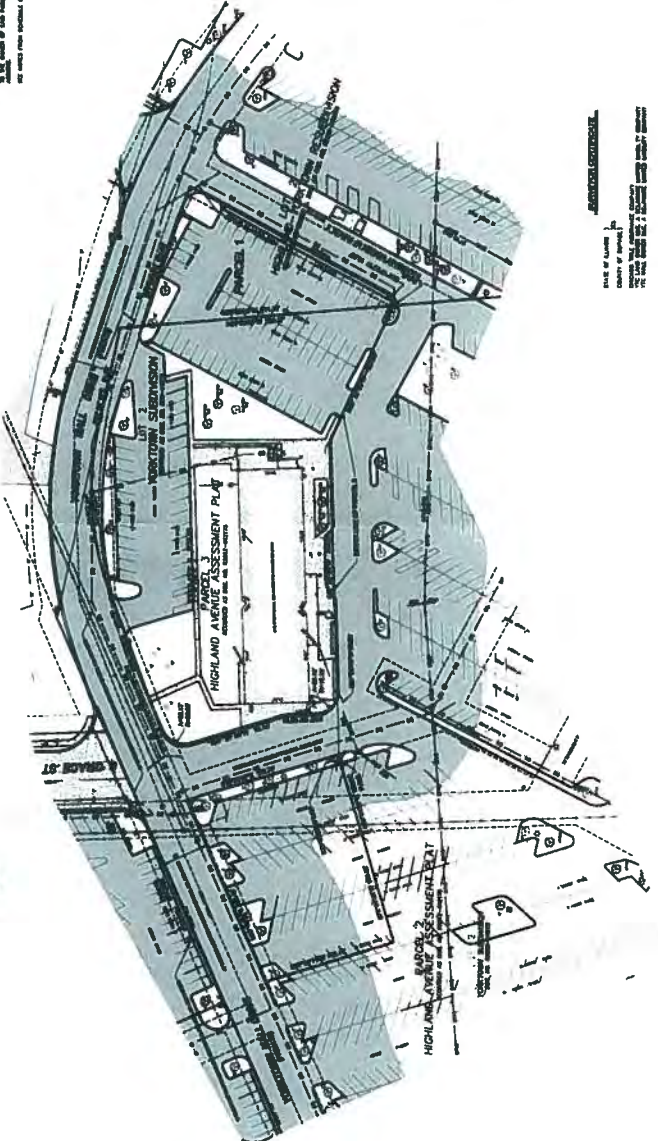


Table with 3 columns: Station, Description, and Remarks. This table continues the list of survey points and measurements from the previous table.

ALL OTHER DISTANCES ARE NOT PLANNED

NOTES FROM SCHEDULE B
1. THE SURVEY WAS MADE BY THE SURVEYOR IN THE FIELD AND THE RESULTS WERE CHECKED BY THE SURVEYOR IN THE OFFICE.
2. THE SURVEY WAS MADE IN ACCORDANCE WITH THE REQUIREMENTS OF THE ILLINOIS SURVEYING ACT.
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ALTAIACSM LAND TITLE SURVEY
OF
YORKTOWN MIXED USE DEVELOPMENT
LOMBARD, ILLINOIS
YORKTOWN COMMONS - PARCEL 3

ALL INFORMATION CONTAINED HEREIN IS THE PROPERTY OF THE SURVEYOR AND IS NOT TO BE REPRODUCED OR TRANSMITTED IN ANY FORM OR BY ANY MEANS, ELECTRONIC OR MECHANICAL, INCLUDING PHOTOCOPYING, RECORDING, OR BY ANY INFORMATION STORAGE AND RETRIEVAL SYSTEM, WITHOUT THE WRITTEN PERMISSION OF THE SURVEYOR.

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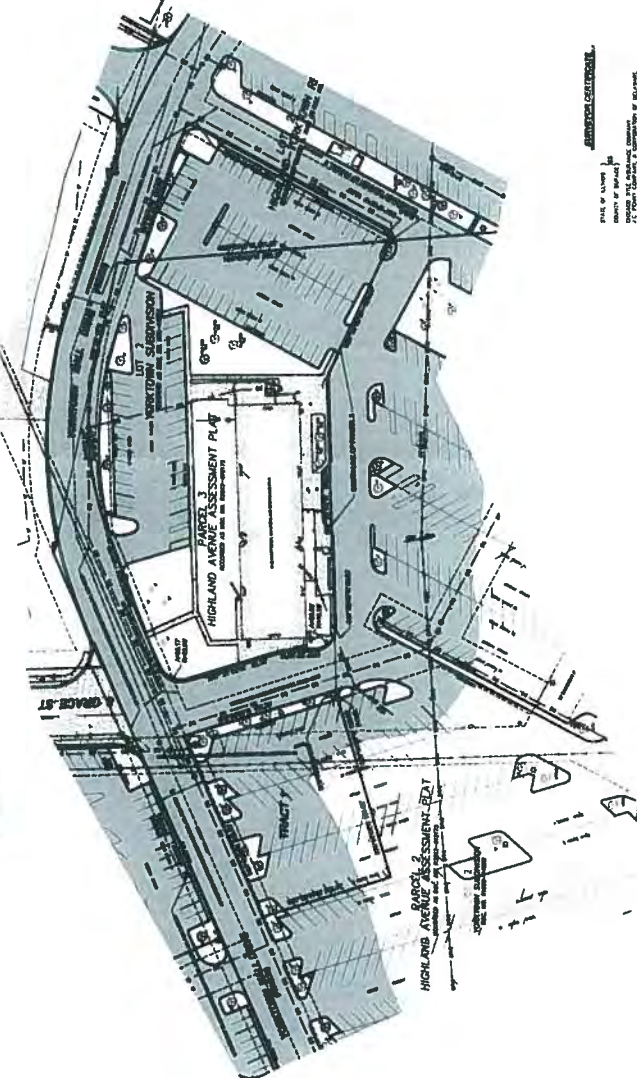
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ALTAIACSM LAND TITLE SURVEY
OF
YORKTOWN MIXED USE DEVELOPMENT
LOMBARD, ILLINOIS
YORKTOWN COMMONS - PARCEL 3

ALTAIACSM LAND TITLE SURVEY
OF
YORKTOWN MIXED USE DEVELOPMENT
LOMBARD, ILLINOIS
YORKTOWN COMMONS - PARCEL 3

NOTES FROM SCHEDULE B

NO.	DESCRIPTION	DATE
1	REVISION	08-20-2011
2	REVISION	08-20-2011
3	REVISION	08-20-2011
4	REVISION	08-20-2011
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96	REVISION	08-20-2011
97	REVISION	08-20-2011
98	REVISION	08-20-2011
99	REVISION	08-20-2011
100	REVISION	08-20-2011



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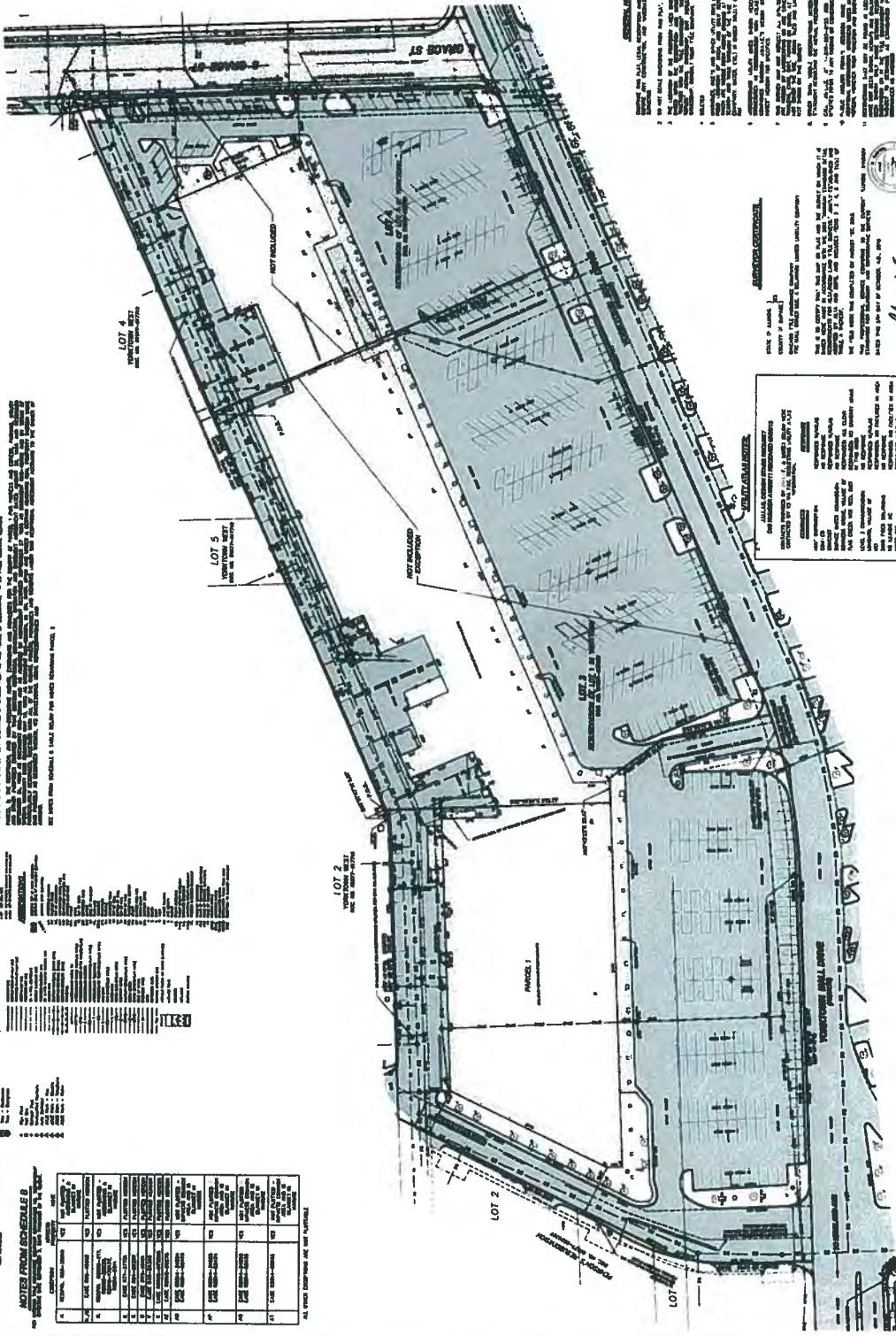
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This aerial photograph shows a residential area with a road and a large building. The road is a dark, winding line that runs diagonally across the image. The building is a large, light-colored structure with a dark roof, located in the upper left quadrant. The surrounding area is filled with smaller buildings and greenery.

This technical drawing illustrates a mechanical assembly, possibly a pump or engine component, with various parts and dimensions. The drawing includes a detailed cross-section view of a component, showing internal features like a central shaft, a piston, and a connecting rod. The assembly is shown in a perspective view, with various parts labeled with letters (A, B, C, D, E, F, G, H, I, J, K, L, M, N, O, P, Q, R, S, T, U, V, W, X, Y, Z) and numbers (1, 2, 3, 4, 5, 6, 7, 8, 9, 10, 11, 12, 13, 14, 15, 16, 17, 18, 19, 20, 21, 22, 23, 24, 25, 26, 27, 28, 29, 30, 31, 32, 33, 34, 35, 36, 37, 38, 39, 40, 41, 42, 43, 44, 45, 46, 47, 48, 49, 50, 51, 52, 53, 54, 55, 56, 57, 58, 59, 60, 61, 62, 63, 64, 65, 66, 67, 68, 69, 70, 71, 72, 73, 74, 75, 76, 77, 78, 79, 80, 81, 82, 83, 84, 85, 86, 87, 88, 89, 90, 91, 92, 93, 94, 95, 96, 97, 98, 99, 100). The drawing also includes a detailed view of a component, showing its internal structure and dimensions. The overall drawing is a technical illustration of a mechanical assembly, likely a pump or engine component, with various parts and dimensions.

NOTES FROM SCHEDULE B		APPROPRIATION		FUND	
LINE	DESCRIPTION	AMOUNT	PERCENT	AMOUNT	PERCENT
1	GENERAL	100	100	100	100
2	ADMINISTRATIVE	10	10	10	10
3	PERSONNEL	50	50	50	50
4	EQUIPMENT	10	10	10	10
5	TRAVEL	10	10	10	10
6	TELEPHONE	10	10	10	10
7	POSTAGE	10	10	10	10
8	RENT	10	10	10	10
9	OTHER	10	10	10	10
10	TOTAL	100	100	100	100

[illegible]

Discussion

[illegible]

[Handwritten signature]

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DATE 08-14-2019 BY 60322 UCBAW

U.S. DEPARTMENT OF JUSTICE
FEDERAL BUREAU OF INVESTIGATION
WASHINGTON, D.C. 20535

16147	Project No.	TAJACAN LAND TITLE SURVEY
46113414	Drawn by	YORKTON FIELD USE DEVELOPMENT

[illegible]

CONTINUING PARTNERSHIP
AGREEMENT

7200 Johns Avenue, Suite 100
Washington, D. C. 20017
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620.734.8044 fax
VISA/MC

